



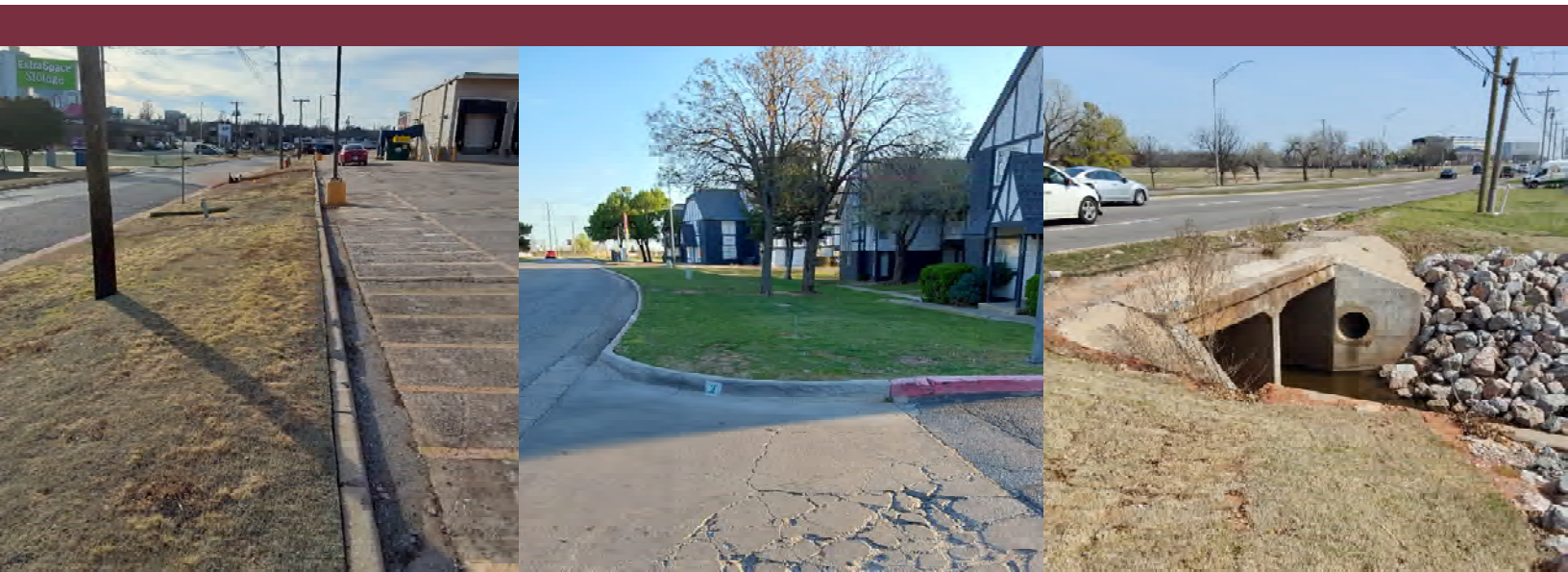
PROJECT M4-TS020

MAPS 4 Sidewalks and Related Amenities

Phase 2A

Preliminary Engineering Report

September 2024

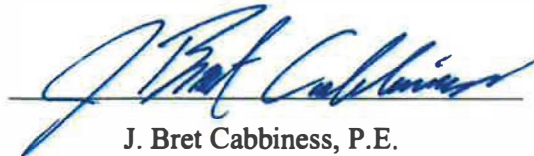


THE CITY OF OKLAHOMA CITY

APPROVAL SHEET


**PROJECT M4-TS020
MAPS 4 SIDEWALKS AND RELATED AMENITIES
PHASE 2A**


Prepared by Garver
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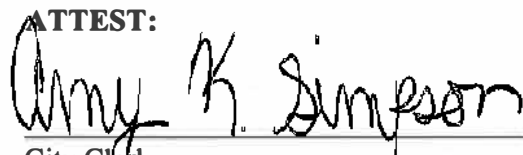
Recommended for Approval




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RECEIVED by the Council of the City of Oklahoma City this 10TH day of SEPTEMBER, 2024.

ATTEST:

Amy K. Simpson
City Clerk




David Holt
Mayor



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Appendix B	Preliminary Cost Estimates



2.0 Executive Summary

2.1 Project Description

The funding for the MAPS 4 Sidewalks and Related Amenities Phase 2A package of projects was approved as part of the MAPS 4 Implementation Plan on September 14, 2021 and revised in October 2022. The design contract for Phase 2 of the MAPS 4 Sidewalks package was approved by City Council on December 19, 2023.

The City of Oklahoma City (OKC) and the MAPS 4 Program (MAPS 4) desires to design and construct new sidewalks and related amenities for Phase 2 of the Pedestrian Plan of the overall BikeWalkOKC plan. The vision of the BikeWalkOKC plan is to create a fully walkable community allowing pedestrians to safely travel along a street and cross from one side to the other as well as provide connectivity between people and places. Phase 1 of the plan improved arterial streets by filling in gaps in the existing sidewalk network and improved pedestrian facilities at existing intersections. M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2 will now propose to connect neighborhoods to the Phase 1 improvements.

2.2 Scope of Work

Garver was tasked to develop preliminary sidewalk designs based upon field assessments of the M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A extents as outlined in the BikeWalkOKC Master Plan which are highlighted in green in **Figures 1, 2, and 3** on the subsequent pages. The collected data for all green segments and was evaluated against established criteria, including the Proposed Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG) and the BikeWalkOKC Master Plan. The evaluation aimed to identify the optimal sidewalk alignment, considering factors such as connectivity, compliance with the Americans with Disabilities Act (ADA), right-of-way and utility impacts, project amenities, and project costs.



Figure 1: Full Extent of Lyrewood Lane (West Area) per BikeWalkOKC Master Plan

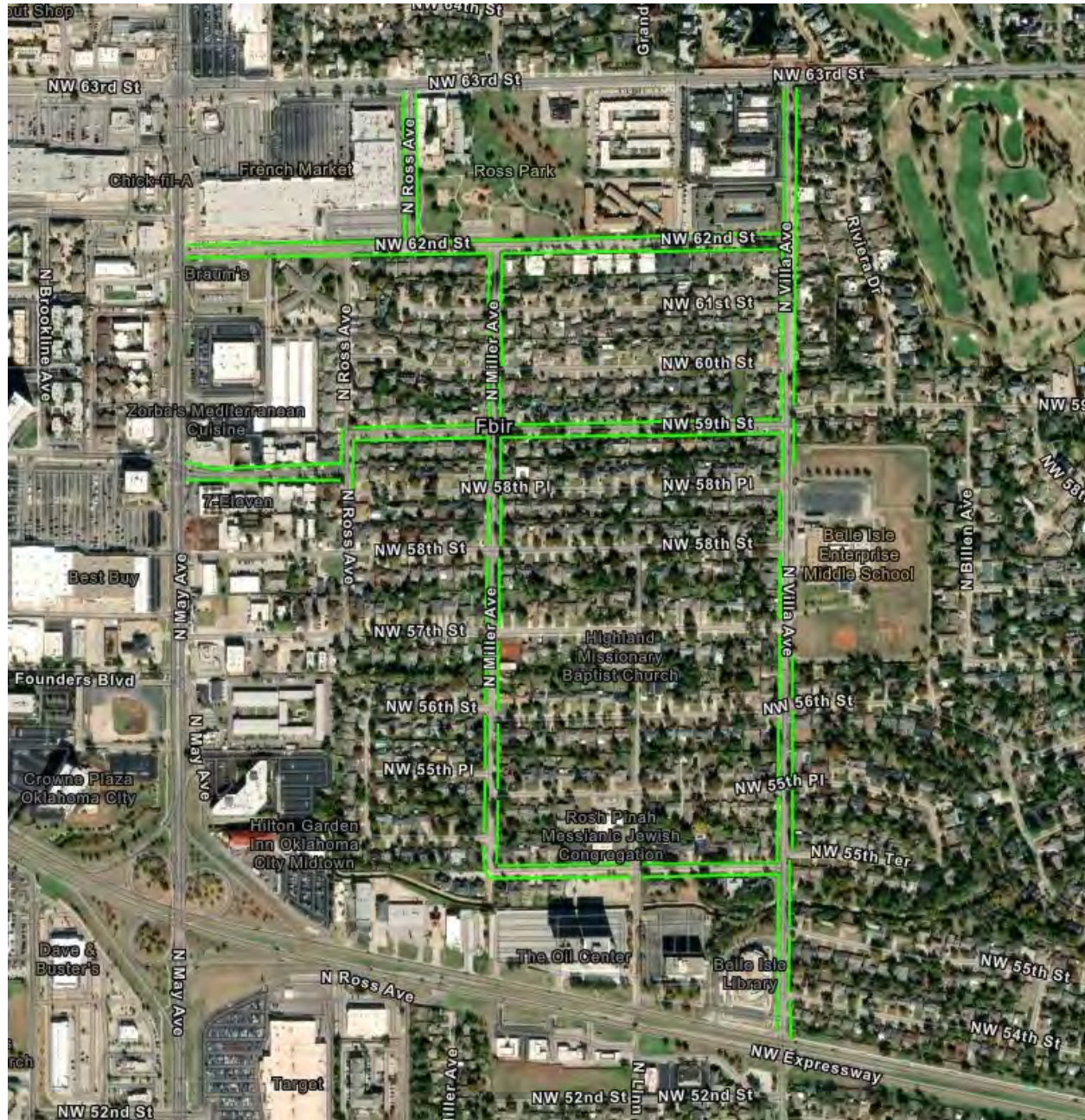


Figure 2: Full Extent of NW 63rd Street and May Avenue (East Area) per BikeWalkOKC Master Plan



Figure 3: Full Extent of Northwest Expressway (Central Area) per BikeWalkOKC Master Plan



2.3 Summary

Upon completion of the field assessments for approximately 35,000 linear feet of sidewalk in the three (3) M4-TS020 MAPS 4 Sidewalks and Related Amenities 2A project areas outlined in the BikeWalkOKC Master Plan, Garver began developing preliminary alignments. The initial layout of the preliminary sidewalk alignments were based on a full build out of M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A of the BikeWalkOKC Master Plan per verbatim. This included maximum sidewalk widths throughout the project extents of 5-foot-wide in residential areas and 6-foot-wide in commercial areas at the back of curb on arterial streets. Based upon the preliminary construction costs as shown in Section 2.4 of this report, the full build out of M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A of the BikeWalkOKC Master Plan nearly doubled the available \$3,784,600 construction budget.

To reduce construction cost below the available construction budget, Garver shifted focus to identifying areas where the project could be value engineered while still aligning with the project's scope of work. Generally, the BikeWalkOKC Master Plan outlined pedestrian improvements on both sides of each street in both residential and commercial areas of the project. To provide feasible project options, Garver elected to reduce sidewalk improvements to only one (1) side of each street to help reduce cost and stay within the established M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A \$3,784,600 construction budget. The primary factors in determining which side of the street to construct a sidewalk included pedestrian improvement connectivity, topographical design challenges, and ultimately the anticipated construction cost.

Garver determined the Lyrewood Lane segment of the BikeWalkOKC Master Plan, as shown in **Figure 4** on Page 12, should be constructed in its entirety to: 1) maximize pedestrian connectivity to meet ADA guidelines for accessible routes (PROWAG Section R302 "Pedestrian Access Routes"); 2) the number of bus stops that provides residents additional transit options; 3) and to serve the large number of multi-family units along this alignment. It was also determined the sidewalks along Lyrewood Lane should remain the maximum commercial sidewalk width of 6-foot-wide.

Next, Garver looked at reducing sidewalks to only one side of the residential streets in the East Area, as shown in **Figure 5** on Page 13, but maintaining the maximum residential sidewalk width of 5-foot-wide. Even with these reductions, Garver believes these design changes would create the desired pedestrian connections as planned by the BikeWalkOKC Master Plan and meet both ADA guidelines as well as the City of Oklahoma City's standards.

Lastly, the NW Expressway segment was evaluated as originally proposed in the BikeWalkOKC Master Plan. Garver determined the NW Expressway segment, as shown in **Figure 6** on Page 14, should be constructed in its entirety to provide a continuous ADA-compliant path and meet the original intent of the project. Garver also determined the sidewalks along NW Expressway should remain the maximum commercial sidewalk width of 6-foot-wide.

Based upon field observations, each of these three (3) M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A project areas outlined in the BikeWalkOKC Master Plan should be constructed to provide connectivity between people and places. Garver has prepared preliminary construction plans for these alignments as shown in **Appendix A** that will meet the established MAPS 4 project budget.



Figure 4: Option 2 Recommended Project Extents along Lyrewood Lane (West Area)

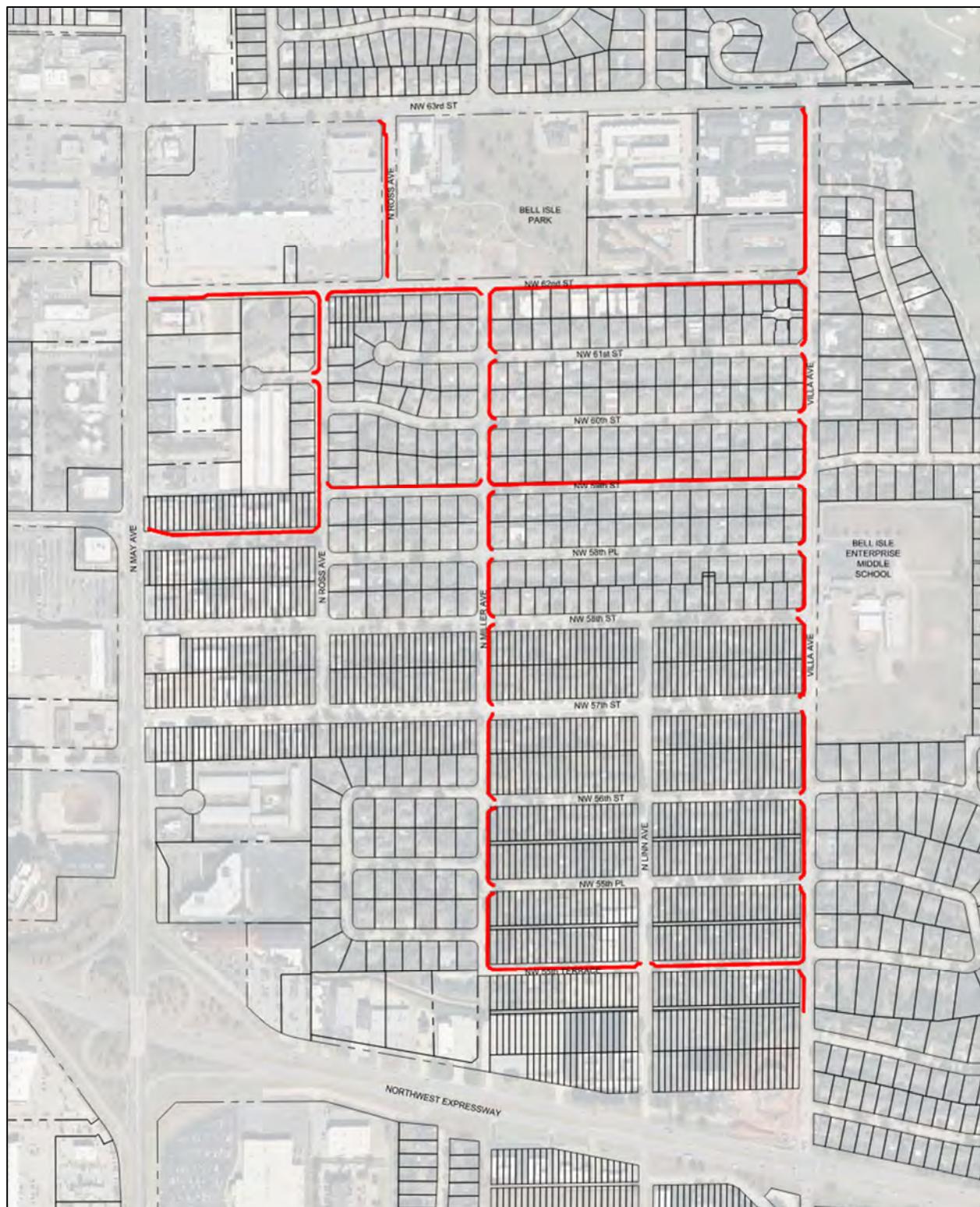


Figure 5: Option 2 Recommended Project Extents for NW 63rd Street and May Avenue (East Area)



Figure 6: Option 2 Recommended Project Extents along NW Expressway (Central Area)



2.4 Budget

Garver has evaluated two (2) design options for the M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A project as outlined below. Each option in Garver's professional opinion will meet the intent of the BikeWalkOKC Master Plan to improve pedestrian connectivity. Detailed construction costs can be seen in **Appendix B**.

