

Planning Commission Minutes

March 10, 2022

(The agenda was filed with the City Clerk of The City of Oklahoma City at 7:24 a.m. on March 7, 2022.)

7. (PUD-1884) Application by M and A Family, LLC., to rezone 8113 Sunnyslane Road from the PUD-1810 District to the PUD-1884 Planned Unit Development District. Ward 4.

The applicant was present. There were no protestors present.

Amended Technical Evaluation:

1. Outdoor storage is prohibited.
2. EMD signage is prohibited.

RECOMMENDED APPROVAL SUBJECT TO TECHNICAL EVALUATION, AS AMENDED.

MOVED BY PRIVETT, SECONDED BY HINKLE

AYES: CRAVENS, CLAIR, POWERS, PRIVETT, HINKLE,
HIGHSMITH, PENNINGTON

ABSENT: LAFORGE



STAFF REPORT
The City of Oklahoma City
Planning Commission
March 10, 2022

Item No. IV. 7.

(PUD-1884) Application by M and A Family, LLC., to rezone 8113 Sunnylane Road from the PUD-1810 District to the PUD-1884 Planned Unit Development District. Ward 4.

I. GENERAL INFORMATION

A. Contacts

Applicant/Developer Representative

Name Mark Zitzow
Company Johnson And Associates
Phone 405-235-8075
Email mzitzow@jaokc.com

B. Case History

This is a new application.

C. Reason for Request

This application is to permit a mixed residential and commercial development.

D. Existing Conditions

1. Comprehensive Plan Land Use Typology Area: Urban – Low Intensity (UL)

UL applies to the least intensively developed areas of the city that still receive urban water, sewer, police, park and fire services. Development in this area should provide horizontal integration of land uses, connectivity within and between individual developments, and design that facilitates pedestrian and bicycle transportation.

2. Size of Site: 8.89 acres

3. Zoning and Land Use

	Subject Site	North	East	South	West
Zoning	PUD-1810	I-1/I-2	PUD-942/R-1	PUD-1810	PUD-1810/I-2
Land Use	Undeveloped	Industrial	Residential	Oilwell	SprtFlds

4. Development Context: The subject site is located on the west side of S Sunnylane Road, south of the I-240 Service Road. The proposed development site is part of the larger PUD-1810 that was approved in April 2021. The subject site is currently zoned Tract 3 of the PUD which allows residential development per the R-1 District.

The new PUD would rezone 8.9 acres of Tract 3 a new PUD that would allow commercial and light industrial development along S Sunnylane Road. The remainder of Tract 3 is located to the west and planned as a residential subdivision (Preliminary Plat of Sunnyvale Estates, C-7417). Also adjacent on the west and taking access from the I-240 Service Road is the 240 Sport Complex. To the north is Tract 1 of PUD-1810 which abuts the the sport complex's east side and allows commercial and light industrial uses. The corner is zoned I-1 and I-2 and developed with a hotel and gas stations. To the south is the planned shared entrance to Sunnyvale Estates, and Tract 2 of PUD-1810 which allows commercial uses. Across the street to the east is a residential subdivision (Sunny Pointe).

II. SUMMARY OF PUD APPLICATION

The use and development regulations of the **C-3, "Community Commercial" District** shall govern this PUD except as herein modified below:

The following uses shall be the only uses allowed within this Tract:

- Dwelling Units and Mixed Use (8200.2)
- Live/Work Units (8200.4)
- Low Impact Institutional: Neighborhood Related (8250.14)
- Murals (8250.16)
- Community Recreation: General (8250.2)
- Community Recreation: General (8250.3)
- Community Recreation: Restricted (8250.4)
- Cultural Exhibits (8250.5)
- Administrative and Professional Offices (8300.1)Automotive: Parking Garages (8300.12)
- Automotive: Parking Lots, as a Principal Use (8300.13)
- Building Maintenance Services (8300.23)
- Business Support Services (8300.24)
- Child Care Centers (8300.25)
- Convenience Sales and Personal Services (8300.32)
- Eating Establishment: Fast Food (8300.35)
- Eating Establishment: Fast Food with Drive-Thru Order Window (8300.36) Required distance of 150 feet from developed single family residential
- Eating Establishment: Sitdown (8300.37)
- Eating Establishment: Sitdown, Alcohol Permitted (8300.38)
- Eating Establishment: Sitdown, limited Alcohol Permitted (8300.39)
- Food and Beverage Retail Sales (8300.41)
- Funeral and Interment Services: Undertaking (8300.44)
- Alcoholic Beverage Retail Sales (8300.5)
- Medical Services: General (830.52)

