



Oklahoma City Community and Neighborhood Enhancement Program



**ANNUAL
REPORT 2021**

BACKGROUND

On September 12, 2017, Oklahoma City voters approved the “Better Streets, Safer City” capital improvements program: 13 general obligation (G.O.) bond propositions, a permanent ¼ cent sales tax to support public safety and operations, and a temporary 27-month continuation of the expired MAPS 3 penny sales tax.

The temporary sales tax component, called the Community and Neighborhood Enhancement Program, continues the MAPS momentum. It will build more complete, livable streets, and fund other community and neighborhood improvements that give residents multiple options to safely get where they need to go. Oklahoma City can use these sales tax funds strategically to enhance and extend multiple forms of transportation and improve the quality of public infrastructure throughout the community.

The temporary sales tax began on January 1, 2018 and was expected to generate about \$240 million during its 27-month term. Upon expiration March 31, 2020, an additional \$19 million was received over original projections.

Collections were received monthly from the Oklahoma Tax Commission, deposited into a dedicated fund and divided into five project categories:

- **street resurfacing - \$168 million**
- **streetscapes - \$30 million**
- **sidewalks - \$25 million**
- **trails - \$20 million**
- **bicycle infrastructure - \$14 million**

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A PROGRAM FOR
DEBT-FREE
PROJECTS
TO IMPROVE
INFRASTRUCTURE
BY BUILDING
BETTER AND
SAFER STREETS
FOR DRIVERS,
SIDEWALKS FOR
PEDESTRIANS, BIKE
LANES AND TRAILS
FOR CYCLISTS.

GOVERNANCE STRUCTURE

The Oklahoma City Council passed a resolution on November 7, 2017, to establish a board to make recommendations regarding priorities, timelines and other elements of the temporary penny sales tax projects.

The Community and Neighborhood Enhancement Advisory Board works with City staff to consider City policies and plans, develop project selection criteria, identify highest-priority projects, and recommend how to allocate funding to projects within the five categories. The board recommends projects to City Council for approval. City Council considers the board’s recommendations for final approval of funded projects.

The board typically met on the first Monday of each month in the Council Chamber on the third floor of City Hall, 200 N Walker Ave. Meeting schedules, agendas, minutes and videos of meetings can be accessed online at okc.gov/agenda