Option 1

Full Build Out of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths

- 6' wide sidewalk along Lyrewood Lane
- 5' wide sidewalk throughout East Area
- 6' wide sidewalk along NW Expressway

Option 1

Phase 2A Construction Budget	\$3,784,600.00
Preliminary Cost Estimate	\$5,460,674.40
Budget Overage	(\$1,676,074.40)

Option 2 (Recommended Option)

Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths

- 6' wide sidewalk along Lyrewood Lane (Full extents)
- 5' wide sidewalk in select locations within East Area (Reduced extents to one side of street)
- 6' wide sidewalk along NW Expressway (Full extents)

Option 2 (Recommended Option)

Phase 2A Construction Budget	\$3,784,600.00
Preliminary Cost Estimate	\$3,582,648.00
Budget Balance	\$201,952.00



2.5 Recommendations

Garver performed field assessments, created preliminary alignments, and evaluated alternative options to best match the available M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A construction budget. Since Option 1 exceeded the M4-TS020 MAPS4 Sidewalk and Related Amenities Phase 2A construction budget, Garver proposed Option 2 which reduces the project extents to only one (1) side of each street to match the available funding for M4-TS020 MAPS 4 Sidewalks and Related Amenities 2A. Although Option 2 is a reduction of the full build out M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A as outlined in the BikeWalkOKC Master Plan, it still achieves the goal of connecting parks, commercial areas, schools, and bus stops, while providing safer travel methods for pedestrians. Table 1 outlines the advantages and disadvantages of the recommended option.

Table 1: Option 2 (Recommended Option) Advantages/Disadvantages

Option 2 (Recommended Option) Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths	
Advantages	Disadvantages
<ul style="list-style-type: none">Provides the most pedestrian connectivity within budget.	<ul style="list-style-type: none">Reduces project sidewalks to one (1) side of each street.
<ul style="list-style-type: none">Meets intent of BikeWalkOKC Master Plan.	
<ul style="list-style-type: none">Allows for maximum sidewalk widths.	
<ul style="list-style-type: none">Construction cost within MAPS 4 budget.	



3.0 Field Assessments

3.1 Lyrewood Lane

The section of Lyrewood Lane being assessed for sidewalks extends from NW 63rd Street on the south terminus to Wilshire Boulevard on the north terminus, situated on the east side of Lyrewood Lane. This area falls within the western extents of M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A. The general land use along Lyrewood Lane primarily consists of multi-family apartments, with limited commercial development approximately halfway along the corridor. Additionally, Tulakes Elementary School is located at the corner of Galaxie Drive and Lyrewood Lane. During Garver's field assessment, several residents were observed walking up and down the side of the street, creating a well-worn dirt path next to the back of the curb.

Currently, there is only a segment of approximately 130 linear feet (LF) of existing sidewalk in the corridor. This segment is situated at Knight Lake Drive and extends north to an updated bus stop, refer to **Image 1**. Apart from this single sidewalk segment, no other sidewalks are present along the east side, which hampers connectivity and creates safety risks for residents as they travel between the commercial area, apartments, bus stops, and Tulakes Elementary School.



Image 1: New Bus Stop located north of Knight Lake Drive

The updated bus stop shown in **Image 1** is the only one along the east side of the corridor that has been updated to the newest Embark standards. However, there are five existing bus stops (three with benches and two without) that have not been updated. **Image 2** and **Image 3** provide a general view of the



unimproved bus stops, where numerous pedestrians were observed waiting for the bus without any shelter or benches.

Table 2: West Area Assessment

Lryewood Lane			
Street	Start	End	Total Sidewalk LF Assessed
Lryewood Lane	Willshire Avenue	NW 63rd Street	5,601



Image 2: Typical bus stop without bench. This stop is located near the intersection of Greenway Drive and Lyrewood Lane.

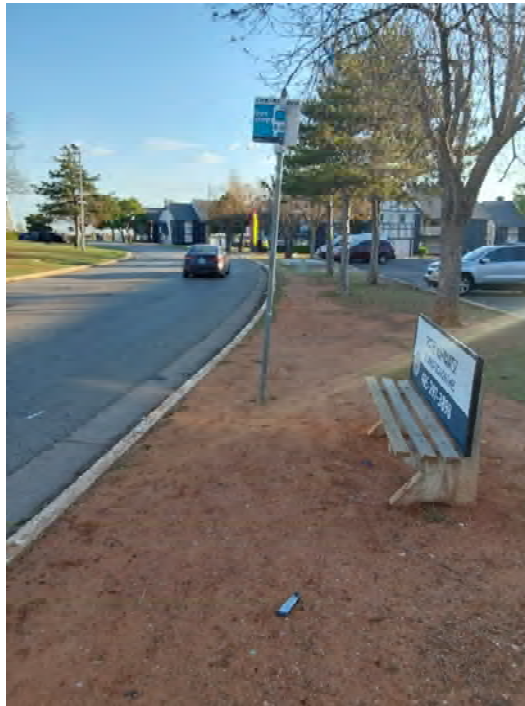


Image 3: Typical bus stop with bench. This stop is located near the intersection of Wilshire Boulevard and Lyrewood Lane in front of the Lakewood Estates complex.

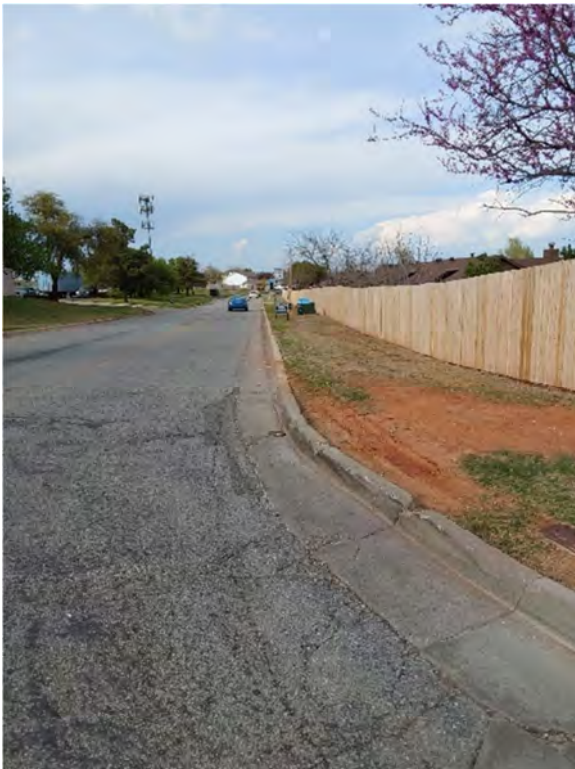




Image 4: General conditions along the east side of Lyrewood Lane photos start form 63rd Street and proceed North to Wilshire Blvd.



3.1.1 Right-of-Way

Garver verified them using the plats obtained from the Open Data Portal site. In the West Area, Garver does not anticipate a great deal of right-of-way that will need to be obtained. The table below outlines the general right-of-way width. Temporary right-of-way may be necessary during construction to accommodate steep drives, and this will be identified at the 60% stage. The subsequent survey table outlines areas where permanent right-of-way is required or where verification is needed to ensure any permanent takings.

Table 3: West Area Right-of-Way

Existing Right-of-Way Table		
Street	Typical Right-of-Way	Is there need for additional Right-of-Way acquisition if yes see detail survey table
Lyrewood Lane	30' either side from center line of road	Yes

3.1.2 Areas of Concern

Between Galaxie Drive and NW 63rd Street, utilities such as communication pedestals, communication vaults, water meters, OGE electric transformers, power poles, and signs occupy the space between the existing fence line and the back of the curb (as seen in **Image 5** and **Figure 7**). While some larger items may be avoided using serpentine alignments, the relocation of certain utilities will likely be necessary, potentially causing longer construction times in this segment.



Image 5: Typical view of the area between block of curb and fence line.



Figure 7: Aerial view for area of concern at NW 63rd Street to Galaxie Drive.



Located approximately 350 LF south of Knight Lake Drive is a USGS Blue Line stream crossing. The east side of the box is showing signs of distress with visible rebar and breaking of concrete see **Image 6** below. Additionally, to meet the required 6-foot width for the sidewalk, the box needs to be extended approximately 4 feet, as the current width from the back of the curb is approximately 2 feet. As of this report, Garver believes that no special permitting will be required to repair and install the end section of the sidewalk. However, should this need arise, Garver is prepared and equipped to handle any required permits. A detailed survey would be necessary if any permits were needed. Considering this information and the need to accurately quantify the work required to repair the end section, a detailed survey should be performed prior to designing and installing the new sidewalk in this section. It's important to note that this area serves as a major connection for the north end of the corridor to the commercial area immediately adjacent to the south of this crossing, as well as the Tulakes Elementary School located farther south.



Image 6: Drainage Structure on Lyrewood Lane showing signs of distress. Location near Knight Lake Drive and Lyrewood Lane.



3.1.3 Detailed Survey Required

The following table outlines the areas, approximate sizes, and reasons for the surveys needed prior to design and construction of the new sidewalk.

Table 4: West Area Required Survey

Survey Required		
Location	Approximate LF	Reason for Survey
Stream crossing near intersection of Lyrewood Lane and Knight Lake Drive (see figure 8)	50	For repair of drainage box and if any special permitting is required for crossing the USGS Blue Line
Stream crossing near intersection of Lyrewood Lane and Knight Lake Drive (see figure 8)	50	Right-of-way acquisition for construction of box extension



Figure 8: Areas for survey highlighted in orange dashed boxes. Location near Knight Lake Drive and Lyrewood Lane.



3.2 Northwest Expressway

The section along Northwest Expressway (NW Expressway) that assessed spans from Meridian Avenue to NW 63rd Street on the south side of NW Expressway. At the northwest terminus, the Bert Cooper Trail runs north-south along Meridian Ave. Continuing north along Meridian Ave, we encounter the new BRT bus stop, and users can use the Bert Cooper Trail (refer to **Image 7**) crossing at NW Expressway that is a signalized intersection.

On the southeast end of the corridor, is NW 63rd Street, there already exists a sidewalk with a signalized crossing. Users can continue their journey along NW 63rd Street, access existing bus stops, or proceed along NW Expressway (see **Image 8**).

Table 5: Central Area Assessment

NW Expressway			
Street	Start	End	Total Sidewalk LF Assessed
NW Expressway	Meridian Avenue	NW 63rd Street	3,350

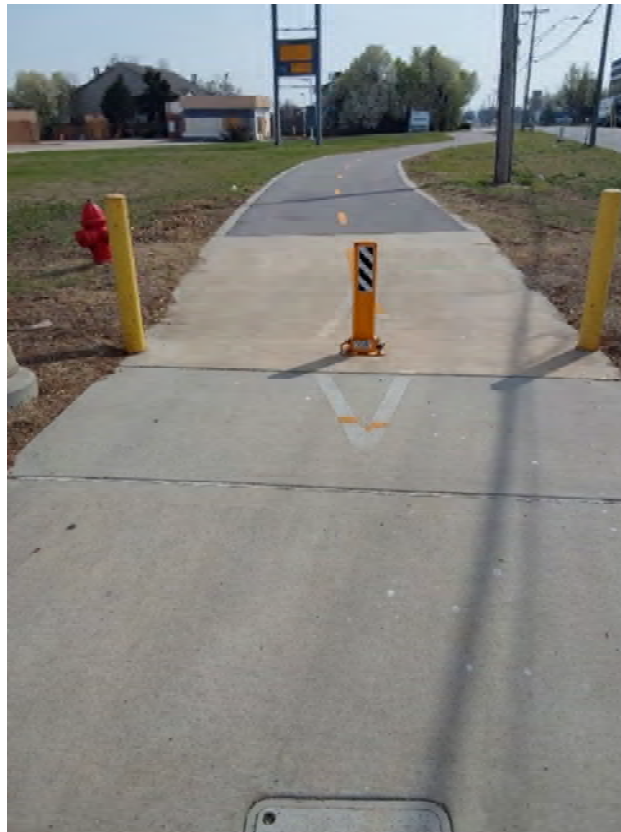


Image 7: Facing South looking at the Bert Cooper Trail at the SW corner of NW Expressway and Meridian Avenue. This will be the terminus point of the NW end of the proposed sidewalk.



Image 8: Facing West looking at the existing sidewalk at the terminus point of the SE end of the proposed sidewalk at the intersection of NW Expressway and NW 63rd Street.

NW Expressway serves a mix of land uses, including office, religious, medical, and commercial properties. The commercial makeup primarily consists of hotels, retail establishments, and restaurants. As a major roadway in NW OKC, NW Expressway experiences higher speeds and greater vehicle volumes compared to other streets in this project phase. These factors significantly impact the design considerations, especially regarding drive crossings and separation distances from travel lanes to the proposed sidewalk.

Apart from the two noted terminus points, no existing sidewalks connect the various land uses in the area or provide access to the city through the bus network. Our assessment team captured photos (see **Image 9**) to convey the character of the corridor.

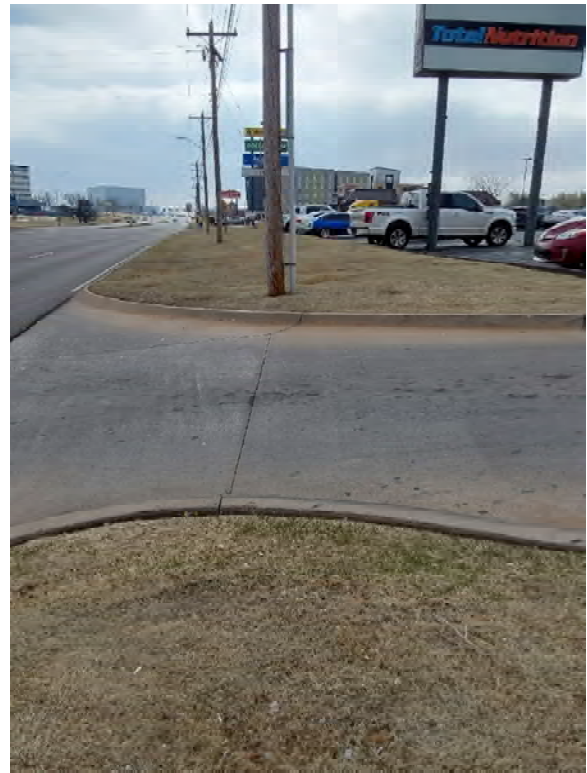


Image 9: General conditions along the south side of Northwest Expressway photos start at Meridian Avenue and proceed SW to NW 63rd Street.



Image 9 (continued): General conditions along the south side of Northwest Expressway photos start at Meridian Avenue and proceed SW to NW 63rd Street.

3.2.1 Existing Utilities

Throughout the entire assessed corridor, both public and private utilities were meticulously recorded. While some utility relocations will be necessary, the generally flat conditions and ample right-of-way allow Garver to employ a serpentine path in many areas, minimizing disruptions during design and construction.

3.2.2 Right-of-Way

Garver verified them using the plats obtained from the Open Data Portal site. In the Central Area, Garver does not anticipate a great deal of right-of-way that will need to be obtained. The table below outlines the general right-of-way width. Temporary right-of-way may be necessary during construction to accommodate steep drives, and this will be identified at the 60% stage. The subsequent survey table outlines areas where permanent right-of-way is required or where verification is needed to ensure any permanent takings.



Table 6: Central Area Right-of-Way

Existing Right-of-Way Table		
Project Street	Typical Right-of-Way	Is there need for additional Right-of-Way acquisition
NW Expressway	Typical right-of-way of appears to be 150' total but no plats are available along this section of NW Expressway	Yes

3.2.3 Areas of Concern

NW Expressway generally maintains a relatively flat profile with adequate right-of-way for sidewalk improvements. However, the higher speeds and traffic volumes pose challenges. Garver's design plan aims to increase separation between the back of the curb and the sidewalk. We intend to move the planned sidewalk closer to the right-of-way line rather than immediately off the back of the curb. The corridor also contains numerous public and private utilities, which will require careful coordination during design and construction for relocations.

In addition to utility concerns, there are specific areas that will necessitate either detailed survey work or the use of variable height walls. For instance, the stream crossing approximately 700 LF southeast of the Meridian Avenue and NW Expressway intersection near the OKC First Church of the Nazarene requires surveying to ensure adequate space for the trail and to design fall protection railing. Similarly, the commercial drive located northwest of the same crossing will need significant rebuilding to meet ADA standards with the new sidewalk.