- Medical Services: Restricted (8300.53)
- Participant Recreation and Entertainment: Indoor (8300.55)
- Personal Services: General (8300.58)
- Personal Services: Restricted (8300.59)
- Personal Storage (8300.6)
- Repair Services: Consumer (8300.61)
- Retail Sales and Services: General (8300.63)
- Research and Development (8350.1)
- Research Services: Restricted (8300.62)
- Animal Sales and Services: Grooming (8300.8)
- Animal Sales and Services: Kennel and Veterinary, Restricted (8300.11)
- Wholesaling, Storage and Distribution: Restricted (8350.16)
- Custom Manufacturing (8350.3)
- Industrial, Light (8350.8)

All other conditional, accessory, special or special exception uses not specifically prohibited shall be permitted subject to the review and approval processes specified in the Zoning Code.

9.0 SPECIAL CONDITIONS:

The following special conditions shall be made a part of this PUD:

9.1 ARCHITECTURAL REGULATIONS

Exterior building wall finish on all structures, exclusive of windows and doors, shall consist of a minimum 70% brick, brick veneer, rock, concrete, stucco, concrete-board, architectural metal or stone masonry. No more than 30% EIFS or wood shall be permitted.

Industrial uses and personal storage shall be permitted the use of coated metal buildings.

9.2 LANDSCAPING REGULATIONS

The subject site shall meet all requirements of the City of Oklahoma City's Landscaping Ordinance in place at the time of development, except as otherwise noted.

A 15-foot landscape buffer with evergreen plantings on 20-foot centers shall be required for commercial / industrial uses when adjacent to single-family residential uses.

9.3 LIGHTING REGULATIONS

The site lighting in this PUD shall be in accordance with Chapter 59, Article XII, Section 59-12350 of the Oklahoma City Municipal Code, 2020, as amended.

To minimize light spillover on residential uses, outdoor lights within the development will be directed away from any adjacent residential properties. To accomplish this, lights shall utilize shields, shades, or other appropriate methods of directing light beams.

9.4 SCREENING REGULATIONS

No less than a six-foot and no greater than an eight-foot-high sight proof screen shall be required along boundaries adjacent to residential uses. Said sight-proof screen may be constructed of stucco, brick, stone, wood, wrought iron and/or any combination thereof and shall be solid and opaque.

9.5 DUMPSTER REGULATIONS

Dumpsters shall be consolidated where practical and located within an area screened by a fence or masonry wall of sufficient height that screens the dumpster from the view of primary vehicle and pedestrian circulation systems. Said enclosure shall be a minimum of 50 feet from any residential use (if developed at the start of construction for this project).

9.6 DRAINAGE REGULATIONS

Development of this parcel will comply with Chapter 16 of the Oklahoma City Municipal Code, 2020, as amended.

9.7 VEHICULAR ACCESS REGULATIONS

Vehicular access into this PUD shall be via a maximum of two (2) private drives from S Sunnyslane Rd. and one (1) access point from the private street to the south.

9.8 PEDESTRIAN ACCESS/SIDEWALK REGULATIONS

Five-foot wide sidewalks shall be constructed along S Sunnyslane Rd. Said sidewalks shall be 6-foot wide if the sidewalk is constructed adjacent to the curb, subject to the policies/procedures of the Public Works Department. All sidewalks shall be in place prior to any occupancy certificates being issued.

9.9 PARKING REGULATIONS

The number and design of all parking facilities in this PUD shall be in accordance with Section 59, Article X of the Oklahoma City Municipal Code, 2020, as amended.

If development within this PUD utilizes the “Commercial District” scheme of development where parking is screened behind or to the sides of structures, the parking requirement for said development shall be reduced by 15%. Parking can be met through shared parking agreements among adjacent office/retail uses.

Proposed uses are not required to have off-street parking located on the same site as the structure. Off street parking may be provided by parking lots or garages located adjacent to the structure it serves.

If on-site parking is not provided for a proposed use/structure, locations and parking calculations for all structures in this PUD shall be provided with the building permit application.

Pervious paving may be used for parking areas, driveways, pathways, and plazas subject to Public Works review and approval. Where connected to public rights-of-way the access drive apron will be constructed of hard surface paving meeting City code for driveway construction. A hard surface border is required around the perimeter of pervious paving areas. Maintenance of the pervious paving is required, and a maintenance plan must be provided when construction documents are submitted for permitting.

9.10 SIGNAGE REGULATIONS

9.10.1 Freestanding Accessory Signs

Freestanding signs shall be in accordance with the C-3 “Community Commercial” district regulations.