board members

Hal McKnight, Chair
At Large

Jason Johnston, Vice-Chair
At Large

Michael Taylor
Ward 1

Anthony Carfang Jr.
Ward 2

Jeff Groves
Ward 3

Vacant
Ward 4

Jim Lehew
Ward 5

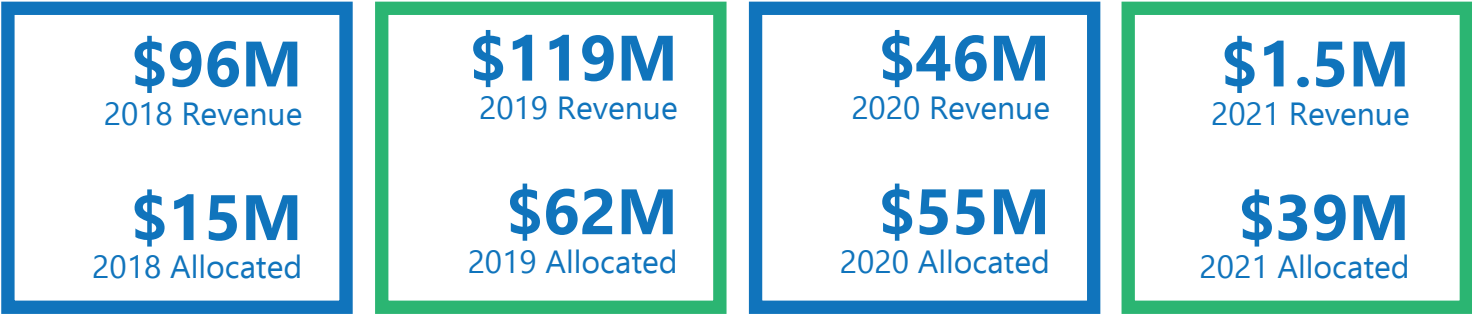
Kim Hickerson
Ward 6

Sandino Thompson
Ward 7

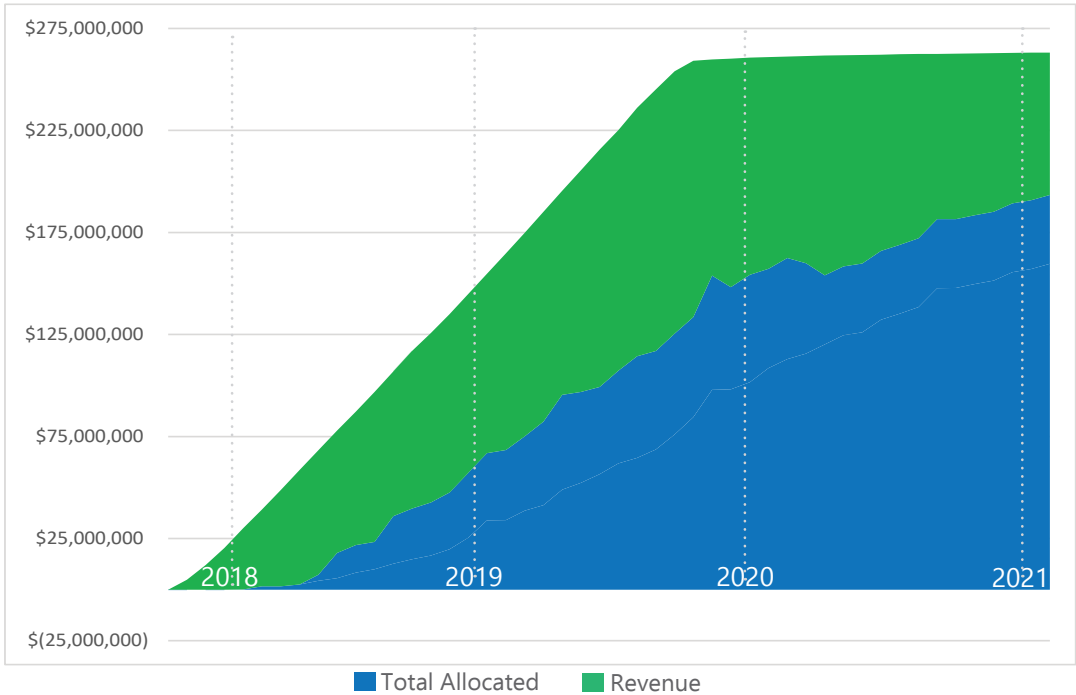
Don Noble
Ward 8

Steve Dixon
At Large

FINANCIAL OVERVIEW



Revenue and Allocations - Four-Year Program Totals



PROCESS

An extensive public outreach process generated thousands of project ideas identified as Better Streets Safer City in the 2017 G.O. bond package. City staff used a range of criteria to evaluate and rank the projects, which generated a priority list.

The board refined project evaluation criteria, which staff used to re-rank unfunded G.O. bond projects. At the onset of the process, the board also recommended several high-impact and high-priority street resurfacing projects from the bond package to be moved to the sales tax list, enabling those projects to be completed before bond funding was available.

As sales tax collections were received, the board funded the highest priority projects within each category.

SUCCESS

STRATEGY

The Implementation Plan includes project scope and budget reports for each project recommended by the Advisory Board and approved by City Council through the Community and Neighborhood Enhancement Program.

The reports contain information about individual projects, including project details, scope of work, map, budget, expenditures and proposed schedule.

Projects approved by City Council are updated in the Implementation Plan and online at okc.gov/BetterSafer

year
1

- adopted project methodology for all project categories;
- received ranked lists identifying projects that could be funded within each project category; and
- recommended more than half of the identified projects to the City Council for approval.

year
2

- evaluated and recommended thirteen residential resurfacing projects, fifty-three sidewalk projects and one trail project;
- assessed and advised budget revisions on twelve arterial and residential resurfacing projects;
- managed the completion of thirty-six arterial and residential resurfacing projects and advised City Council return remaining funds for reallocation; and
- contributed in a Bicycle Infrastructure Workshop.

year
3

- finalized forty-one project budgets, allowed for nearly \$9M returned to the program for reallocation. As a result, six new resurfacing projects were funded;
- allocated \$18M in excess tax collections to street enhancements (\$6.8M), sidewalks (\$1M), trails (\$8M) and bicycle facilities (\$2.2M);
- installed delineators, striping and signage on first bicycle facility: General Pershing from May to Lee; and
- began construction on first street enhancement project to include a mid-block crossing, traffic signal infrastructure, striping and sidewalk at NW 10th and Vermont.

year
4

- completed twelve arterial resurfacing projects, freeing up \$4.6M to combine with CIP funding to address eight of the worst roads spread geographically across the city;
- approved preliminary engineering reports for six street enhancement projects and one bicycle infrastructure project;
- funded an additional street enhancement project in the Britton Old Town district; and
- awarded one new trail project to create a linkage from the Bert Cooper Trail to the Bluff Creek Trail, running along the west side of the Hefner Lake Dam.