Image 10: Stream Crossing Area of Concern. Location 700 LF southeast of Meridian Ave and NE Expressway.



A specific area of concern lies approximately 1,500 LF southwest of the Meridian Avenue intersection, near the Northwest Office Center. Here, our focus centers on utility conflicts, drainage considerations, and topography. Currently, Garver anticipates that the existing 36" RCP (Reinforced Concrete Pipe) will need to be extended to accommodate the proposed sidewalk improvements. Additionally, additional right-of-way may be required to adjust curb inlets and the 30" outfall structure. **Image 11** provides further insight into this critical section.



Image 11: Utility and Drainage Area of Concern. Location in front of the Lakeshore Shopping Center and Northwest Office Center

Lastly, another area of concern lies in front of the Lakeshore Shopping Center commercial area, approximately 420 LF northwest of the Wedgewood Circle and NW Expressway intersection. Variable height walls (less than 1.5' tall) may be employed to address the steep slope in this section (see **Image 12** for details on slope and utility content)."



Image 12: Topography and Utility concern in front of the Lakeshore Shopping Center.



3.2.4 Detail Survey Required

Table 7: Central Area Survey

Survey Required		
Location	Approximate LF	Reason for Survey
Stream crossing approximately 700 LF SE of the intersection of Meridian Avenue and NW Expressway (see figure 9)	100	Information about existing box structure and surrounding area.
Drainage area 1,500 LF SE of the intersection of Meridian Avenue and NW Expressway (see figure 10)	75	Topo and utility information
Drainage area 1,500 LF SE of the intersection of Meridian Avenue and NW Expressway (see figure 10)	75	Right-of-way acquisition for construction and grading of drainage and utility relocations.

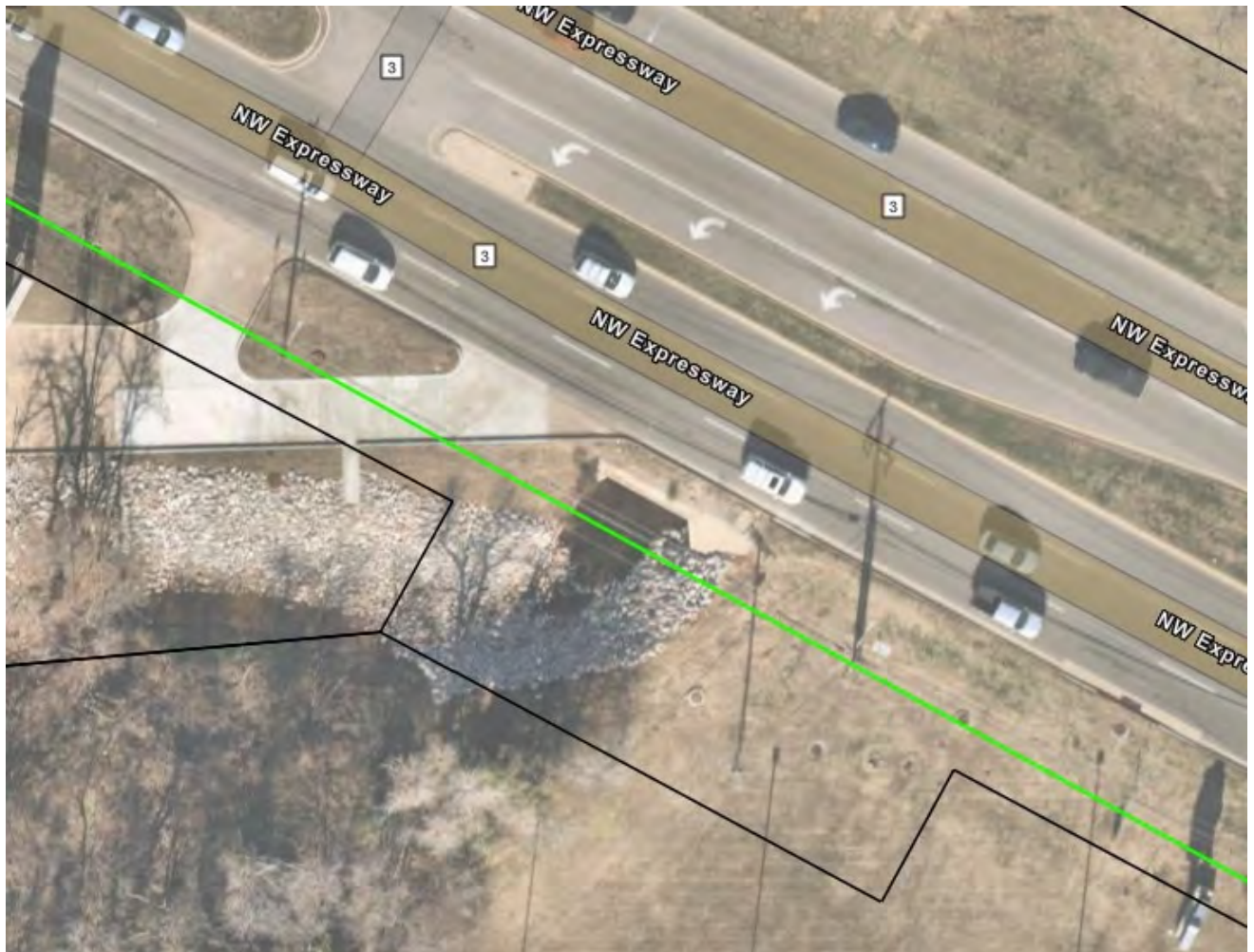


Figure 9: First area of survey. Location 700 LF southeast of Meridian Ave on NW Expressway.

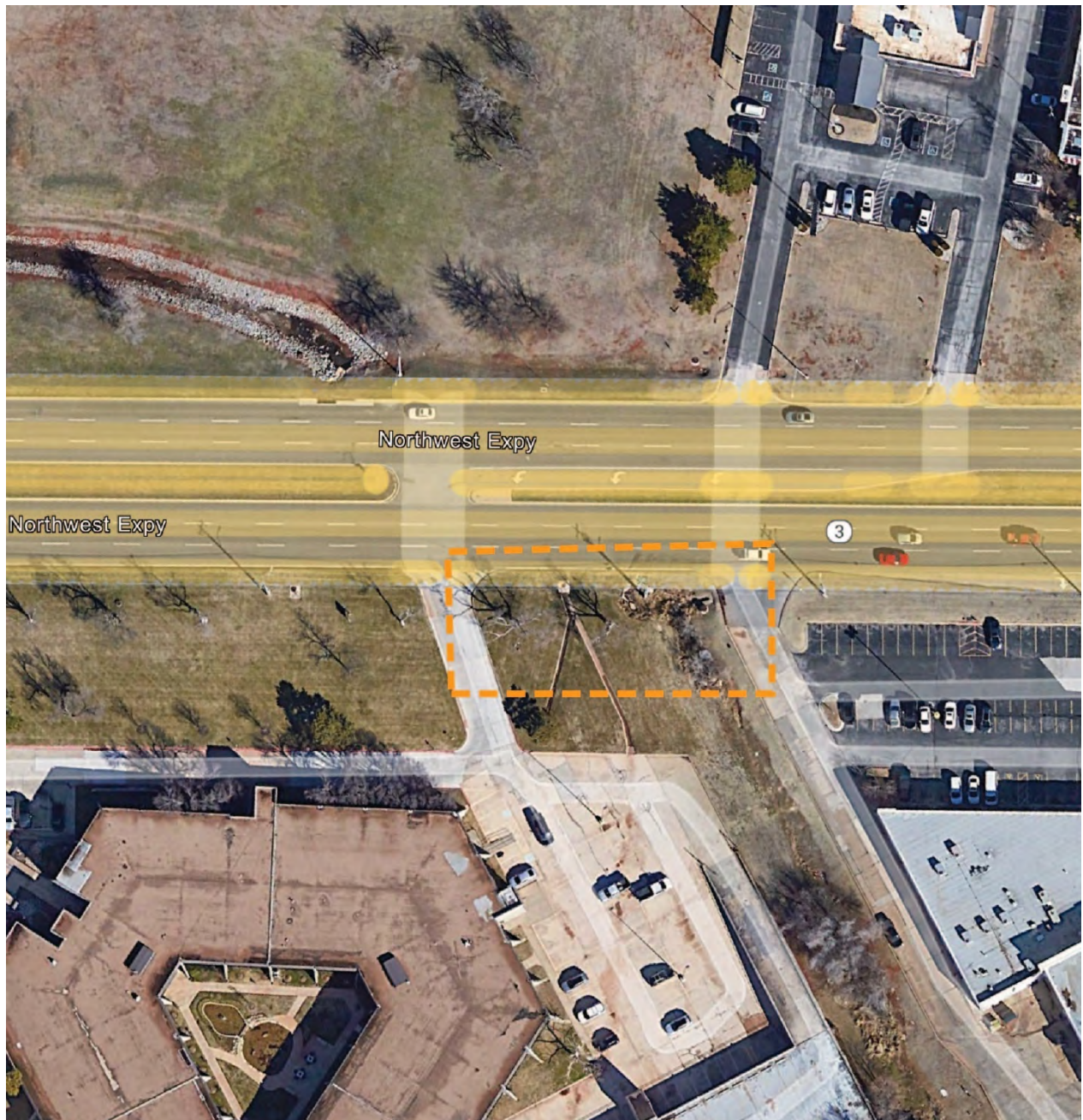


Figure 10: Second area of survey. Location 1500 LF southeast of Meridian Ave on NW Expressway.



3.3 East Area

The East Area is the largest area that is being assessed and designed for proposed sidewalks in the M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A project. The area falls within the borders of NW 63rd Street on the North, NW Expressway on the South, Villa Avenue on the East, and May Avenue on the West. The general land use in the area is single family residential with a few exceptions. Differing land uses are as follows:

- Villa multifamily houses and senior living apartments from NW 63rd Street to NW 62nd Street.
- Belle Isle Enterprise Middle School on the east side of Villa Avenue between NW 56th Street and NW 59th Street.
- Belle Isle Library on the east side at the corner of NW Expressway and Villa Avenue.
- Ross Avenue at the corner of NW 63rd Street where an existing apartment complex is and along the West side commercial.
- Belle Isle Park at the corner of NW 62nd Street and Ross Avenue.
- NW 59th Street from May Avenue to Ross Avenue has commercial on north and south sides and multifamily apartments on north and south side.
- Rosh Pinah Messianic Jewish Congregation at NW Terrace and Linn Avenue.

During our assessments in the area, the Garver team observed several residents walking along the streets. Some of them approached us to inquire about our work. Overall, the reactions were positive. Although we didn't keep an exact count, it was quite common to encounter someone every hour while we were present.

Table 8: East Area Assessment

Streets Within East Area			
Project Street	Start	End	Total Sidewalk LF Assessed
NW 62nd Street	May Avenue	Villa Avenue	5,270
Ross Avenue	NW 63rd Street	NW 62nd Street	1,330
Ross Avenue	NW 59th Street	NW 59th Street	395
NW 55th Terrace	Millar Avenue	Villa Avenue	2,465
NW 59th Street	May Avenue	Villa Avenue	5,160
Villa Avenue	NW 63rd Street	NW Expressway	6,697
Miller Avenue	NW 62nd Street	NW 55th Terrace	4,608

The following images are character photos from the area and are broken down by streets.



3.3.1 NW 62nd Street Photos



Image 13: General conditions along NW 62nd Street Photos starting from May Avenue and proceeding east to Villa Avenue.



Image 13 (continued): General conditions along NW 62nd Street Photos starting from May Avenue and proceeding east to Villa Avenue.



Image 14: Sidewalks that either connect or are near the street at the existing Belle Isle Park Sidewalk system. The Belle Isle Park is located at the corner of NW 62nd Street and Ross Avenue.



3.3.2 Ross Avenue Photos



Image 15: General conditions along Ross Ave photos starting from NW 62nd Street and proceeding north to NW 63rd Street.



Image 16: General conditions along Ross Ave between the segments of NW 59th Street.



3.3.3 NW 55th Terrace Photos



Image 17: General conditions along NW 55th Terrace photos starting from Miller Avenue and proceeding east to Villa Avenue.



Image 17 (continued): General conditions along NW 55th Terrace photos starting from Miller Avenue and proceeding east to Villa Avenue.

3.3.4 NW 59th Street Photos



Image 18: General conditions along NW 59th Street photos starting from May Avenue and proceeding east to Ross Avenue.



Image 18 (continued): General conditions along NW 59th Street photos starting from May Avenue and proceeding east to Ross Avenue.



Image 19: General conditions along NW 59th Street photos starting from Ross Avenue and proceeding east to Villa Avenue.

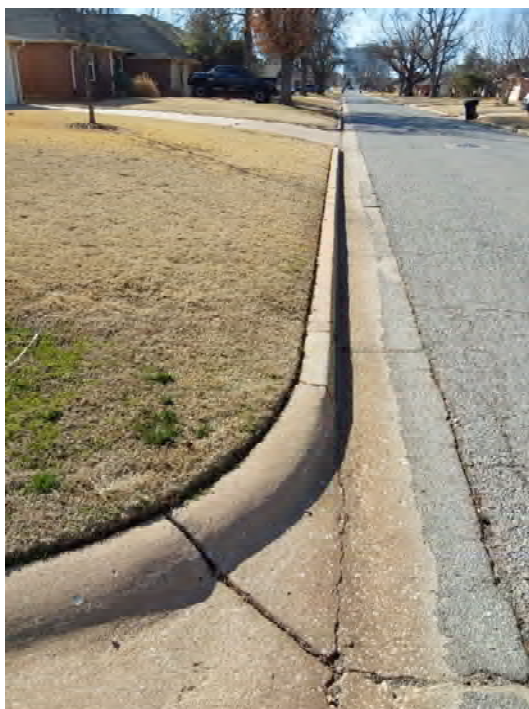


Image 19 (continued): General conditions along NW 59th Street photos starting from Ross Avenue and proceeding east to Villa Avenue.

3.3.5 Villa Avenue Photos



Image 20: General conditions along Villa Avenue photos starting at NW 63rd Street and proceeding south to NW Expressway.

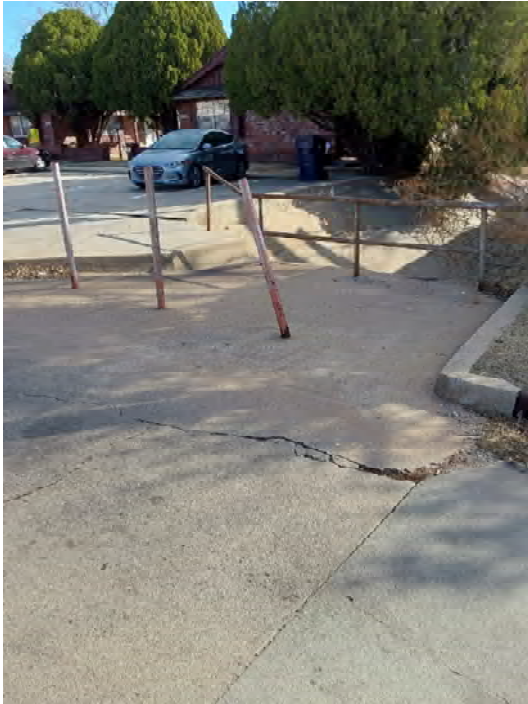


Image 20 (continued): General conditions along Villa Avenue photos starting at NW 63rd Street and proceeding south to NW Expressway.



Image 20 (continued): General conditions along Villa Avenue photos starting at NW 63rd Street and proceeding south to NW Expressway.



3.3.5.1 Existing Sidewalks Along Villa Avenue

Existing sidewalks are present in four locations along Villa Avenue. The following images depict these sidewalks, which are within the city right-of-way and will be incorporated into this phase as the terminus for the proposed sidewalk. Notably, the section of sidewalk in front of Belle Isle Enterprise Middle School was not originally part of the length of Villa Avenue with a sidewalk, so the photos of those existing sidewalks will be the point of terminus.



Image 21: Existing sidewalk and ADA ramps located on the West side of Villa Avenue at the intersection of NW 57th Street.



Image 22: The north and south terminus of existing sidewalk in front of the Belle Isle Enterprise Middle school on Villa Avenue.



Image 23: Existing sidewalk on the west side of Villa Avenue starting approximately 170 LF South of NW 55th Terrace and continuing south to NW Expressway.