9.10.2 Attached Signage

Attached signs shall be in accordance with the C-3 “Community Commercial” district regulations with the following exceptions:

Square footage of signs for individual uses located in a multi-story, multi-tenant structure (stacked uses) shall be calculated based upon the frontage and setback of the individual use requesting the sign, regardless of the floor on which the use is located. (See Municipal Code Section 3-102(c) 3). Frontage is considered that façade(s) where the sign is to be located.

9.10.3 Non-Accessory Signs

Non-accessory signage shall not be permitted within this development.

9.10.4 Electronic Message Display Signs

Electronic Message Display Signs shall be in accordance with the C-3 “Community Commercial” district regulations.

9.10.5 Decorative Artwork, Architectural Elements and Temporary Signage

Decorative artwork and/or architectural structures shall be permitted in this PUD, including, without limitation, upon or across public right-of-way with applications for review for such improvements submitted to the Arts Liaison in the Office of Arts & Cultural Affairs and subject to recommendation by the Oklahoma City Arts Commission and approval by the Public Works Department and require issuance of a permit. If located on upon or across public right-of-way shall also require approval by the Oklahoma City Council.

Decorative artwork, architectural structures may include but are not limited to fountains, individual art pieces, clock towers, decorative entry structures, etc.

Decorative artwork/architectural structures shall not exceed twenty-five (25) feet in height, must have a minimum fourteen (14) feet clearance in height (for archways), and be located a minimum of twenty-five (25) feet from the curb of an adjacent public street.

9.11 ROOFING REGULATIONS

All structures in this PUD shall have Class C roofing or better.

9.12 SETBACK REGULATIONS

Setbacks shall be per the base zoning district.

9.13 HEIGHT REGULATIONS

The maximum building height for structures within this PUD shall be per the C-3, “Community Commercial” District Regulations.

9.14 LOT COVERAGE

Maximum lot coverage shall be per C-3, “Community Commercial” District regulations.

9.15 PUBLIC IMPROVEMENTS

The Developer shall make public improvements throughout the PUD as may be required by The City of Oklahoma City Public Works Department or other City, County, or State Department or agency. All Local, State, and Federal ordinances as they shall apply to the site will be adhered to fully.

9.16 COMMON AREAS

Maintenance of the common areas in the development shall be the responsibility of the developer. No structures, storage of material, grading, fill, or other obstructions, including fences, either temporary or permanent, that shall cause a blockage of flow or an adverse effect on the functioning of the storm water facility, shall be placed within the common areas intended for the use of conveyance of storm water, and/or drainage easements shown.

9.17 SPECIFIC PLAN, PLATTING & PERMITTING REQUIREMENTS

No building permits shall be issued in within this PUD until a specific plan, including all items listed in Section 59-14200 of the Oklahoma City Municipal Code, 2020 as amended shall have been approved by the Planning Commission.

There shall be no platting requirements within this PUD.

9.18 GENERAL DESIGN AND DEVELOPMENT GUIDELINES

Specific information and plans for General Design and Development Guidelines are contained in Chapter 59, Section 14200.4, for density, amenities, relationship to abutting uses, site design, safety, and circulation systems.

10.0 DEVELOPMENT SEQUENCE:

Developmental phasing shall be allowed as a part of the development of this PUD. Required traffic improvements shall be permitted to be constructed in conjunction with the phased development of this PUD, as approved by the City’s Traffic Division.

11.0 EXHIBITS:

The following exhibits are hereby attached and incorporated into this PUD. These exhibits are:

- A: Legal Description
- B: Conceptual Site Plan

III. REVIEW COMMENTS

This application was submitted to the following agencies, departments, and/or divisions for review and comment. An asterisk indicates that the agency, department, and/or division responded with no adverse comments.

A. Outside Agencies

- 1. Oklahoma City-County Health Department**
- 2. Oklahoma City Urban Renewal Authority (OCURA)**
- 3. Oklahoma Gas and Electric (OGE)**
- 4. Oklahoma Natural Gas:** Please be advised ONG has an underground or aboveground facility in the area. If you need our facilities relocated to accommodate your excavation or construction, ONG will provide you a cost estimate. Prior to excavation, please contact OKIE-ONE 1-800-522-6543 a minimum of 48 hours prior for exact location of our facilities.
- 5. Oklahoma Water Resources Board (OWRB)**
- 6. School District(s) Mid-Del**
- 7. Oklahoma Department of Transportation (ODOT)**

B. City Departments

- 1. Airports**
- 2. Central Oklahoma Transportation and Parking Authority (COTPA)**
- 3. Fire:** No objection, but a turnaround will be required for building permit for the west end of parking lot.
- 4. Information Technology/Geographic Support**