STREET RESURFACING

PROJECTS MAY INCLUDE:

- pavement repair
- milling
- asphalt overlay
- concrete crack sealing
- intersection/traffic signal upgrades
- wheelchair ramps
- minor drainage improvements
- signage
- pavement markings

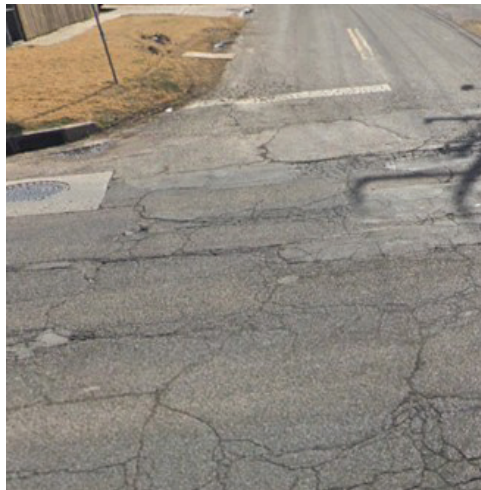
BENEFITS WE WILL GAIN:

- reduced annual maintenance costs
- improved pavement condition index
- reduced vehicle operating costs
- improved ride quality

\$168 million of the total program funding is allocated to street resurfacing.



SW 119th Street - Before and After



SW 15th Street - Before and After

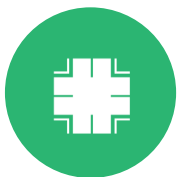




467,138

Tons of Asphalt

amount of
material used
to resurface



153

Intersection Improvements

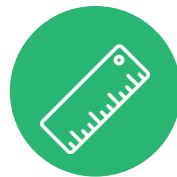
signals, signs
or crossings
improved



1,948

ADA Ramps

accessibility
ramps
installed



227,072

Linear Feet

curb and
gutter
replacement

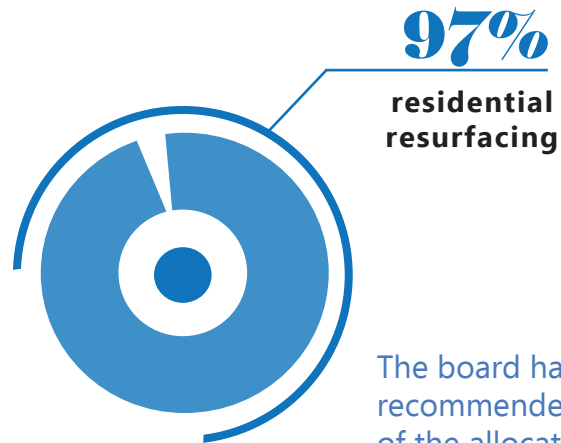
292
Total Miles
Resurfaced

*Four-Year Program Totals

RESIDENTIAL PROJECTS

residential project details:

- pavement removal
- base stabilization
- curb and gutter
- driveway approaches
- drainage improvements
- resurfacing
- striping



The board has recommended 97 percent of the allocated funding for residential resurfacing.