3.3.6 Miller Avenue Photos



Image 24: General conditions along Miller Avenue photos starting at NW 55th Terrace and proceeding north to NW 62nd Street.

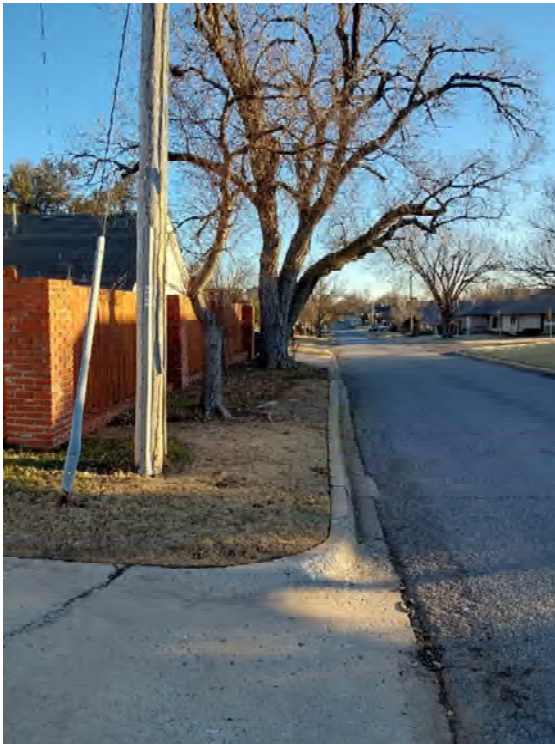
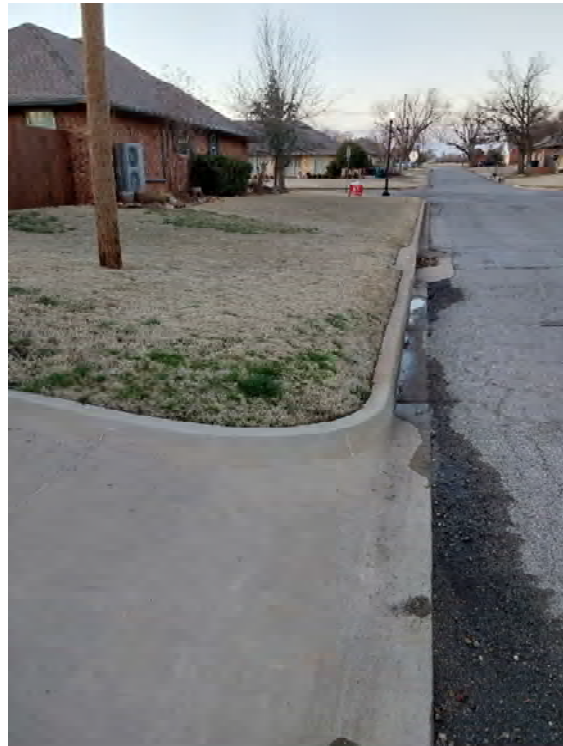


Image 24 (continued): General conditions along Miller Avenue photos starting at NW 55th Terrace and proceeding north to NW 62nd Street.



3.3.7 Right-of-Way

For this project, rights-of-way was verified using the plats obtained from the Open Data Portal site. In the East Area, Garver anticipates the need for multiple right-of-way verifications and has identified larger areas of temporary right-of-way. As part of the verification process, the potential requirement for acquiring permanent right-of-way will be assessed and addressed with the MAPS 4 Connectivity Subcommittee and MAPS 4 Citizens Advisory Board after completing the boundary survey and before the 60% Design submittal. Additionally, temporary right-of-way may be necessary during construction to accommodate steep drives, and this will be identified during the 60% Design stage. The table and subsequent survey table will outline areas where permanent right-of-way is required or where verification or existing right-of-way is needed to ensure any permanent takings.

Table 9: East Area Right-of-Way

Existing Right-of-Way Table		
Street	Typical Right-of-Way	Is there need for additional Right-of-Way acquisition
NW 62nd Street	30' either side from center line of road	Yes
Ross Avenue from NW 63rd Street to NW 62nd Street	30' either side from center line of road	Yes
Ross Avenue from NW 63rd Street to NW 58th Street	25' either side from center line of road	Yes
Villa Avenue	30' either side from center line of road	Yes
Miller Avenue from NW 62nd Street to NW 58th Street	25' either side from center line of road	Yes
Miller Avenue from NW 58th Street to NW 56th Street	60' Total right-of-way per plat 2038	Yes
Miller Avenue from NW 56th Street to NW 55th Terrace	50' Total right-of-way per plat 3047	No
NW 55th Terrace	50' Total right-of-way per plat 924	Yes
NW 59th Street from Villa Avenue to Ross Avenue	25' either side from center line of road	Yes
NW 59th Street from Ross Avenue to May Avenue	80' Total right-of-way per plat 2038	No

3.3.8 Areas of Concern

Due to not all streets having concerns or the challenges are minor that can be overcome by using serpentine paths, or variable height walls under 18" tall the falling sections have been broken into topography concerns, existing land use concerns, and right of way concerns. Some of the concerns may include multiple of these and that will be outlined in the paragraph detail the cause for concern. The biggest concern will dictate the final categorization.



3.3.9 Topography Concerns

The north side of NW 62nd Street, starting at the corner of May Avenue and continuing east for approximately 590 LF, presents a steep slope that necessitates retaining walls, adjustments to drainage structures, and significant work on an existing commercial drive and parking lot to ensure ADA accessibility for crossing the drive. The images below illustrate the extent of the slope and the potential challenges in adding a sidewalk along this stretch of NW 62nd Street.

In addition to addressing the slopes we have consider the terminus point for the users. The sidewalk will terminate at the corner of NW 62nd Street and May Avenue, where no existing sidewalks exist. Furthermore, there is no protected crossing for westward travel across May Avenue, and no path extends to the north or south along May Avenue. In the subsequent sections of this report, we will outline our recommendations for this specific section of NW 62nd Street.



Image 25: Area of Concern 1 at NW 62nd Street and May Ave.



Image 25 (continued): Area of Concern 1 at NW 62nd Street and May Ave.

Area of concern 2 lies on the south side of NW 62nd Street, approximately 405 LF east of the May Avenue and NW 62nd Street intersection, continuing east for about 315 LF to the Ross Avenue and NW 62nd Street intersection. Currently, the ground slope from the back of the curb to the point where it increases is manageable, with a 6" drop over +/-6'. However, with the proposed 6' sidewalk in this area, a survey is necessary to confirm elevations, slope, and the design of required grade-changing features and appropriate fall protection. Additionally, in this alignment section, we anticipate relocating two power poles and some communication boxes. **Image 26** provides insight into the steepness of the grade.



Image 26: Area of Concern 2 at NW 62nd Street and N. Ross Ave.

Area of concern 3 extends 420 LF west on NW 62nd Street and 270 LF north on Villa Avenue from the intersection of NW 62nd Street and Villa Avenue. The NW 62nd Street section is on the north side of the road, while the Villa Avenue section is on the west side. To address the topography here, long retaining walls will be necessary, along with the reconstruction of four private staircases to maintain access for residents. Additionally, we plan to relocate three power poles on NW 62nd Street and possibly two on Villa Avenue. The figures illustrate the slope and highlight two of the staircases requiring reconstruction.



Image 27: Topography concerns and staircases at the corner of Villa Avenue and NW 62nd Street.

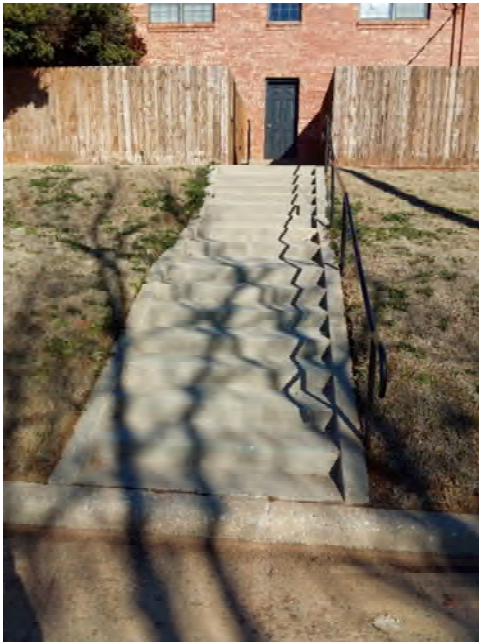


Image 27 (continued): Topography concerns and staircases at the corner of Villa Avenue and NW 62nd Street.

Finally, area of concern 4, located at the northwest corner of NW 55th Terrace and Linn Avenue, poses the last topographical challenge in the east area of M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A. This spot houses the Rosh Pinah Messianic Jewish Congregation and their existing sidewalk and staircase. Constructing a sidewalk within the city right-of-way and off the back of the curb will likely require retaining walls and reconstruction of the existing staircase. Refer to **Image 28** for a general overview of this section's character.

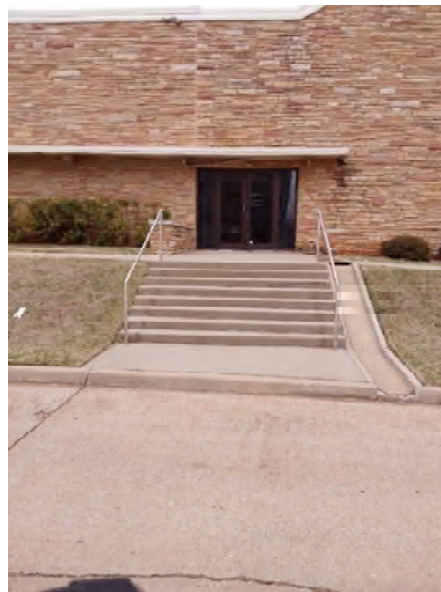


Image 28: Topography concerns and staircases at the corner of NW 55th Terrace and Linn Avenue.



3.3.10 Existing Land Use Concerns

The primary land use concerns relate to on-street parking or parking lanes that could potentially put pedestrians in conflict with vehicles. In three of the highlighted areas below, there is no grade separation between the street and the right-of-way line. Additionally, the lengths of pavement that are on the same level as the street exceed 100 LF, making it challenging to distinguish parking or drive areas from the street. This lack of clarity results in a less safe environment for pedestrians.

The first area of existing land use that raises concern, as identified by Garver, lies on the east side of Ross Avenue, and extends south for 400 LF from the intersection of NW 63rd Street. The primary issue here is on-street parking for an existing apartment complex. Approximately half of all parking stalls are located within the public right-of-way. Furthermore, there is no separation, such as a mountable curb, between the parking lot and the street. Refer to for a visual representation of this area.

Like the situation at Ross Avenue, there are two more instances within the East Area where on-street parking exists. Starting at the intersection of NW 62nd Street on the east side of Villa Avenue, there is a section approximately 175 LF long. Additionally, at the intersection of NW 61st Street on the east side of Villa Avenue there is a stretch of approximately 240 LF that extends all the way to NW 60th Street. As with the Ross Avenue area, there is no grade separation between Villa Avenue and the right-of-way line, and the parking stalls partially encroach into the city right-of-way.

The final area of land use concern is situated on the south side of NW 59th Street, near the intersection of May Avenue. This location features a commercial parking lot that currently utilizes the north side of their lot for a drive aisle with angled parking stalls. While the stalls remain within the lot boundaries, the drive aisle extends beyond the right-of-way line and is paved all the way to the back of the curb. Unlike other areas where sidewalk users might directly conflict with parking or be at street level, this section benefits from a separation provided by a standard 6" curb. The figure below illustrates the parking and drive lane adjacent to the curb and street.



3.3.11 Detail Survey Required

Table 10: East Area Survey

Survey Required		
Location	Approximate LF	Reason for Survey
North side of NW 62nd Street at the corner of May Avenue and NW 62nd Street (see Figure 11)	600	Topography for retaining wall and determination of drive and parking lot repair
South side of NW 62nd Street 405 LF East of the corner of May Avenue and NW 62nd Street (see Figure 11)	315	Topography for retaining wall
Corner of Villa Avenue and NW 62nd Street, North side of NW 62nd Street and West side of Villa Avenue (see Figure 12 and Figure 20)	700	Topography for retaining wall and staircase replacement, temporary ROW anticipated to re-build stairs
West side of Villa Avenue at the intersection of NW 60th Street and Villa Avenue (see Figure 13)	50	Drainage structure crossing
Villa Avenue both sides just north of Belle Isle Library at stream crossing (see Figure 14)	50	Drainage structure crossing
North Side of NW 55th Terrace at the intersection of NW 55th Terrace and Linn Avenue (see Figure 15)	190	Topography for retaining wall and staircase replacement
East side of Ross Avenue at the intersection of NW 63rd Street and Ross Avenue (see Figure 16)	290	Boundary survey required to verify ROW
South side NW 62nd Street at the corner Miller Avenue (see Figure 17)	425	Anticipated permanent ROW acquisition
North side of NW 59th Street at intersection of May Avenue and NW 59th Street (see Figure 18)	215	Boundary survey required to verify ROW
East side of Miller Avenue from NW 62nd Street to NW 68th Place (see Figure 19)	1,110	Boundary survey required to verify ROW
East side of Villa Avenue from NW 62nd Street to NW 60th Street (see Figure 20)	975	Boundary survey required to verify ROW
South side of NW 55th Terrace (see Figure 21)	1,300	Boundary survey required to verify ROW



All images below are oriented north up and are not to scale.



Figure 11: Areas for survey highlighted within red dashed boxes. Location May Ave and NW 62nd Street.



Figure 12: Areas for survey highlighted within red dashed boxes. Location Villa Ave and NW 62nd Street.

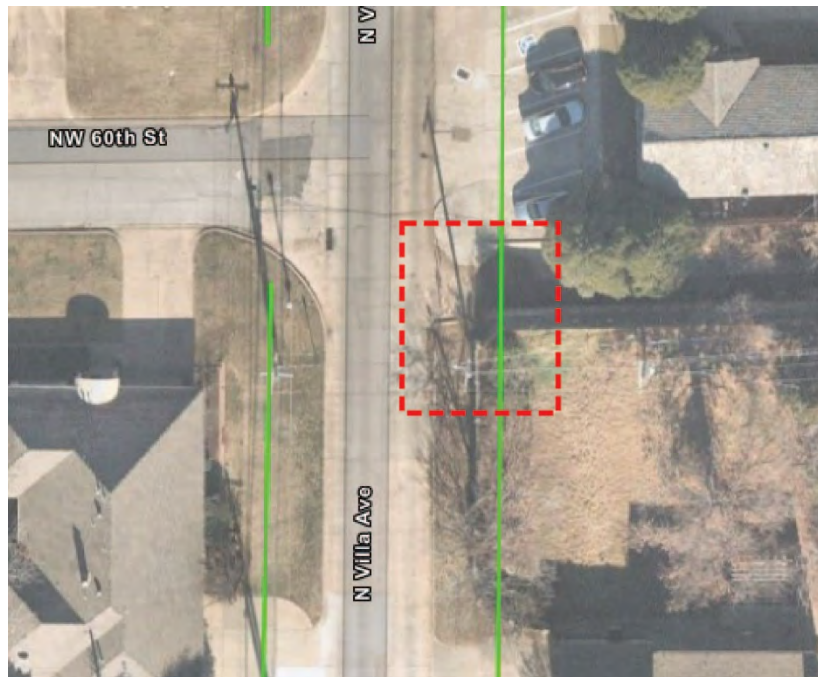


Figure 13: Areas for survey highlighted in red dashed boxes. Location Villa Ave and NW 60nd Street.

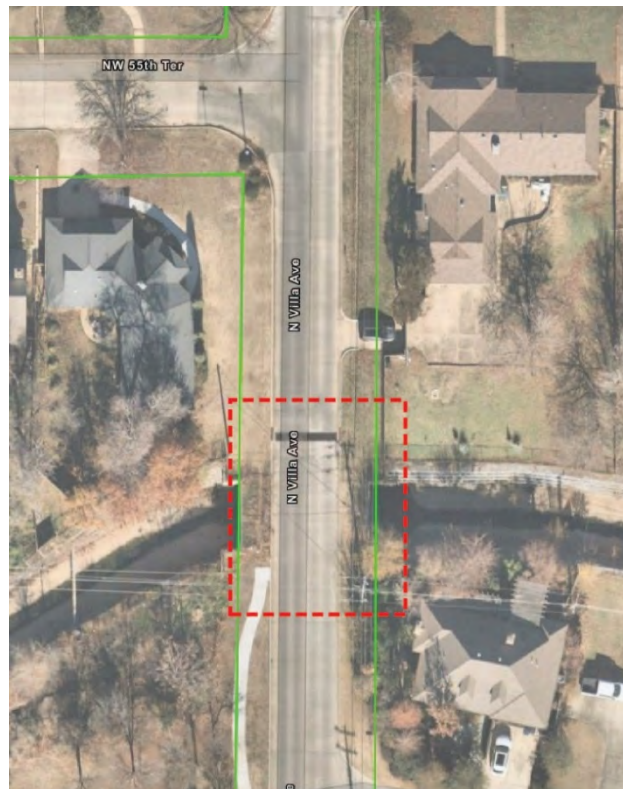


Figure 14: Areas for survey highlighted within red dashed boxes. Location Villa Ave and NW 55th Ter.