5. Parks and Recreation

6. Police

7. Public Works

a. Engineering

b. Streets, Traffic and Drainage Maintenance

Storm Sewer Availability

- 1) The Public Works Drainage Division staff has reviewed the subject application. All development, new construction, and/or substantial improvements planned within the proposed area shall be subject to chapter 16 of the Oklahoma City Municipal Code.
- 2) Storm sewers in accordance with the City's Drainage Ordinance will be required. Development abutting section line roads with drainage flows that exceed the capacity of OKC standard ditch detail (D-100) will necessitate the installation of enclosed storm sewer and/or concrete channel. These improvements shall be placed to provide a minimum of 35' clearance distance from the centerline of the section. Concrete channels must be entirely outside public right-of-way.
- 3) A flood and/or drainage study will be required to establish finished floor elevations, common lot areas or private drainage easements. A final plat should not be submitted until the study has been reviewed and approved by the Public Works Department.
- 4) Flood Study will be required to show no rise in FEMA Q 100 and the City of Oklahoma City Q 100 water surface elevation, compared pre-and post-development.
- 5) A portion of subject property is situated within a F.E.M.A. Flood Plain. Therefore, the establishment of Minimum Finish Floor Elevation for each lot within the flood plain will be required (100-year) frequency plus 1'. The Public Works Department Engineering staff shall approve these elevations prior to the filing of the final plat.
- 6) A floodplain activity permit must be submitted for any work contemplated in The Waters of the United States.

- 7) A Corps of Engineers 404 permit must be submitted with plans for any work contemplated within the floodplain, creek, or channel (waters of the United States) included in the case limits.
- 8) Plans for drainage improvements within the private drainage reserves and/or common areas must be submitted for review, and payment of inspection fees shall be made prior to construction. Building permits will not be issued until construction is complete. If a subdivision abuts a stream, the private drainage easements and/or common area along stream shall extend to flow line of stream at a minimum.
- 9) Place the following note on the plat and construction plans: Maintenance of the Common Areas and/or Private Drainage Easements shall be the responsibility of the property owner. No structures, storage of material, grading, fill or other obstructions, including fences, either temporary or permanent shall be placed within the common areas and/or drainage easements shown.
- 10) Drainage easements shall be clearly denoted as public or private in owner's dedication, on the plat, and/or in the plat notes.
- 11) Construction within the limits of this SPUD will require an erosion control plan in accordance with EPA Storm Water Discharge permitting: CFR Vol. 57, No. 175, September 9, 1992. A copy of the EPA Notice of Intent (NOI) will be required prior to the issuance of work orders or permits for construction activities disturbing an area of 1/2 acre or greater.
- 12) Sidewalk shall be installed for all new construction and/or at the time of a residential use to a more intense use on all lots having frontage on public streets classified as a major or minor arterial.
- 13) All private roads /streets will have private storm sewer systems.
- 14) Engineer/developers will be contacting PW for detention determination before they submit their final plat and plans.

c. Stormwater Quality Management

d. Traffic Management

- 1) Driveway access to Sunnyslane Rd must meet driveway separation as stated in the Subdivision Regulation.

8. Utilities

a. Engineering

Paving

Wastewater Availability

- 1) An existing 12” wastewater main(s) is adjacent to the subject site(s) and a service connection will be permitted in accordance with City Standard Specifications. A separate service connection is required for each building/structure.
- 2) All wastewater mains must flow within the existing drainage basin. Proposed wastewater mains that will flow to another drainage basin across a ridgeline will not be allowed.
- 3) Minimum ten (10) foot horizontal separation required for water mains and five (5) foot for other crossings. Minimum two (2) foot vertical separation required for all crossings.
- 4) The developer will be required to extend the wastewater collection system to each lot or site within the subdivision or land improvements in accordance with City Standard Specifications and the current Subdivision Regulations.
- 5) All existing unused wastewater services must be abandoned at the wastewater main.
- 6) Proposed wastewater main(s) must be located in a twenty (20) foot wide easement.
- 7) No trees, signs, dumpster, fences, and/or structures shall be permitted within any proposed or existing utility easement.
- 8) All wastewater mains must have a minimum cover of six (6) feet and a depth no greater than eighteen (18) feet.

b. Solid Waste Management

- 1) The City cannot provide service, contact private hauler.