curb and
gutter

resurfacing

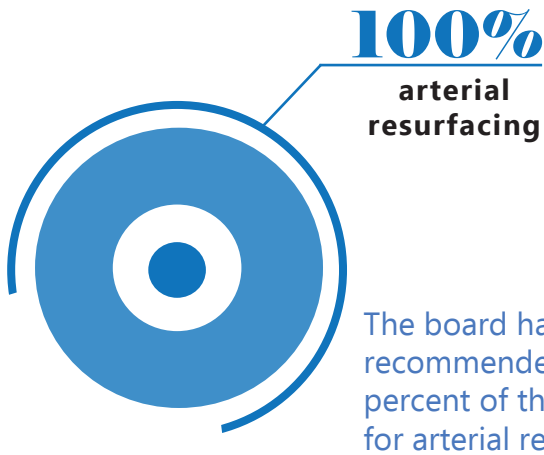


ARTERIAL PROJECTS



resurfacing
with bike lane

striping

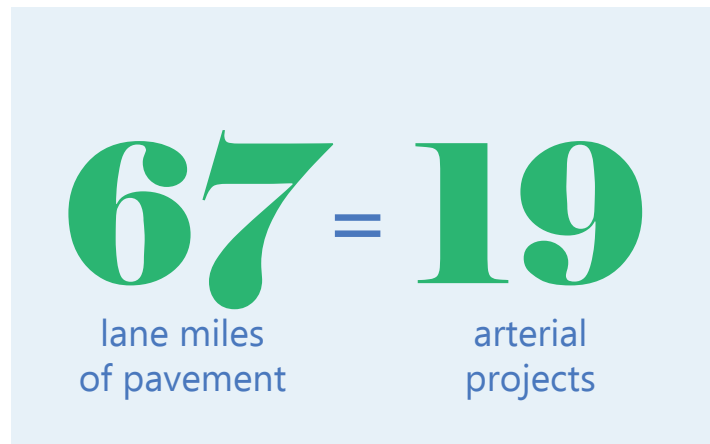
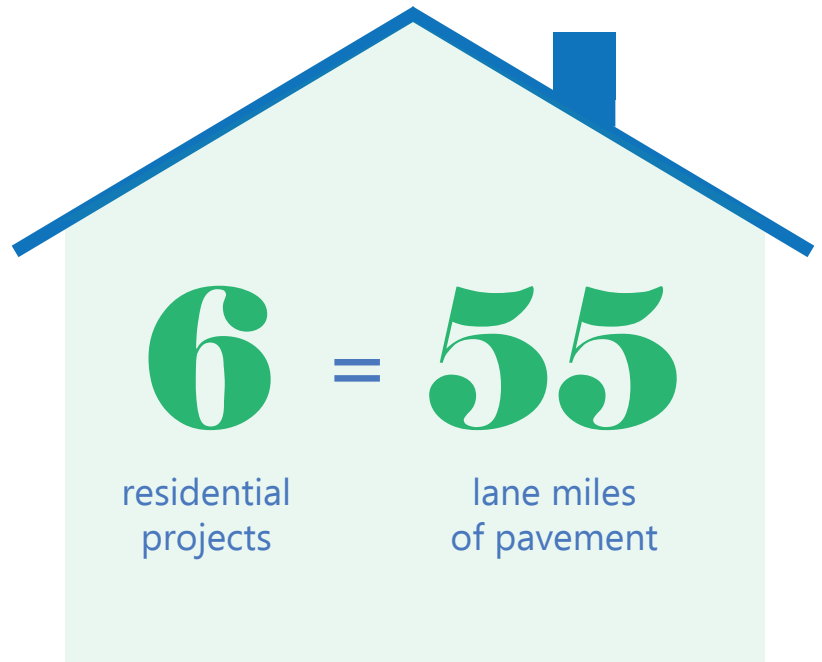


arterial project details:

- bridge replacement
- backfill and dirt work
- soil stabilization
- roadway base layer
- street resurfacing
- striping