Figure 15: Areas for survey highlighted within red dashed boxes. Location Linn Ave and NW 55th Ter.

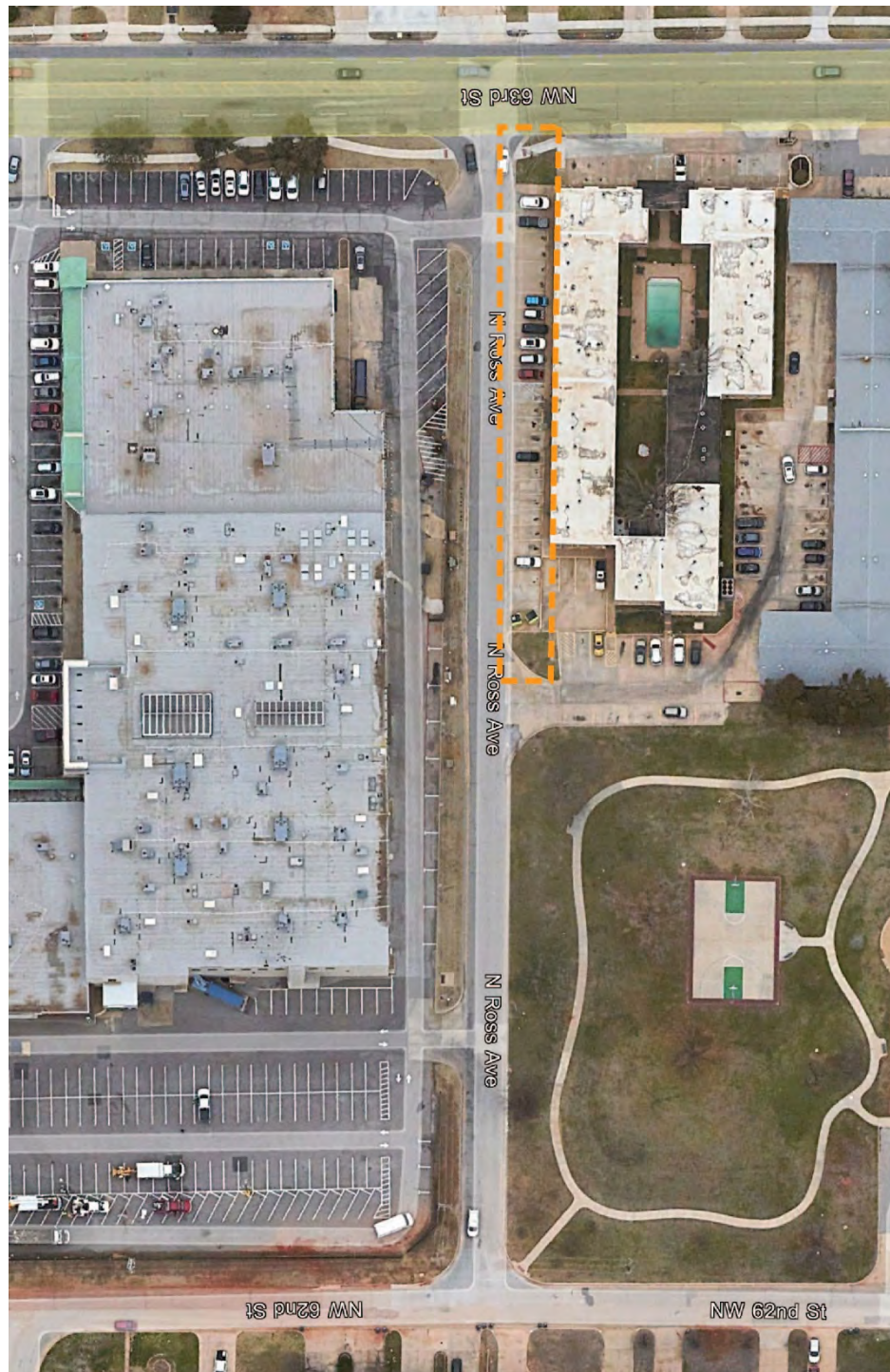


Figure 16: Areas for boundary survey highlighted within orange dashed boxes. Location N Ross Ave and NW 63rd Street.



Figure 17: Areas for boundary survey highlighted within orange dashed boxes. Location N Millar Ave and NW 62nd Street.



Figure 18: Areas for boundary survey highlighted within orange dashed boxes. Location N May Ave and NW 59th Street.



Figure 19: Areas for boundary survey highlighted within orange dashed boxes. Location N Miller Ave from NW 58th Street and NW 60th Street.



Figure 20: Areas for boundary survey highlighted within orange dashed boxes. Location N Villa Ave and NW 62nd Steet and N Villa Ave from NW 60th Street to NW 61st Street.



Figure 21: Areas for boundary survey highlighted within orange dashed boxes. Location NW 55th Ter from N Millar Ave to N Villa Ave.

4.0 Proposed Elements

4.1 Proposed Sidewalks

Garver proposes new ADA-compliant sidewalks throughout the three (3) M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A project areas outlined in the BikeWalkOKC Master Plan. Sidewalk widths will comply with City of Oklahoma City standards which require widths of 6' at curb and 5' at property line for commercial areas and 5' at curb and 4' at property line for residential areas. Typically, the West and Central Areas will be considered commercial, and the East Area will be considered residential. In addition to the proposed sidewalks, improvements will also include upgraded wheelchair ramps as well as driveway replacement, as necessary. To comply with ADA guidelines and City of Oklahoma City standards, sidewalks must be constructed with a maximum cross slope of 2.00% and affected driveways with a maximum slope of 8.00%. Typical sections for the proposed sidewalks are shown on the following page in **Figure 22** and typical driveway replacement is shown in **Figure 23**.

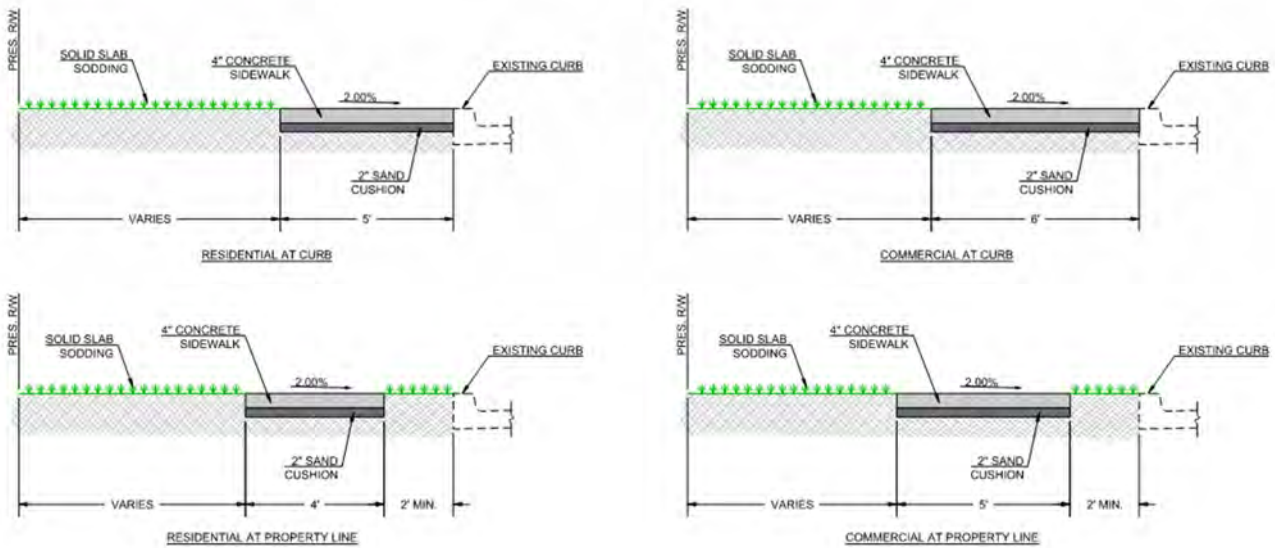


Figure 22: Proposed Sidewalk Typical Sections

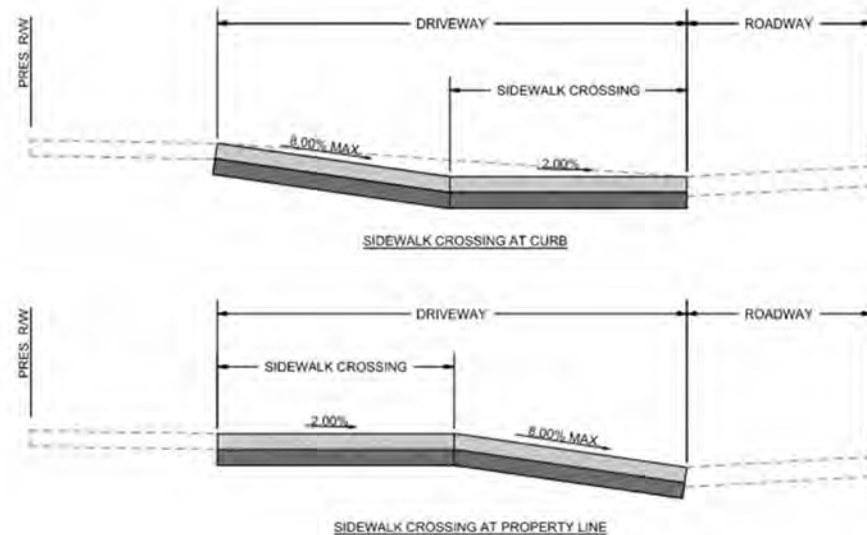


Figure 23: Typical Driveway Replacement

Garver anticipates there will be several locations along the project corridor where header curb or retaining wall will be necessary due to excessively steep grades. Such locations will require topographical survey to design the header curb and retaining wall to the required heights. **Figure 24** on the following page illustrates the header curb and retaining wall concept to accommodate proposed sidewalks through areas with steep grades.

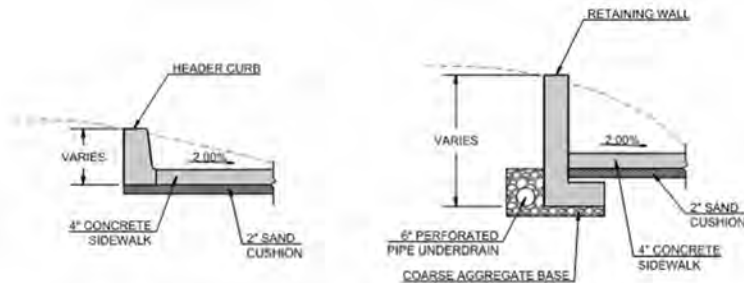


Figure 24: Header Curb and Retaining Wall Concept

4.2 Proposed Bus Stops

Garver evaluated the transit needs throughout the project extents which primarily included bus stop improvements. These bus stop improvements were typically located within the West Area as shown in **Appendix A**. Bus stops will be upgraded as necessary throughout the project extents to provide ADA-compliant pedestrian access to Embark bus routes and will comply with City of Oklahoma City and Embark standards. Proposed bus stop improvements will only consist of bus stop pads and will not include bus stop furniture. **Figure 25** below shows a general detail of an Embark bus stop pad. Bus stop improvements will need to be coordinated with the City of Oklahoma City and Embark.

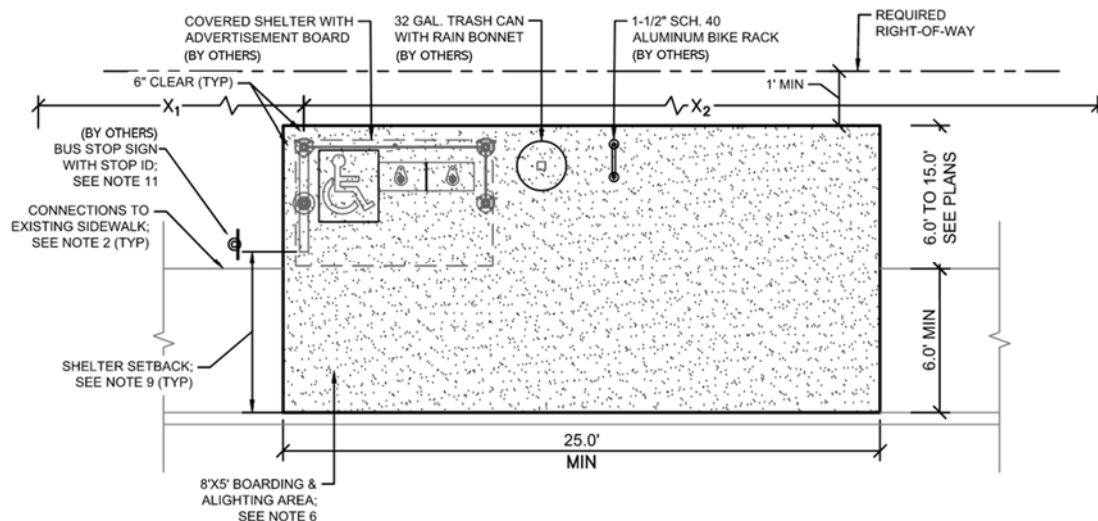


Figure 25: Embark Bus Stop Pad Layout Detail

4.3 Utility Conflicts

Water, sanitary sewer, and storm sewer infrastructure is present throughout the project extents. Manholes, valves, and meters will need to be adjusted to the grade of the proposed sidewalk or final grading as necessary. Additionally, there are several locations where existing light poles, storm sewer curb inlets, and fire hydrants could conflict with the sidewalk alignments. The ability to serpentine the sidewalk alignments as compared to a fixed alignment at the back of curb or property line will help to avoid such conflicts. However, it is anticipated that light poles, storm sewer curb inlets, and fire hydrant relocations may be necessary throughout the project extents.



Constraints with existing power poles is a common theme throughout the project extents but can generally be avoided. However, there are locations within each area where power pole relocations may be necessary. Garver anticipates communication and gas utility conflicts as well throughout the project extents. Most of these utility conflicts will be surface level conflicts such as meters and control boxes that will need to be relocated beyond the limits of the proposed sidewalk. Garver will notify all known utility companies and entities within the extents of the project affected by the proposed sidewalk construction early in the design phase. A utility conference will be coordinated at the 60% design phase to facilitate necessary utility relocations.

5.0 Preliminary Cost Estimates

Garver prepared construction costs for two (2) options as noted below in **Table 11** to best match the available budget set for M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A. The preliminary construction cost estimates for the project are reflective of the latest available unit price bid tabs from the Oklahoma Department of Transportation and most recent MAPS 4 sidewalk projects. A detailed preliminary cost estimate can be found in **Appendix B**.

Table 11: Summary of Cost Estimates

Preliminary Cost Estimates	
M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A Construction Budget	\$3,784,600.00
Option 1	
Full Build Out of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths	
<ul style="list-style-type: none">6' wide sidewalk along Lyrewood Lane5' wide sidewalk throughout East Area6' wide sidewalk along NW Expressway	\$5,460,674.40
Option 2 (Recommended Option)	
Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths	
<ul style="list-style-type: none">6' wide sidewalk along Lyrewood Lane (Full extents)5' wide sidewalk in select locations within East Area (Reduced extents to one side of street)6' wide sidewalk along NW Expressway (Full Extents)	\$3,582,648.00



6.0 Project Schedule

Table 12: Project Schedule

MAPS 4 Sidewalks and Related Amenities Phase 2A Project Schedule	2024												2025												2026													
	January	February	March	April	May	June	July	August	September	October	November	December	January	February	March	April	May	June	July	August	September	October	November	December	January	February	March	April	May	June	July	August	September	October	November	December		
Preliminary Engineering Report																																						
Owner Review																																						
MAPS Presentation																																						
60% Final Plans for Review																																						
Owner Review																																						
ROW Acquisition and Utility Relocations																																						
95% Final Plans for Review																																						
Owner Review																																						
PS&E Submittal																																						
MAPS Presentation																																						
Bidding and Award																																						
Construction																																						

7.0 Conclusion and Recommendations

Garver performed field assessments, created preliminary alignments, and evaluated alternative options to best match the available M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A construction budget. MAPS 4 utilized the BikeWalkOKC Master Plan to identify the streets and which sides of the street sidewalk should be constructed for M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A. However, based on initial construction costs provided in Section 1.3 of this report, the full build out option (Option 1) of the M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A portion of the BikeWalkOKC Master Plan exceeded the MAPS 4 construction budget.