c. Water/Wastewater Quality

Water Availability

- 1) An existing 16" water main(s) is adjacent to the subject site(s) and a service connection will be permitted in accordance with City Standard Specifications. A separate service connection and meter is required for each building/structure.
- 2) Minimum ten (10) foot horizontal separation required for wastewater mains and five (5) foot for other mains. Minimum two (2) foot vertical separation required for all crossings.
- 3) Dead-end water mains must be avoided where applicable.
- 4) All existing unnecessary water services must be abandoned at the water main.
- 5) In accordance with ODEQ regulations, the City provides water at a minimum pressure of 25 psi. The developer is responsible for designing and construction of all fixtures to provide adequate domestic and fire protection under minimum pressure conditions. The developer will be responsible for any failure of domestic and fire protection systems which require water pressure in excess of 25 psi.
- 6) Proposed water main(s) must be located within a utility easement or right-of-way. No trees, signs, dumpster, fences, and/or structures shall be permitted within any proposed or existing utility easement.
- 7) All water mains must have a minimum cover of five (5) feet along section line roads and four (4) feet elsewhere.
- 8) A 12-inch water main is required to be looped through the development to provide adequate water supply and fire flow protection. Connection to two (2) separate active water mains is required to prevent partial or total shut-off of water supply when a pipeline failure occurs within the proposed development.
- 9) All existing and proposed meters must meet current specifications and standard details and be located in the right-of-way or utility easement within grassy areas outside of sidewalks, driveways, streets and/or paving.
- 10) All domestic and fire suppression services must have separate water service connections.
- 11) Fire hydrants maximum spacing is 500 foot and must be located within 10 foot of a hard surface (i.e. sidewalk, street, and/or paving).

9. Planning

a. Comprehensive Plan Considerations

1) LUTA Development Policies:

Site Design:

- Avoid developing within or modification of 100-year floodplains or floodways.
- Where possible, provide a continuous system of open space along stream corridors or other appropriate areas that link neighborhoods and park lands.
- Utilize Best Management Practices (BMP) for stormwater whenever possible.
- Developments should be served by urban water and sewer utility systems.

Floodplain is present on the north half of the site. The conceptual plan places detention in this area. National, state, and local permitting require basic best management practices for stormwater management. Water and sewer are available.

Location:

- Locate uses that generate high traffic volumes along arterial streets, frontage roads, and interstates.

The subject site is located along an arterial street.

Density: The Urban Low Intensity LUTA outlines a floor to area ratio (FAR) of 0.2 to 1.0 for non-residential developments. *The PUD regulations could provide development within this range.*

Automobile Connectivity:

- Maintain, create, and enhance a street network that is highly connected to increase number of viable commercial locations and the effectiveness of the transportation system.
- Limit number of dead-end streets and cul-de-sacs.
- Limit curb cuts on arterial streets, and where possible concentrate access at shared entrance points.
- Development fronting arterials should take access from intersecting streets where possible.
- Primary entrance points should be aligned with access points immediately across the street.
- Retail developments should provide an internal vehicle and pedestrian circulation system between new and existing centers and individual stores.
- Provide vehicular connectivity between adjacent developments.

- Horizontally mixed-use developments should have connectivity between land uses.

The PUD indicates access would be from a future street to the south that will also serve the proposed Sunnyvale Estates subdivision, in conformance with the comprehensive plan. An additional two driveways are proposed along S Sunnyslane Road.

Pedestrian Connectivity:

- Provide sidewalk connections to adjacent development.
- Within parking lots, provide pedestrian access ways separated from vehicle aisles.
- Horizontally mixed-use developments should have public sidewalk connectivity between land uses.

Sidewalks are required along the arterial street and internal pedestrian circulation will be shown at the specific plan stage.

- 2) **Compatibility:** The comprehensive plan includes a land use compatibility matrix used to identify potential conflicts between major land use categories. When locating commercial and light industrial uses adjacent to existing residential uses or zoning, “Building Scale and Site Design”, “Traffic”, and “Operational Impact” are potential compatibility issues identified by the comprehensive plan.

Building Scale and Site Design: The comprehensive plan calls for mitigation measures for new projects if setbacks are significantly deeper or more shallow than the predominant character of adjacent development; the building’s orientation to the street differs from the predominant character of adjacent development; building size or street frontage dramatically differs from that of adjacent uses, the lot coverage (including buildings and paved areas) dramatically exceeds that of adjacent uses, or if buildings within 40 ft. of a single-family residential structure exceed a 3:1 ratio in height. *No triggers requiring mitigation were identified on the north or west where abutting the I-2 District. A 15-foot landscape buffer is required adjacent to the planned subdivision on the west and street trees will be required along Sunnyslane across from the neighborhood to the east.*