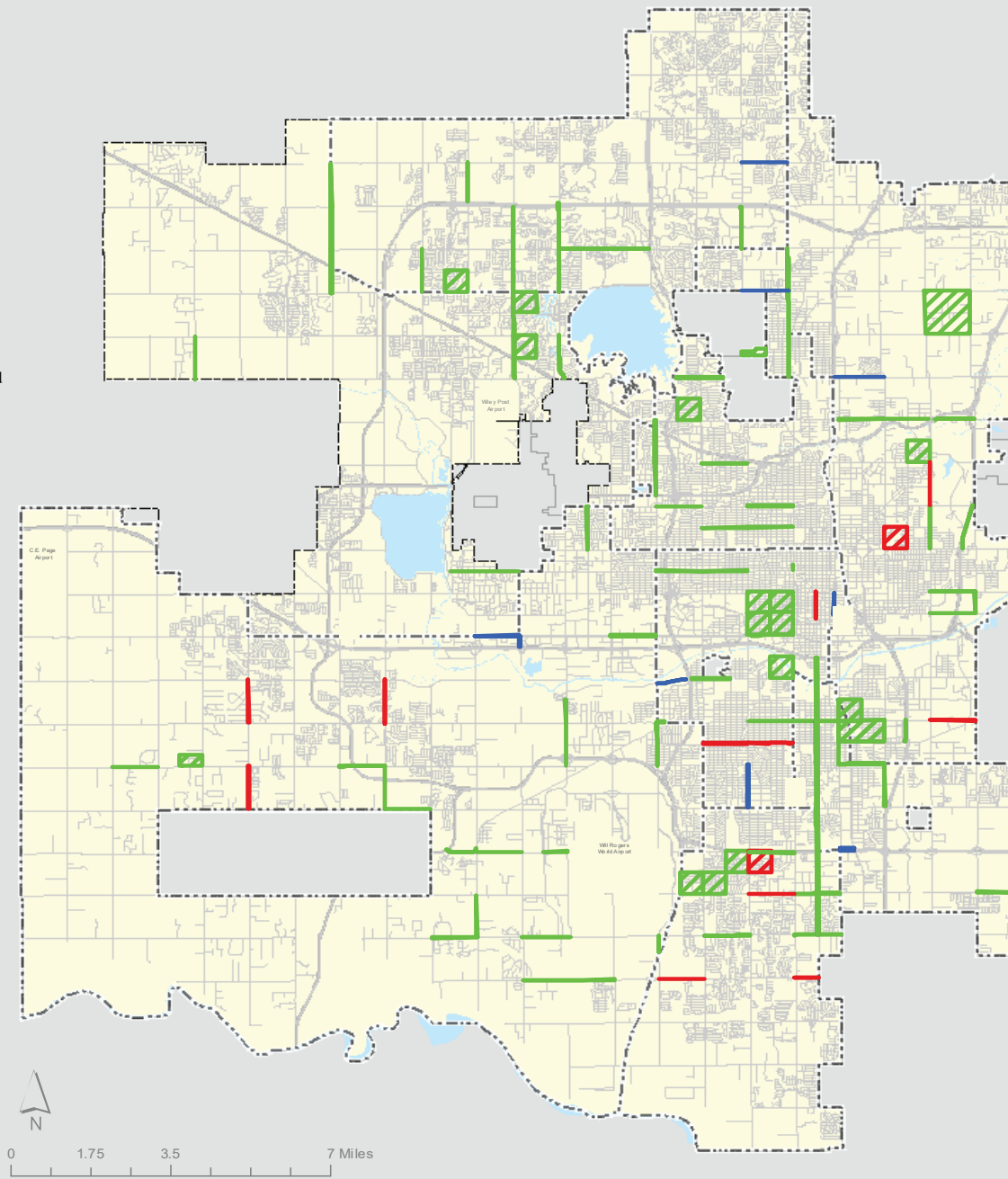


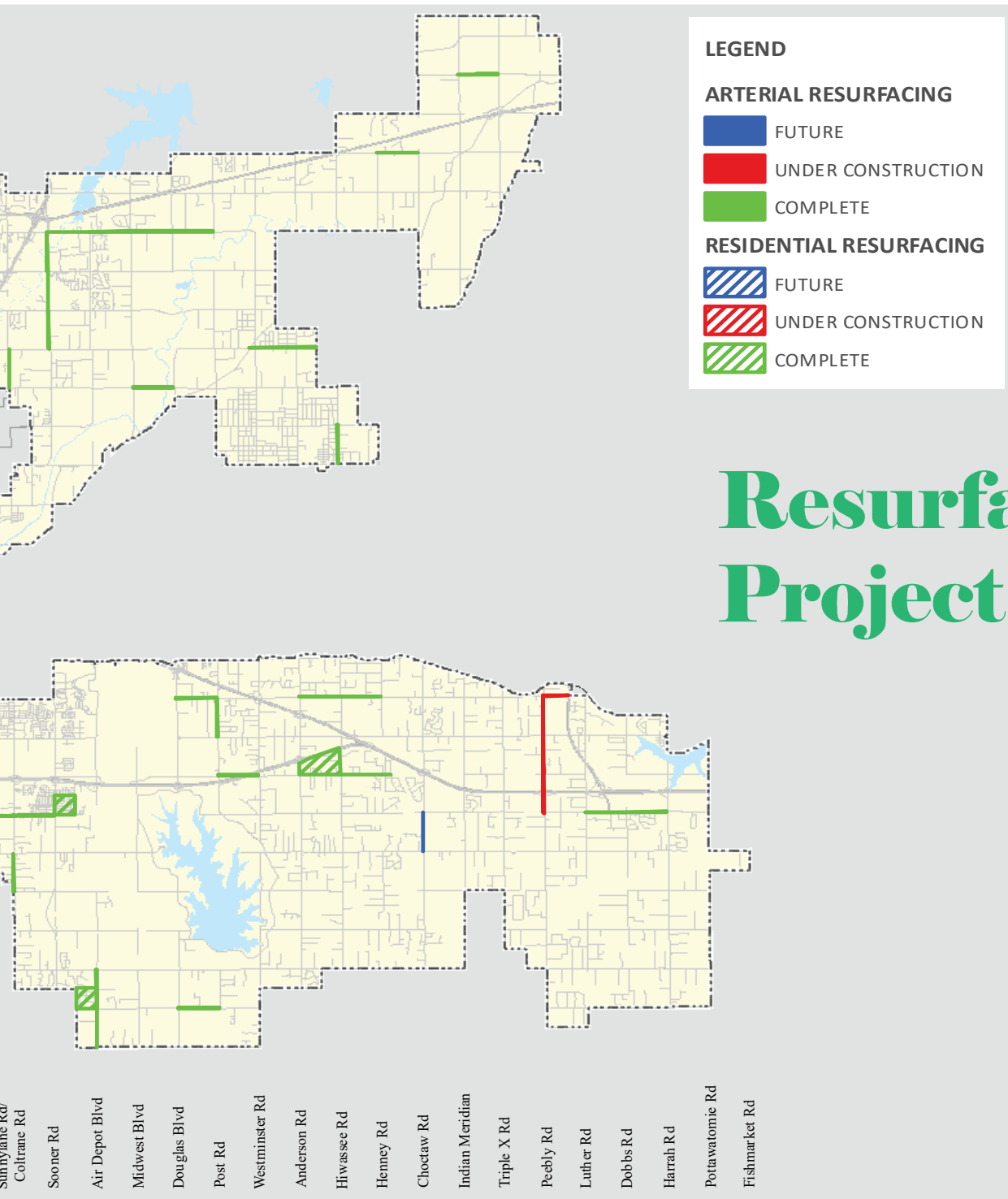
Resurfacing Project NE 56th Street



* 2021 Totals

N. 192nd St
 N. 178th St
 N. 164th St
 N. 150th St
 Memorial Rd
 N. 122nd St
 Hefner Rd
 Britton Rd
 Wilshire Blvd
 N. 63rd St
 N. 50th St
 N. 36th St
 N. 23rd St
 N. 10th St
 Reno Ave
 S. 15th St
 S. 29th St
 S. 44th St
 S. 59th St
 S. 74th St
 S. 89th St
 S. 104th St
 S. 119th St
 S. 134th St
 S. 149th St
 S. 164th St
 S. 179th St