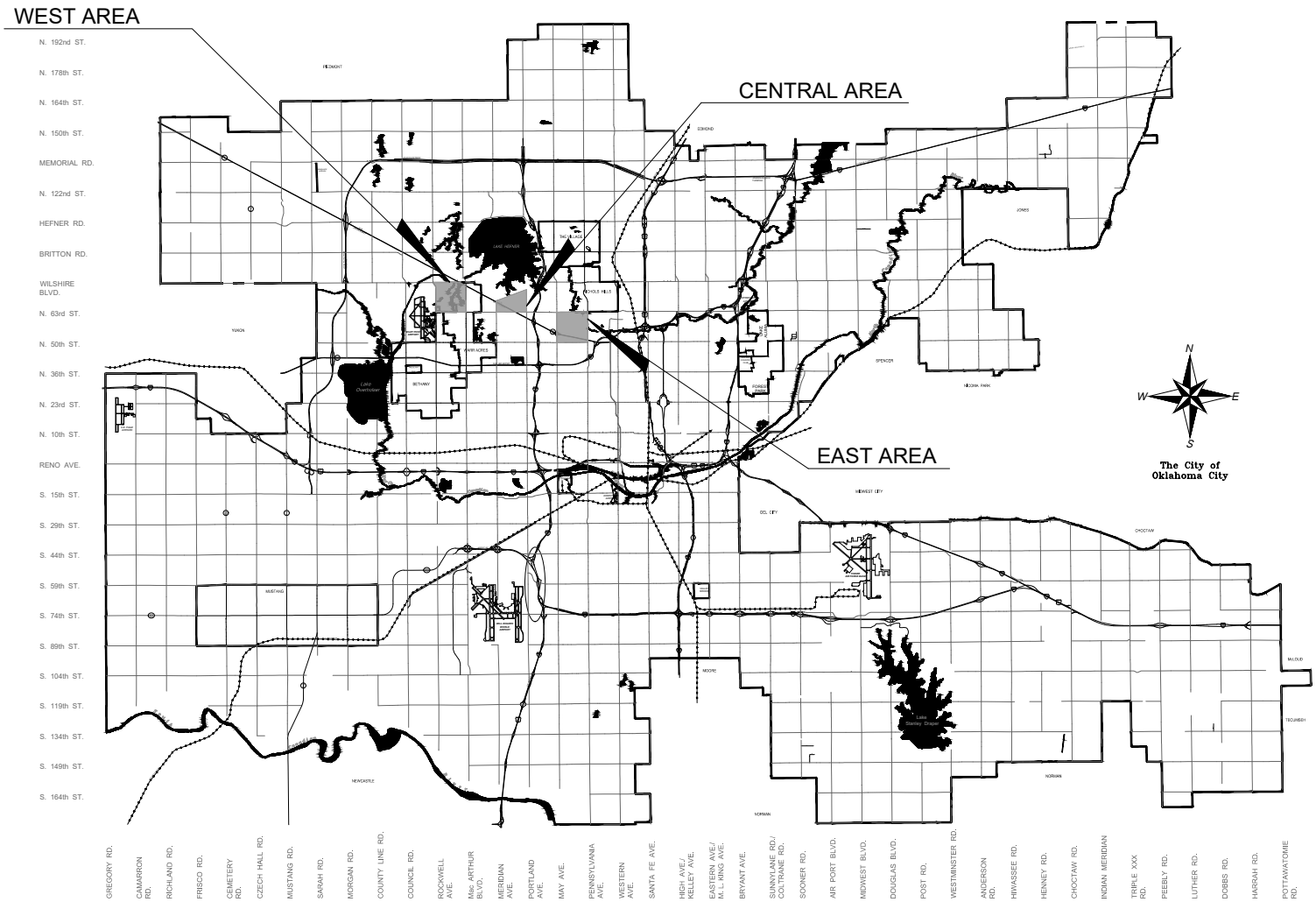
To address this challenge, Garver explored construction costs for an alternative option. A second design option (Option 2 (Recommended)) was developed that reduces the project extents to only one (1) side of each street to match the available funding for M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A. Although Option 2 (Recommended) is a reduction of the full build out option, it still achieves the goal of connecting parks, commercial areas, schools, and public transit stops to pedestrians while providing safer, ADA compliant travel routes.

Given that Option 2 is the only option that meets the established M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A budget, Garver recommends Option 2 (Recommended) for approval to move forward with final construction plans.



Appendix A

Preliminary Sidewalk Layout




MAPS 4
PROJECT M4-TS020
MAPS 4 SIDEWALKS AND RELATED AMENITIES
PHASE 2A
30% DESIGN
SEPTEMBER 2024



OWNER
City of Oklahoma City
MAPS
420 West Main Street, Suite 400
Oklahoma City, OK 73102
Phone: (405) 297-3461
Project Manager: KEVIN BROWN

CIVIL
GARVER
750 SW 24th St.
Suite 200
Moore, OK 73160
(405) 329-2555
OK COA #4193
EXPIRES 06/30/2024



The City of

OKLAHOMA CITY

Public Works Department

DAVID HOLT

Mayor

- COUNCIL MEMBERS
- BRADLEY CARTER

WARD 1
- JAMES COOPER

WARD 2
- BARBARA PECK

WARD 3
- TODD STONE

WARD 4
- MATT HINKLE

WARD 5
- JOBETH HAMON

WARD 6
- NIKKI NICE

WARD 7
- MARK K. STONECIPHER

WARD 8
- CRAIG FREEMAN

CITY MANAGER
- DEBBIE MILLER, P.E.

CITY ENGINEER

SHEET INDEX	
SHEET NO.	DESCRIPTION
0001	TITLE SHEET
0002-0004	LOCATION MAPS
0005-0015	SIDEWALK LAYOUT - NW 63RD AND MAY AVE.
0016-0017	SIDEWALK LAYOUT - NW EXPRESSWAY
0018-0021	SIDEWALK LAYOUT - LYREWOOD LANE

ONE CALL UTILITY LOCATION NUMBER

840-5032

1-800-522-6543

This number is to be used for information on the location of all underground utilities. Contact this number and other numbers specified in the plans prior to any excavation.

PREPARED BY

J. BRET CABBINESS, P.E.

NO. 18093

RECOMMENDED FOR APPROVAL

DAVID E. TODD, P.E.
MAPS PROGRAM MANAGER

DATE

DEBBIE MILLER, P.E., DIRECTOR
PUBLIC WORKS / CITY ENGINEER

DATE

APPROVED AS FINAL PLANS

MAYOR

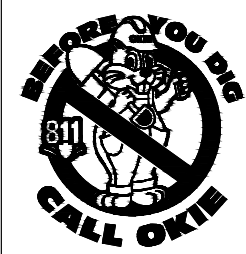
DATE

CITY CLERK

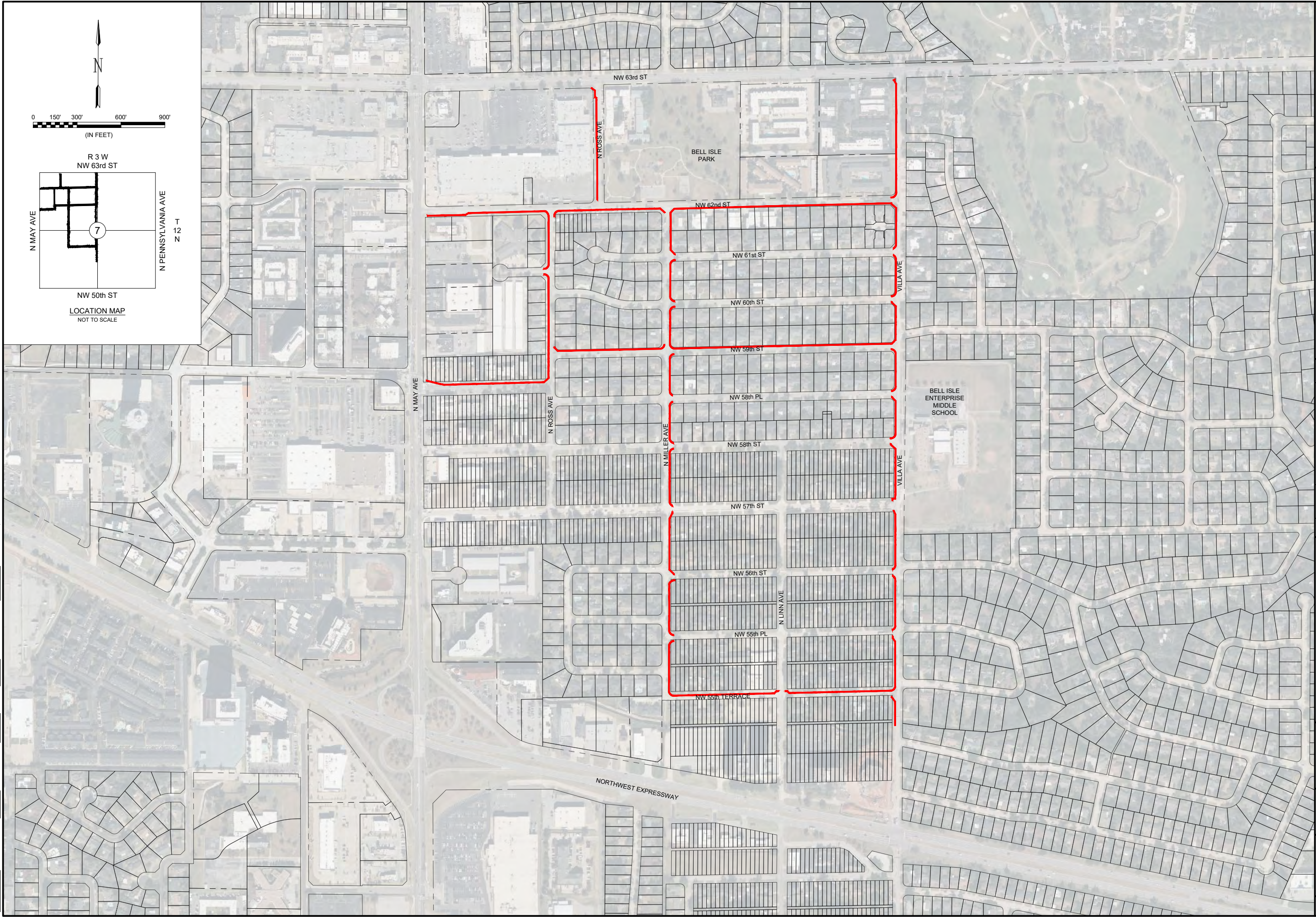
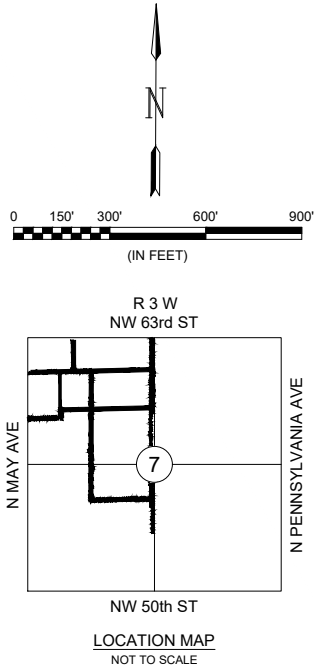
DATE


PROJECT NO. M4-TS020

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File: L:\2023\23T28050 - OKC MAPS 4 Sidewalks, Amenities PH 2A 2B\Drawings\23T28050 - Phase 2A Location Map.dwg Last Save: 9/23/2024 10:54 AM Last saved by: MStrahorn
Last plotted by: Strahorn, John M. Plot Style: ASBUILTS.ctb Plot Scale: 1:0.4255 Plot Date: 8/23/2024 11:47 AM Plotter used: DWG To PDF.pc3






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CA #4193 EXPIRES JUNE 30, 2026

PRELIMINARY REVIEW

REV.	DATE	DESCRIPTION	BY

CITY OF OKLAHOMA CITY
OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Location Map - NW 63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

BAR IS ONE INCH ON ORIGINAL DRAWING


0 1"

IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

SHEET
NUMBER 0002

File: L:\2023\23T28050 - OKC MAPS 4 Sidewalks Amenities PH 2A 2B\Drawings\23T28050 - Phase 2A Location Map.dwg Last Save: 9/23/2024 10:54 AM Last saved by: MStrahorn
Last plotted by: Strahorn, John M. Plot Style: ASBUILTS.ctb Plot Scale: 1:0.4255 Plot Date: 8/23/2024 11:56 AM Plotter used: DWG To PDF.pc3






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CITY OF OKLAHOMA CITY
OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Location Map - NW Expressway

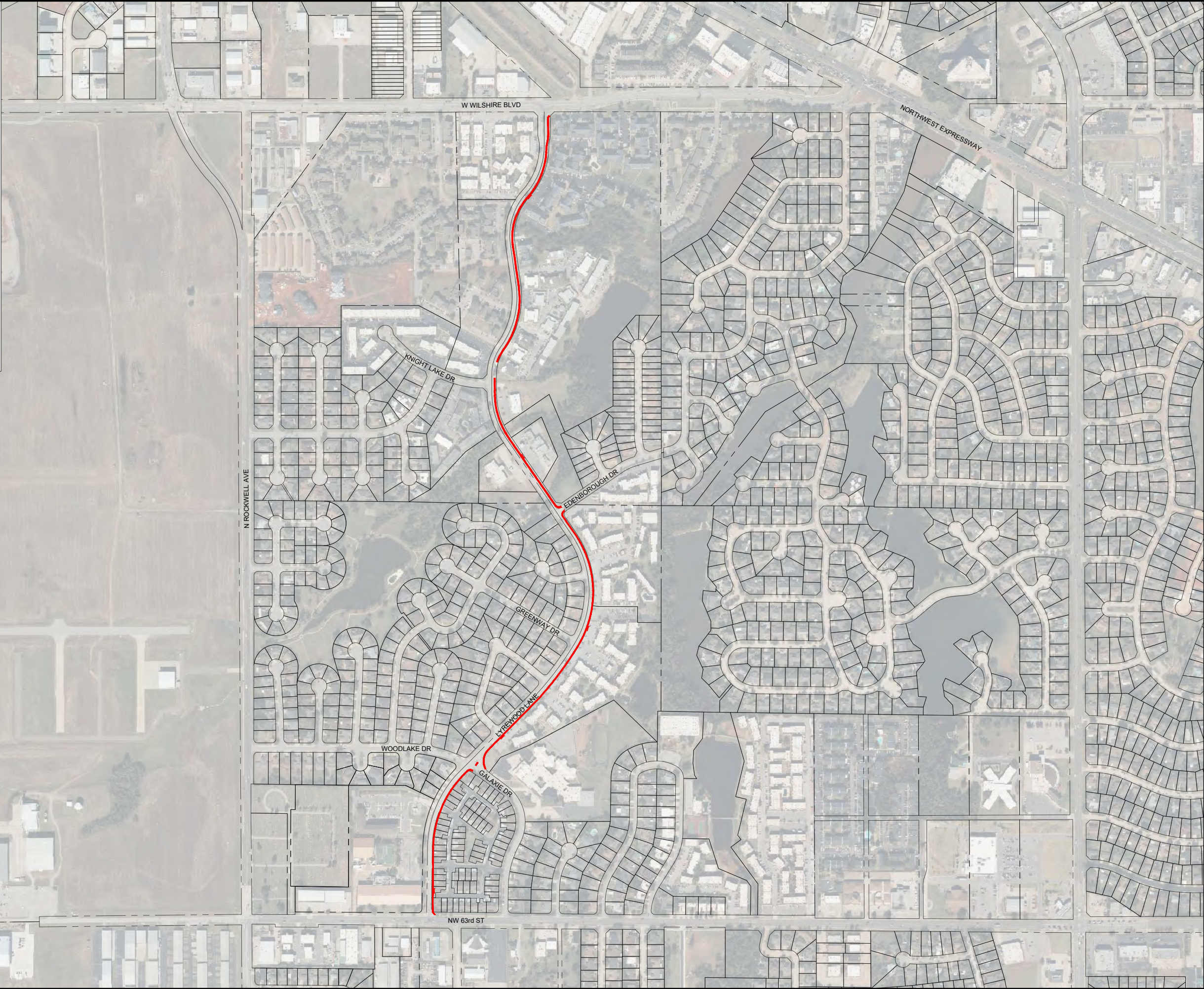
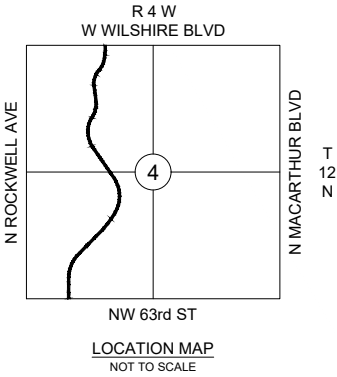
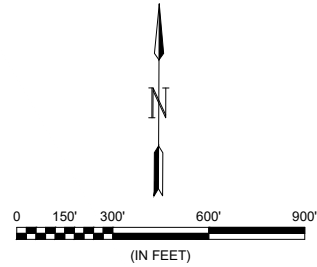
JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS


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SHEET NUMBER 0003

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Last plotted by: Strahorn, John M. Plot Size: ASBUILTS.ctb Plot Scale: 1:0.4255 Plot Date: 8/23/2024 11:47 AM Plotter used: DWG To PDF.pc3





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CITY OF OKLAHOMA CITY
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MAPS4 SIDEWALK & AMENITIES

Location Map -
Lyrewood Lane

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

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CITY OF OKLAHOMA CITY

OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050

DATE: SEP. 2024

DESIGNED BY: JMS
DRAWN BY: JHS

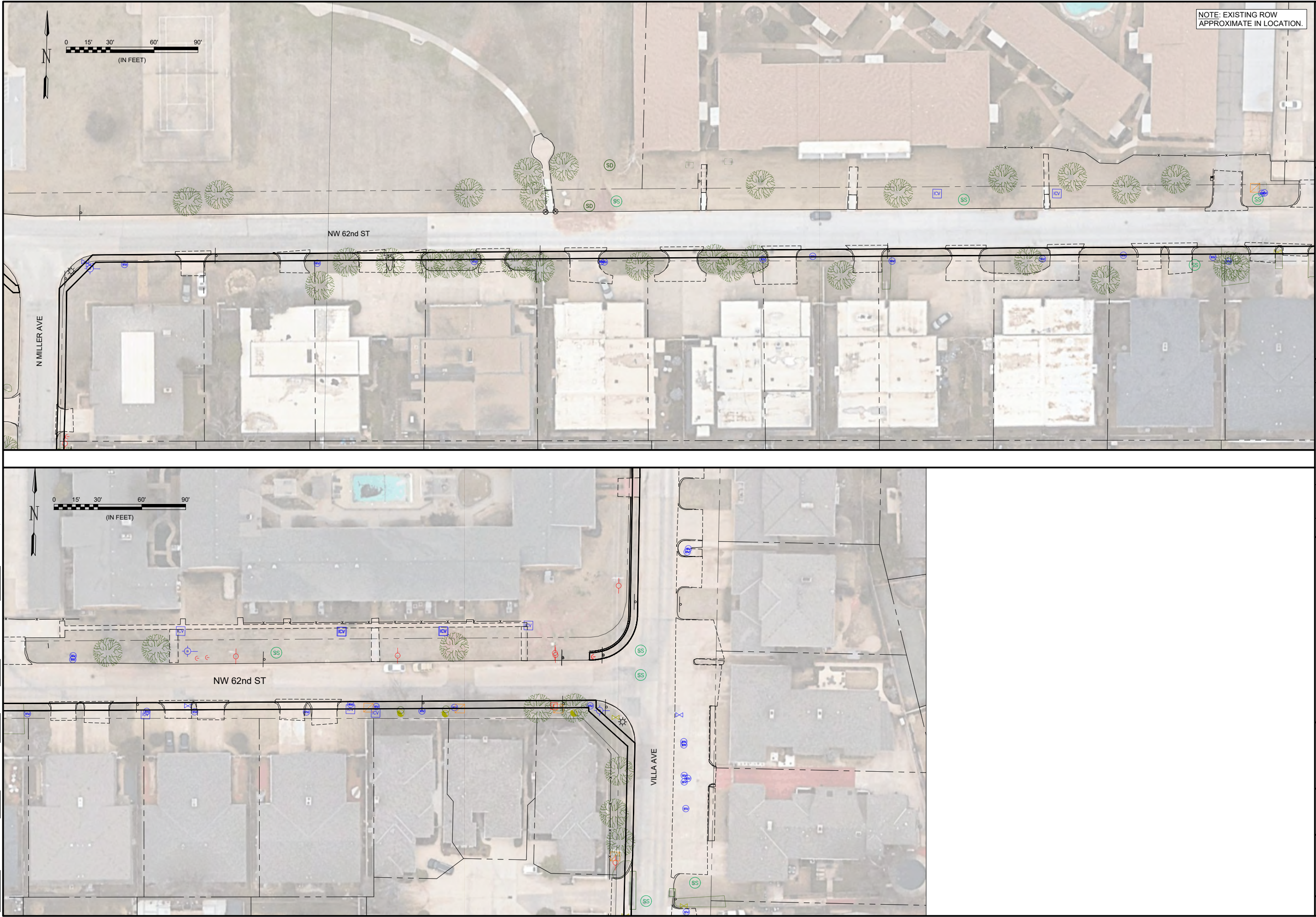
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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

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CITY OF OKLAHOMA CITY

OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050

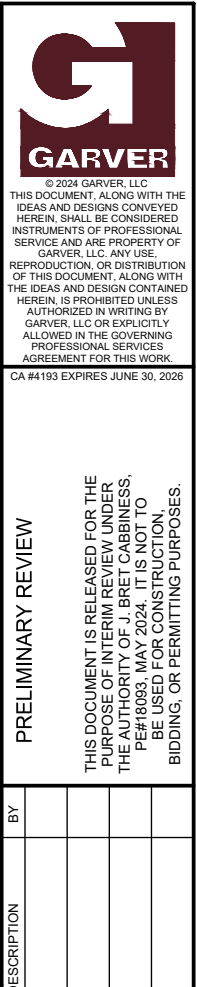
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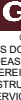
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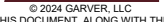
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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

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ORIGINAL DRAWING

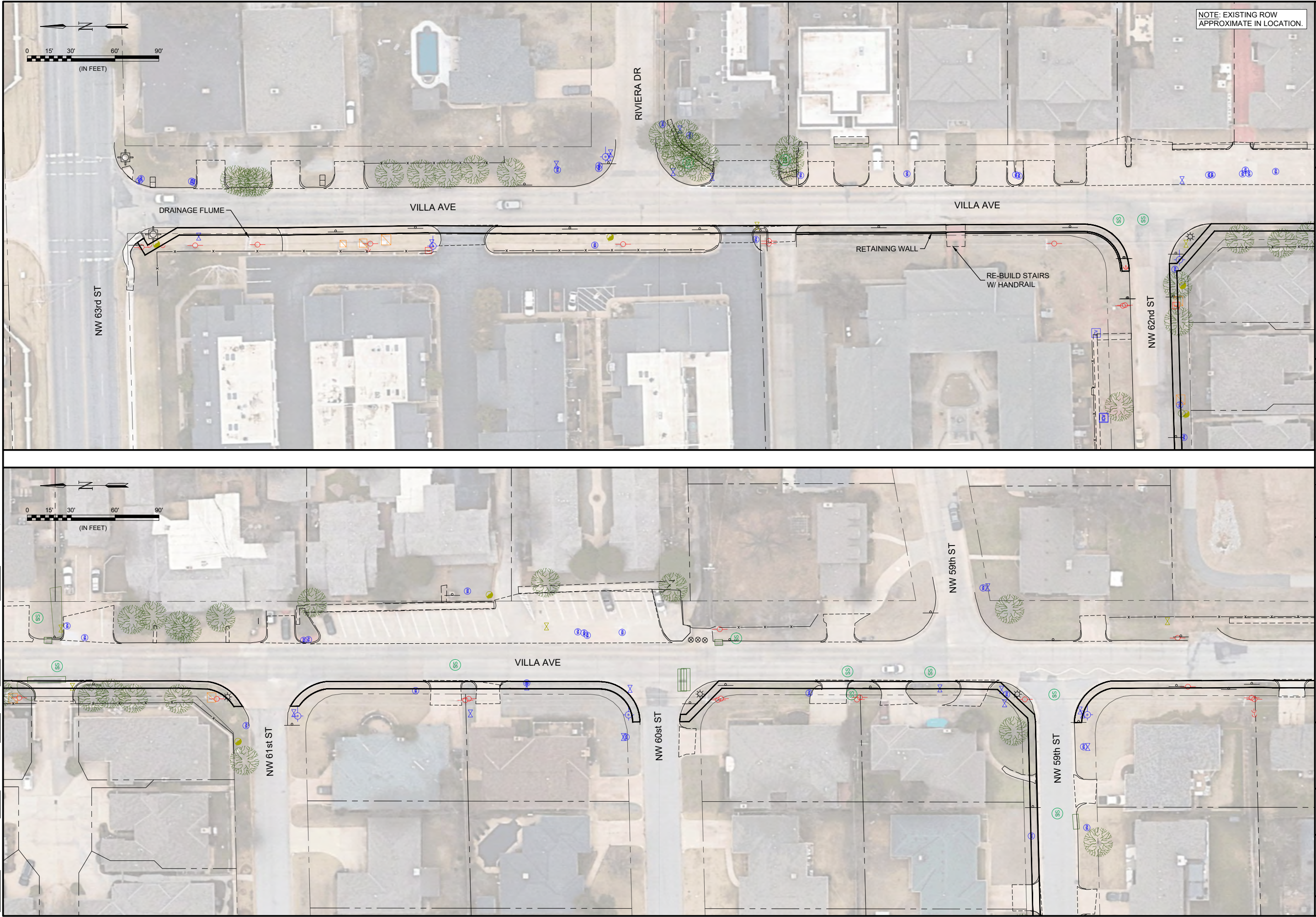
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Last plotted by: Strahorn, John M., Plot Style: ASBUILTS.ctb, Plot Scale: 1:0.4255, Plot Date: 8/23/2024 11:49 AM, Plotter used: DWG To PDF.pc3



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MAPS4

MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

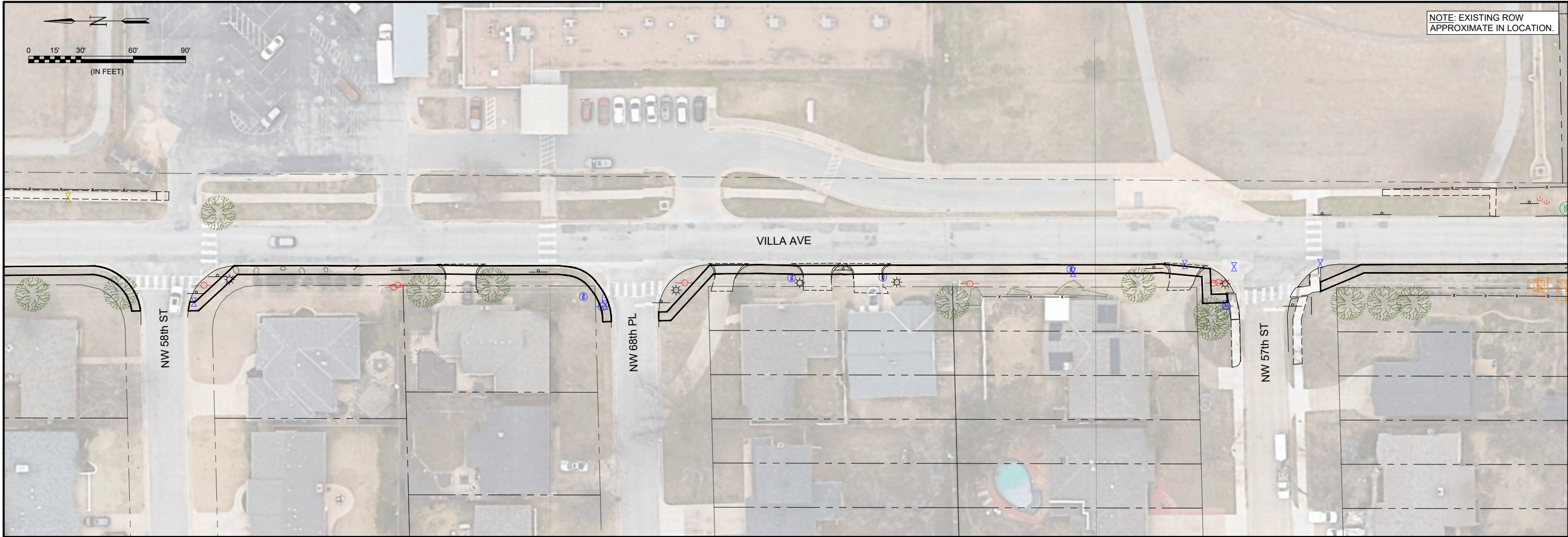
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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

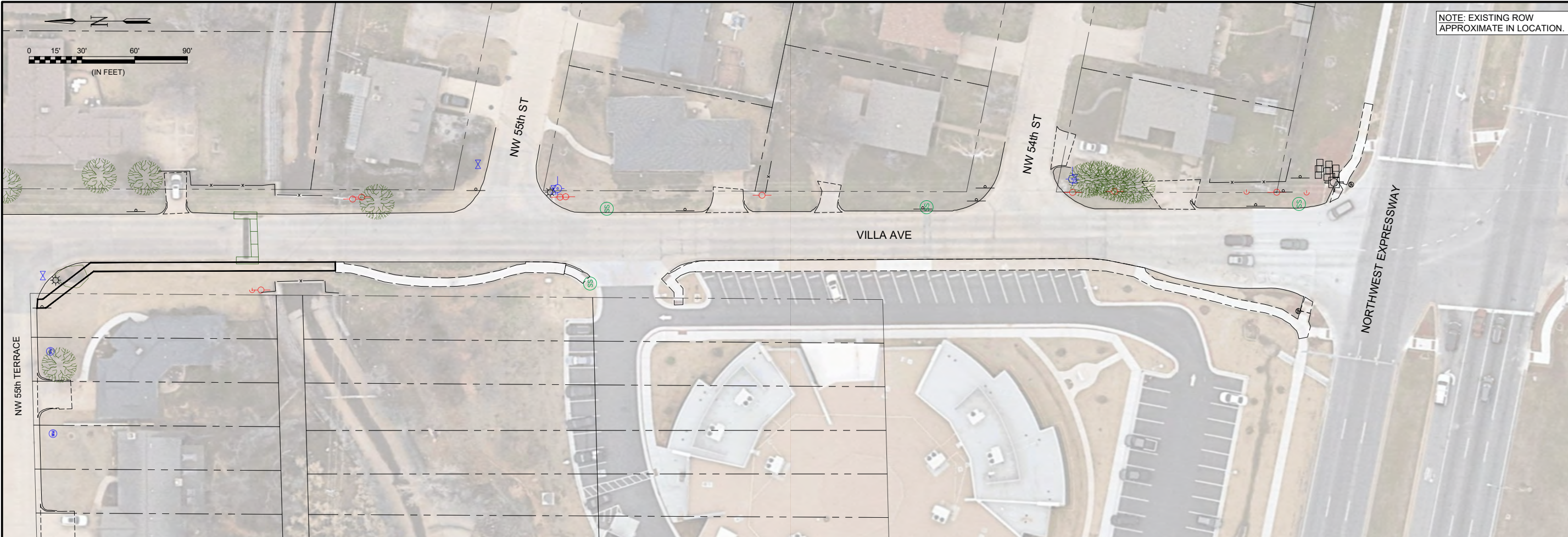
Sidewalk Layout - NW 63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

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MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW
63rd and May Ave.