Traffic: Mitigation measures should be used if the development is inconsistent with the Street Typology, or would substantially exceed the average daily traffic of existing, adjacent uses. *No triggers requiring mitigation were identified.*

Operational Impact: The comprehensive plan calls for mitigation measures if new projects propose uses that generate noise levels above those found in typical residential settings, have detectable odors, spill light above that found in typical residential settings, or operate after 10 p.m. within proximity to residential uses. When located near residential uses, the comprehensive plan calls for containing operating effects (including noise and odor) within building walls, using vertical screening to block any mechanical equipment and service areas, and directing light away from adjacent residential areas. *Potential operational impacts are identified when placing commercial and light industrial uses next to residential uses or zoning. Mitigation offered in the PUD include eliminating gas station uses, requiring a 150-foot setback for drive-thru restaurants, and providing a 15-foot landscape buffer with evergreen plantings on 20-foot centers where commercial or industrial uses are adjacent to single-family residential uses.*

- 3) **Service Efficiency:**
 - Water: *Fully served*
 - Sewer: *Fully Served*
 - Fire Service: *Urban Service Level*
- 4) **Environmentally Sensitive Areas:** The following apply to the proposed development site:
 - Riparian areas: Riparian areas are vegetated areas adjacent to streams, lakes, ponds, and wetlands that are composed of a mixture of trees, shrubs, and grasses. These areas may also contain floodplain. The comprehensive plan recommends a 100-foot buffer from stream banks. *Riparian area including 100-year floodplain is present on the northern half of the subject site. Exhibit B envisions this area as a detention pond.*
 - Upland Forests: *N/A*
 - Vulnerable aquifers: *N/A*
- 5) **Transportation System:** This site is located off S Sunnyslane Rd, a Major Arterial Street in the Urban Low LUTA. The nearest transit (bus) service is located north of I-240 at SE 59th Street and Sunnyslane Rd.
- 6) **Other Development Related Policies**
 - Require sidewalks on both sides of all streets in urban LUTAs. (C-7)
 - Improve the functionality and efficiency of the street network by:
 - Providing direct connections from residential developments to nearby places and to each other.
 - Providing street and sidewalk stubs to adjacent vacant land in anticipation of future development.

- Connecting new development to existing street and sidewalk stubs, and to existing trail, open space, and bicycle networks.
- Reducing block sizes and use of dead-end streets.
- Maintaining the existing street grid to preserve connectivity and mobility options. (C-11)
- Limit driveways on arterials and collectors and increase connections between uses to improve safety and traffic efficiency. (C-30)
- Incorporate natural features (such as ponds, lakes, streams, rock outcroppings, stands of mature trees, and/or sizeable individual trees) into the design of all residential, commercial, and industrial projects rather than eliminating, hiding, or limiting access to those features. (E-31)
- Regional-, community-, and neighborhood-scale retail developments should provide an internal vehicle and pedestrian circulation system between new and existing centers and individual stores that draws on the following principles:
 - Concentrate access for new retail development at shared primary entrance points. Primary entrance points should be aligned with access points immediately across intersecting roads. Limit curb cuts on primary highways and arterials.
 - Provide pedestrian circulation, including sidewalks and median breaks along interior and exterior fronting roads and within parking lots.
 - Encourage coordinated development of retail centers in order to facilitate internal pedestrian and vehicle circulation and optimal center performance (SU-27)
- New neighborhood-scale retail should be located within new residential growth areas to serve daily shopping needs and limit trip distances. In newly developing areas, this retail format is preferred to the existing linear development patterns along arterials. (SU-30)
- Initiate new efforts to reduce sign clutter and improve the aesthetics of signs, while allowing for adequate and visible business identification (E-14)
- Encourage unified planning for all adjoining land owned or controlled by a project's developer to ensure proper circulation and land use relationships (SU-47)

b. Plan Conformance Considerations

The subject site is located on the west side of S Sunnyslane Road, south of the I-240 Service Road. The proposed development site is part of the larger PUD-1810 that was approved in April 2021. The subject site is currently zoned Tract 3 of the PUD which allows residential development per the R-1 District.

The new PUD would rezone 8.9 acres of Tract 3 a new PUD that would allow commercial and light industrial development along S Sunnyslane Road. The proposed Preliminary Plat of Sunnyvale Estates will provide a public street connection to the planned subdivision on the west. The new PUD would share access with the subdivision, in conformance with the comprehensive plan, and allow two new driveways within 650 feet of frontage. To mitigate for potential operational impacts next to the new subdivision, the PUD requires a 150-foot setback for drive-through uses and a 15-foot landscape buffer adjacent to the new lots.