Resurfacing Project Status

STREET ENHANCEMENTS

PROJECTS MAY INCLUDE:

- resurfacing and restriping of streets
- traffic calming such as on-street parking and curb bump-outs
- sidewalks and other pedestrian improvements
- bicycle infrastructure and bike racks
- trees and landscaping (with maintenance partnership)
- other amenities like gateway markers, benches, and public art

BENEFITS WE WILL GAIN:

- increased visitation, population and employment
- revitalization and economic development
- improved connectivity and safety

\$30 million of the total program funding is allocated to street enhancements.

Project Status

In Design

Auto Alley



Paseo District



Walker Ave



N Portland Ave



NW 10th: Rockwell to Council



Plaza District



NE 23rd Street



Stockyards Area



Meridian: NW 16th to NW 26th Street



In Construction

NW 39th Street



Complete

NW 10th and Vermont



Additional project details can be found at: okc.gov/BetterSafer

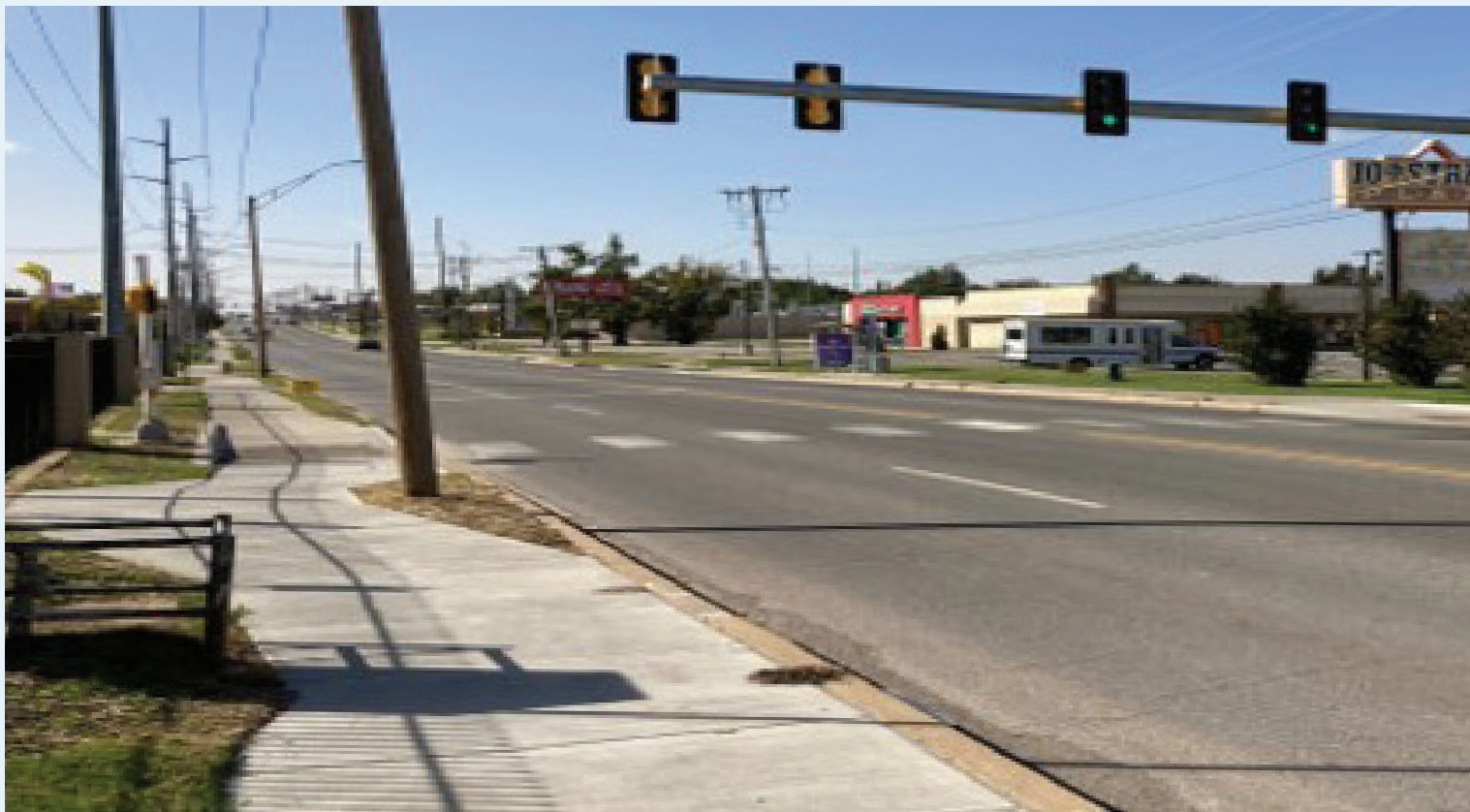
NW 39th Street

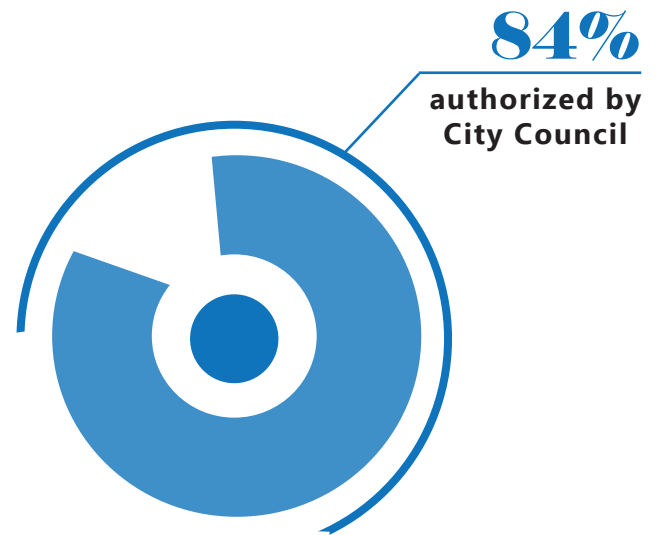


STREET ENHANCEMENTS

Project Highlight - NW 10th and Vermont

enhancement project details: new sidewalk and mid-block crossing





The board has recommended, and the City Council has authorized, eighty-four percent of the total allocated program funding for street enhancement projects.

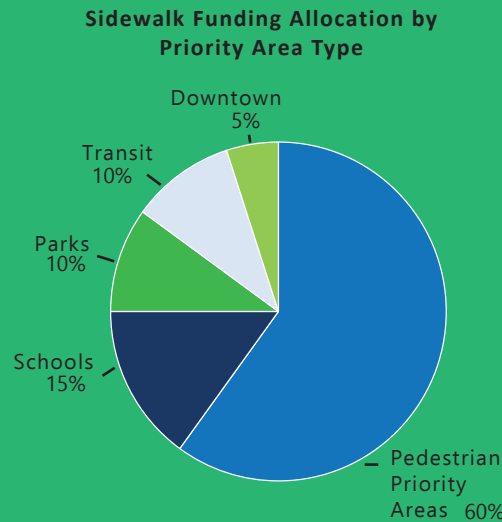
SIDEWALKS

PROJECTS MAY INCLUDE:

- sidewalk construction, ADA ramps
- crosswalks, bus stops, and/or
- pedestrian signals

BENEFITS WE WILL GAIN:

- more opportunities for people to walk to schools, parks, work, and shops
- a better connected sidewalk network
- improved pedestrian safety



Priority for sidewalk projects was determined using the following criteria:

- Responsive populations
- Connectivity to schools and parks
- Neighborhood revitalization
- Urban commercial districts



School Connections

29

amount of schools with new connections



Park Connections

17

sidewalks installed to connect Parks



Bus Stops

66

connections for travelers

*Four-Year Program Totals

Project Highlight sidewalk project details: replace broken sidewalk to create ADA-compliant connectivity throughout downtown