IV. STAFF RECOMMENDATION

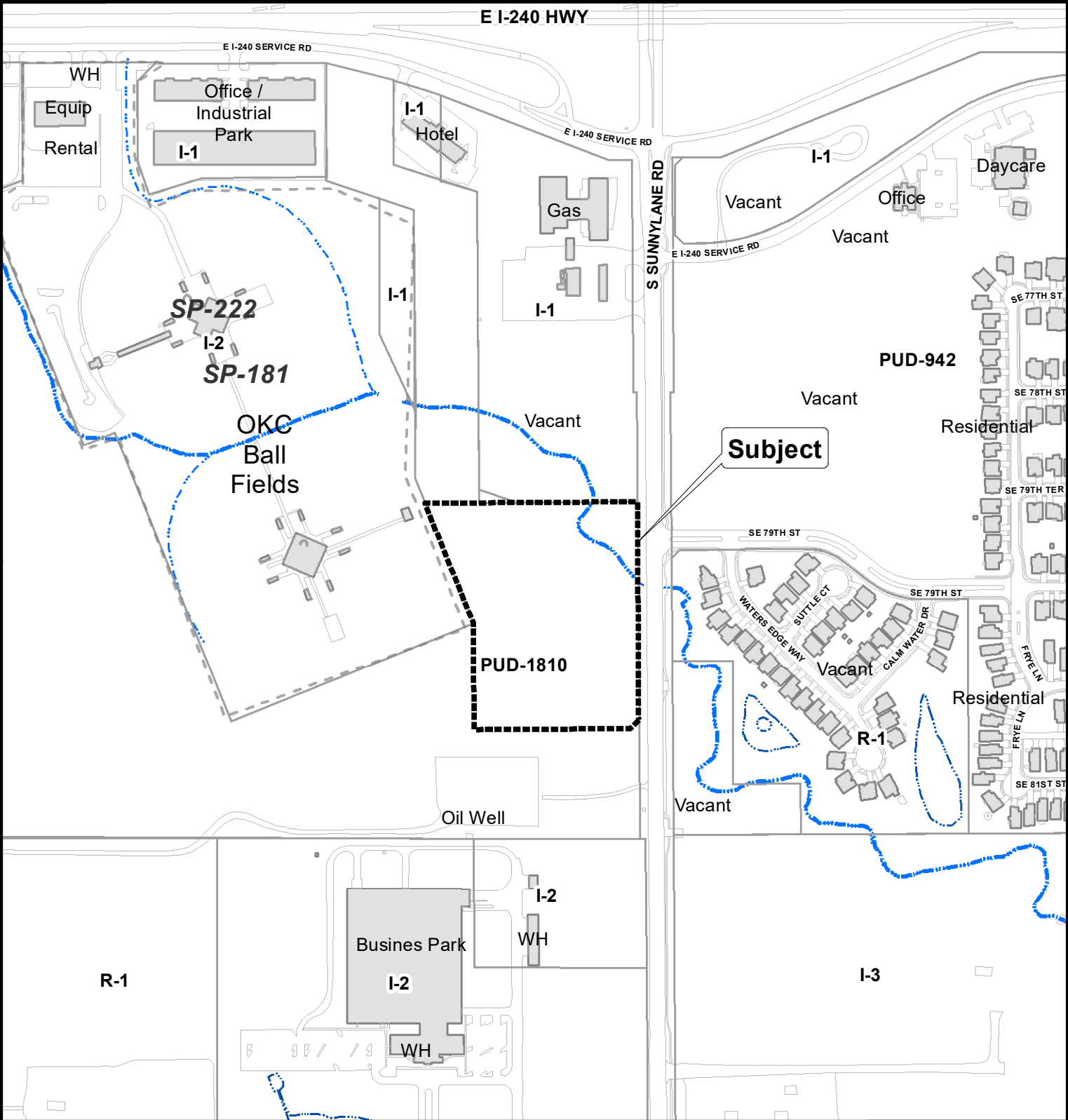
Staff recommendations are advisory only and do not constitute Planning Commission decisions. Staff recommendations are based on a technical evaluation of information submitted at the time of review. Determination of conformance with policies contained in the comprehensive plan is the purview of the Planning Commission. Planning Commission decisions require a vote of five members to approve or deny an item. The Planning Commission may consider in its decision-making process any additional relevant information presented at the public hearing by the public, applicant, and/or City departments.

Approval of the application.

All Engineering Division and Water/Wastewater Utilities Division requirements must be met. Additional changes to the SPUD may be required during either Divisions review of construction plans and prior to City Council approval.