NW 10th: Hudson to Harvey



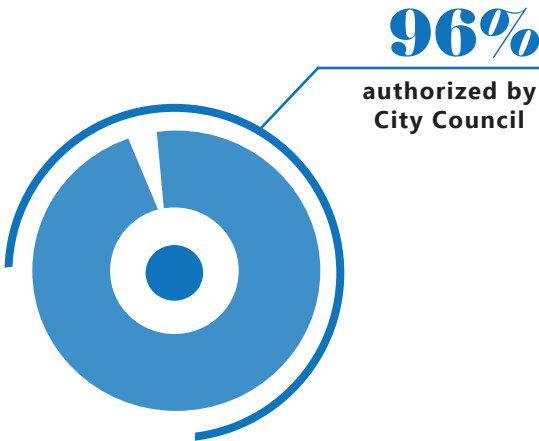
Before



After - looking west



After - looking east



**\$25 million
of the total
program funding
is allocated to
sidewalks.**

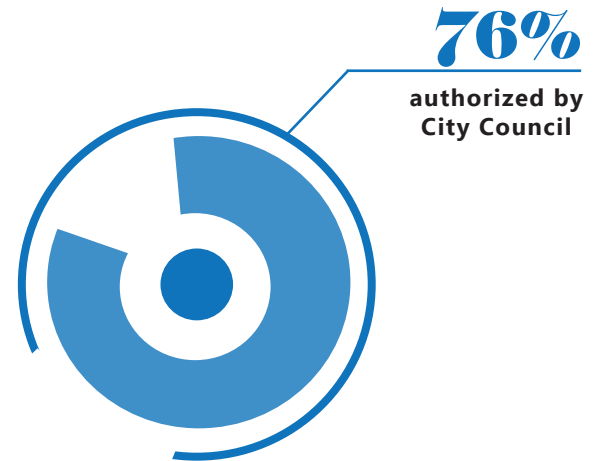
BICYCLE INFRASTRUCTURE

PROJECTS MAY INCLUDE:

- bike lanes
- protected bike lanes
- off-street paths

BENEFITS WE WILL GAIN:

- more opportunities for people to work, shop, and recreate without a car
- improved safety for cyclists
- a well-connected network



Project Highlight - Main Street



Main St - Lee to Classen



Project Status

In Design

Villa North



Grand/Independence



South 59th Street



South 25th Street



Indiana/Linwood



Springlake

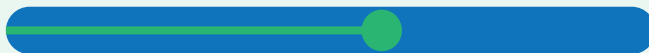


Lottie



Construction

General Pershing



Complete

Shartel to Deep Fork Creek



**\$14 million of
the total program
funding will
be spent to
update bicycle
infrastructure.**



**Bicycle
Delineators**

3,337

channelized markers
installed

*Four-Year Program Totals

TRAILS

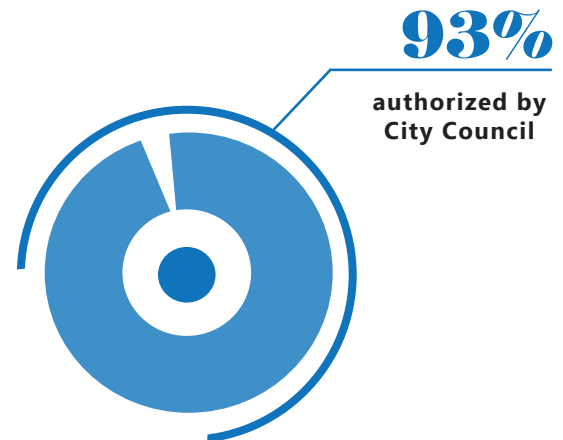
PROJECTS MAY INCLUDE:

- resurfacing and improving existing trails
- wayfinding signage
- new trails

BENEFITS WE WILL GAIN:

- expansion of trail network to new areas
- connection of existing trails
- enhanced existing trails with new amenities

future signage



**\$20 million of
the total program
funding is allocated
to trails.**

Project Status

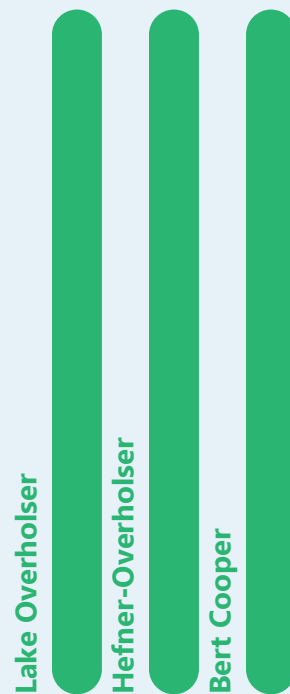
New Trails —



Resurfacing and Signage —



Complete —



$$\begin{array}{ccc}
 \text{---} & \text{---} & \text{---} \\
 4 & = & 22 \\
 \text{projects} & & \text{miles of trails} \\
 \text{---} & \text{---} & \text{---}
 \end{array}$$

Project Highlight - Bert Cooper Trail

trail project details: resurfaced trail and striping



Additional project details can be found at: okc.gov/BetterSafer

Project Highlight - Overholser Trail



2021

YEAR IN REVIEW

28

**Miles of
Sidewalk**
amount of
sidewalks
constructed

5

**Miles of
Bike Lanes**
amount of
bicycle
infrastructure

140

**Miles
Resurfaced**
amount of
streets
resurfaced

22

**Miles of
Trails**
amount of
trails
resurfaced



PROJECT MANAGERS

Eight project managers supervise all of the resurfacing, enhancement, sidewalk, bicycle infrastructure and trail construction.



PROJECT IDEAS

The board continues to identify projects for funding with recommendations to the Council for highest priority areas.



IMPLEMENTATION PLAN

The implementation plan is revised throughout the year. The most recent update is posted to okc.gov/BetterSafer



[OKC.GOV/BETTERSAFER](https://okc.gov/bettersafer)