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Case No: PUD-1884 Applicant: Mark W. Zitzow, Johnson & Associates
Existing Zoning: PUD-1810
Location: 8113 S. Sunnylane Rd.



The City of
OKLAHOMA CITY

Planned Unit Development



0 200 400
Feet

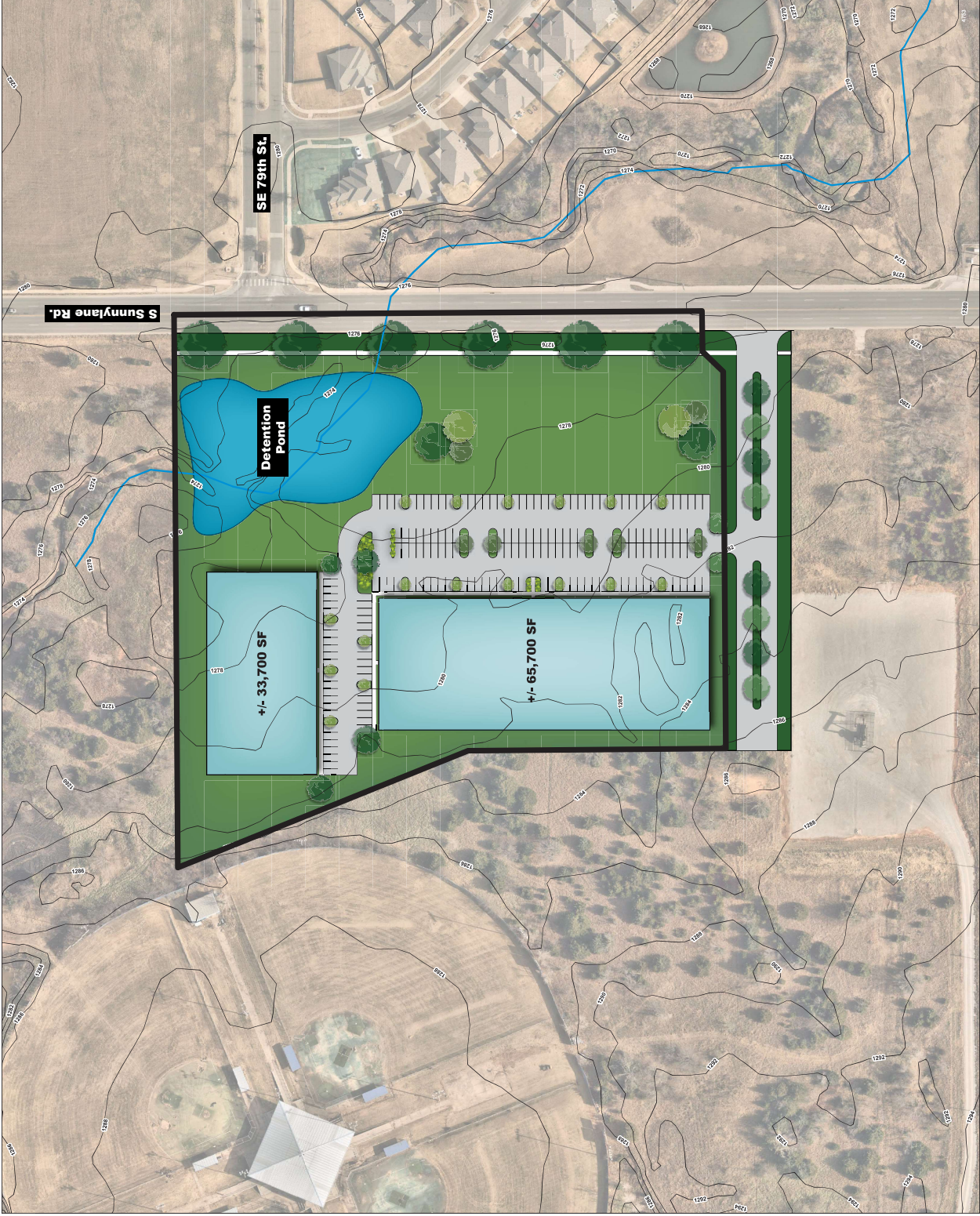


EXHIBIT B

PUD-1884
8113 S Sunnylane Rd.
Exhibit B
Conceptual Site Plan

Case No: PUD-1884 Applicant: Mark W. Zitzow, Johnson & Associates
Existing Zoning: PUD-1810
Location: 8113 S. Sunnylane Rd.



Aerial Photo from 2/2020



The City of
OKLAHOMA CITY

Planned Unit Development