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW Expressway

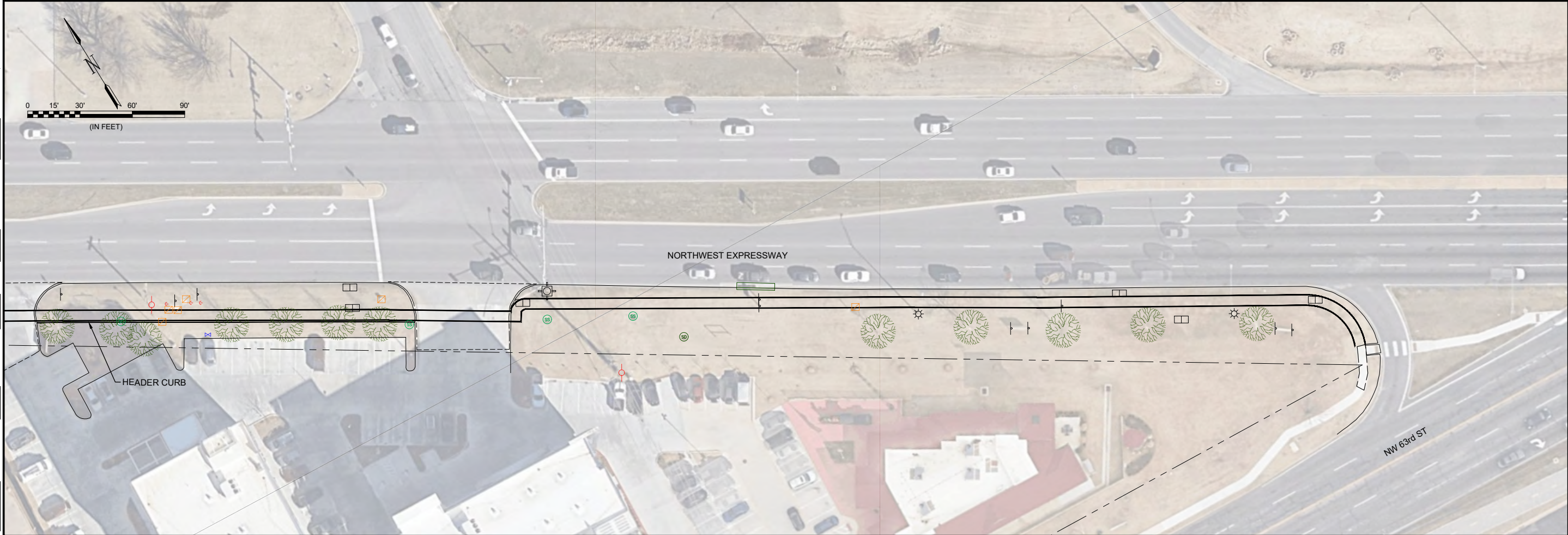
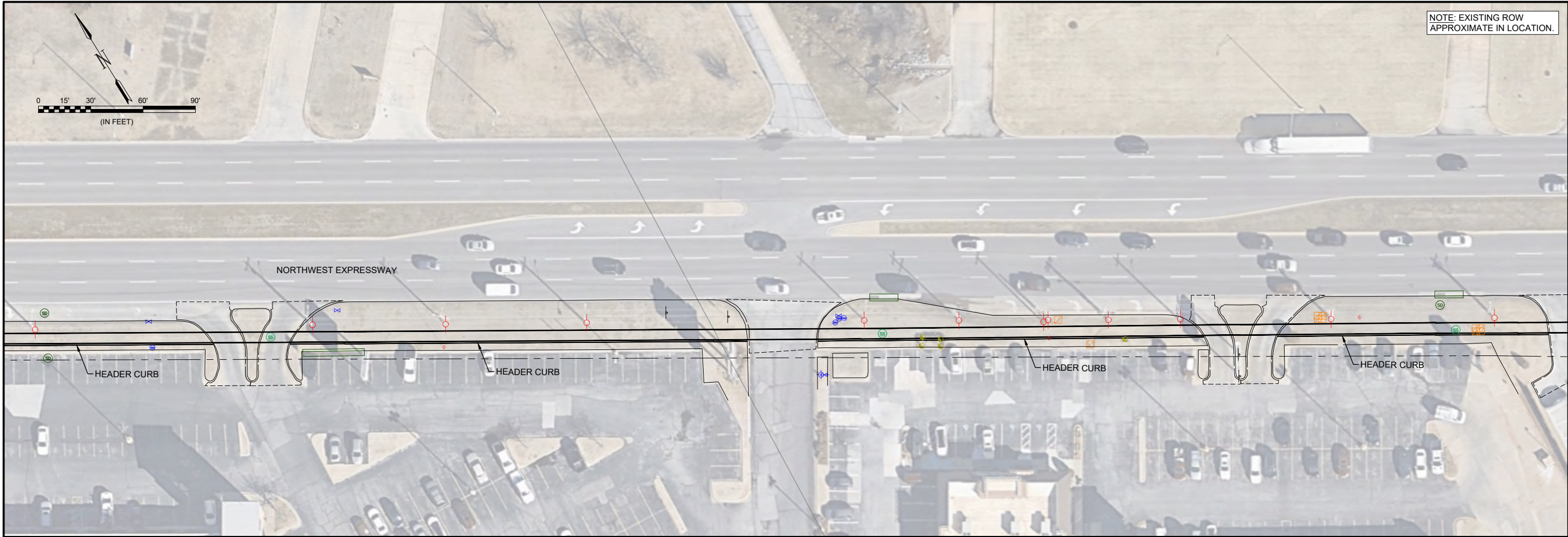
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DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS


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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout - NW Expressway

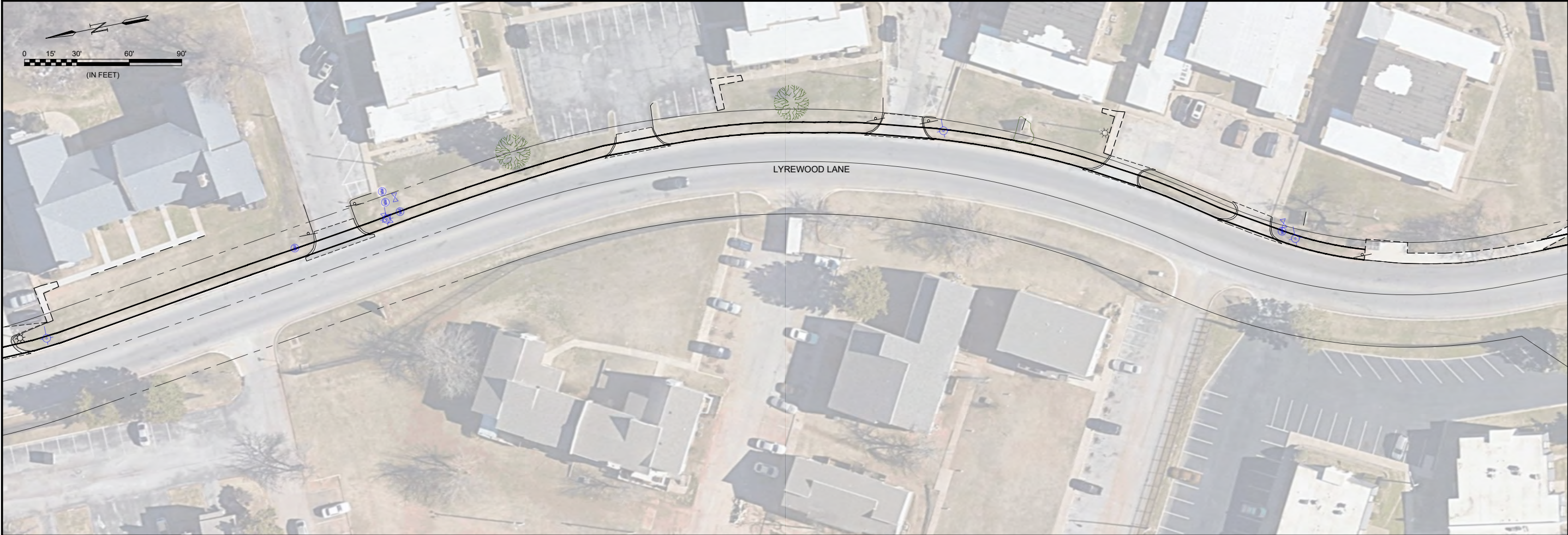
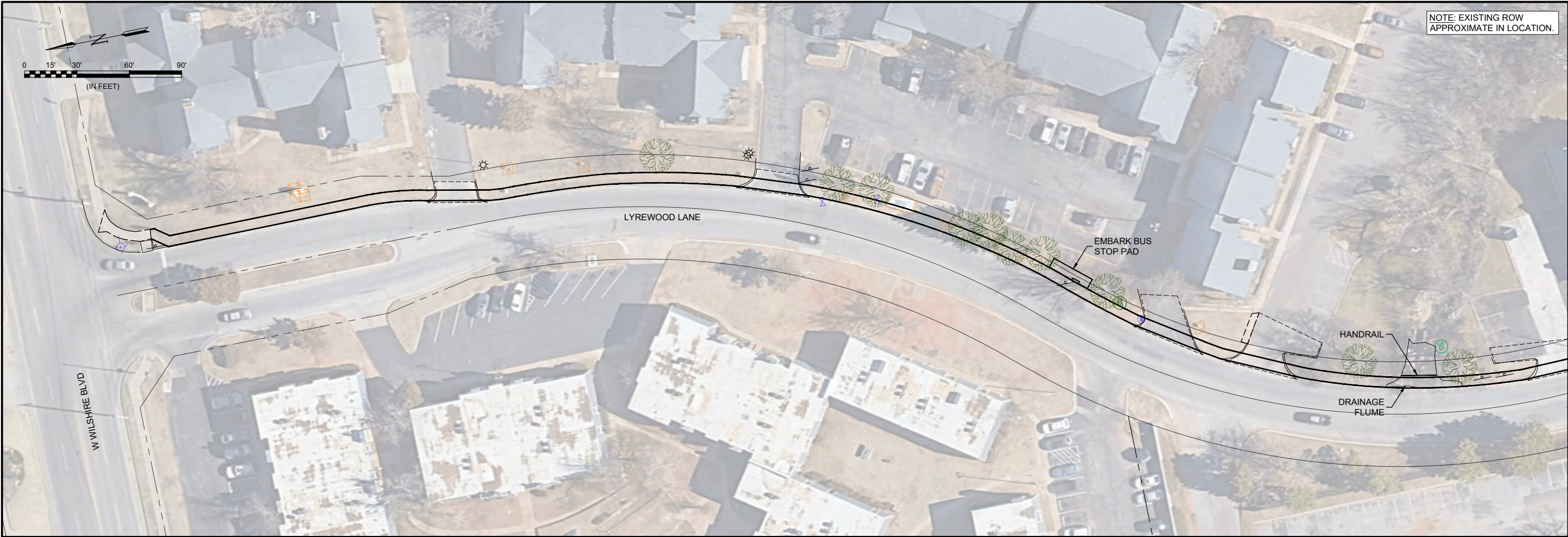
JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS


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OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout -
Lyrewood Lane

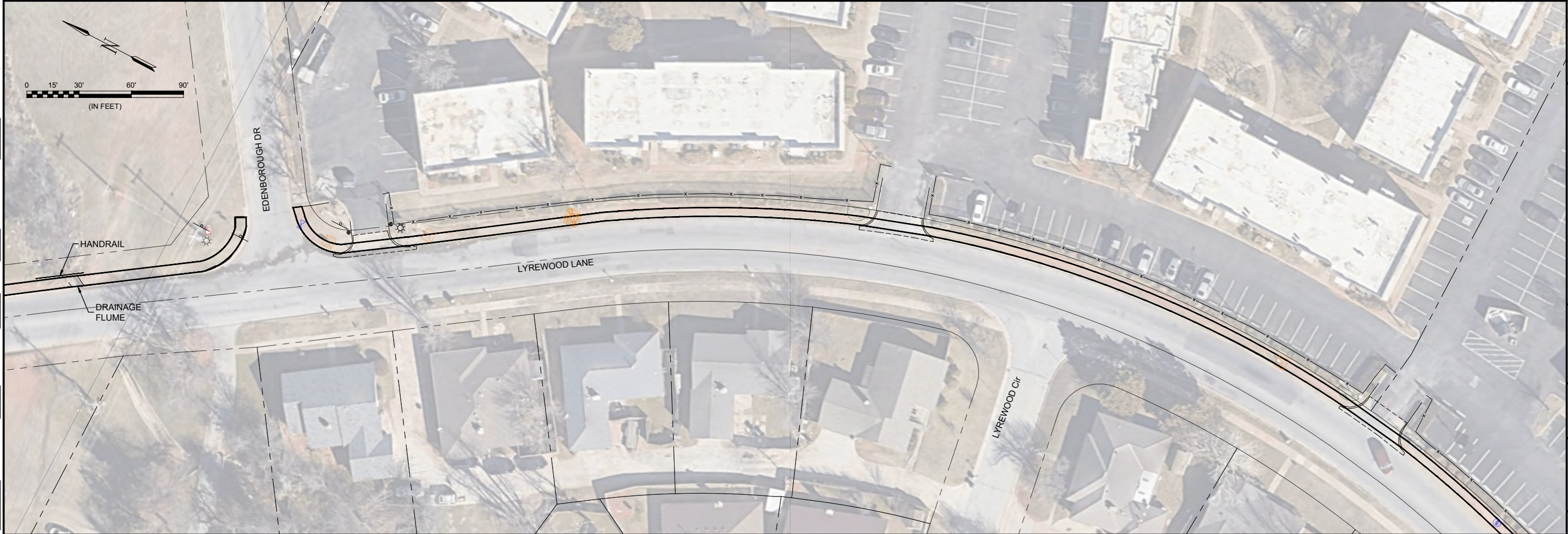
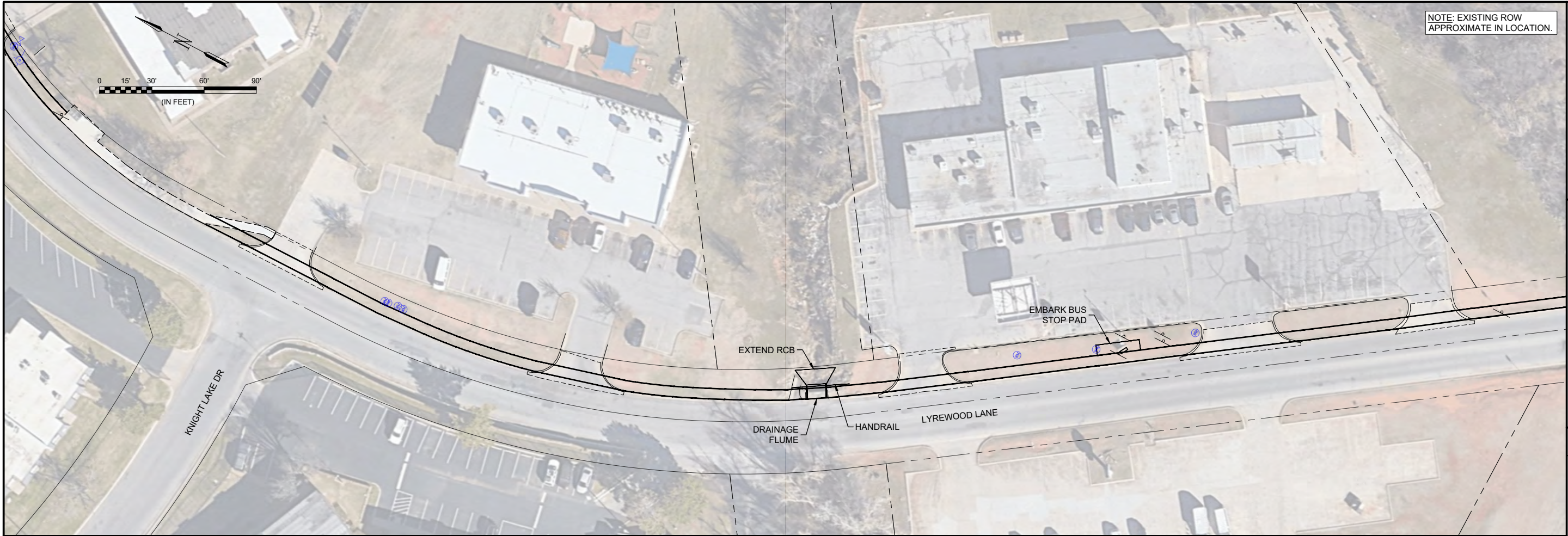
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DESIGNED BY: JMS
DRAWN BY: JHS


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Last plotted by: Strahom, John M. Plot Size: ASBUILTS.ctb Plot Date: 8/23/2024 11:51 AM Plotter used: DWG To PDF.pc3






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CITY OF OKLAHOMA CITY
OKLAHOMA CITY, OKLAHOMA



MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout -
Lyrewood Lane

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

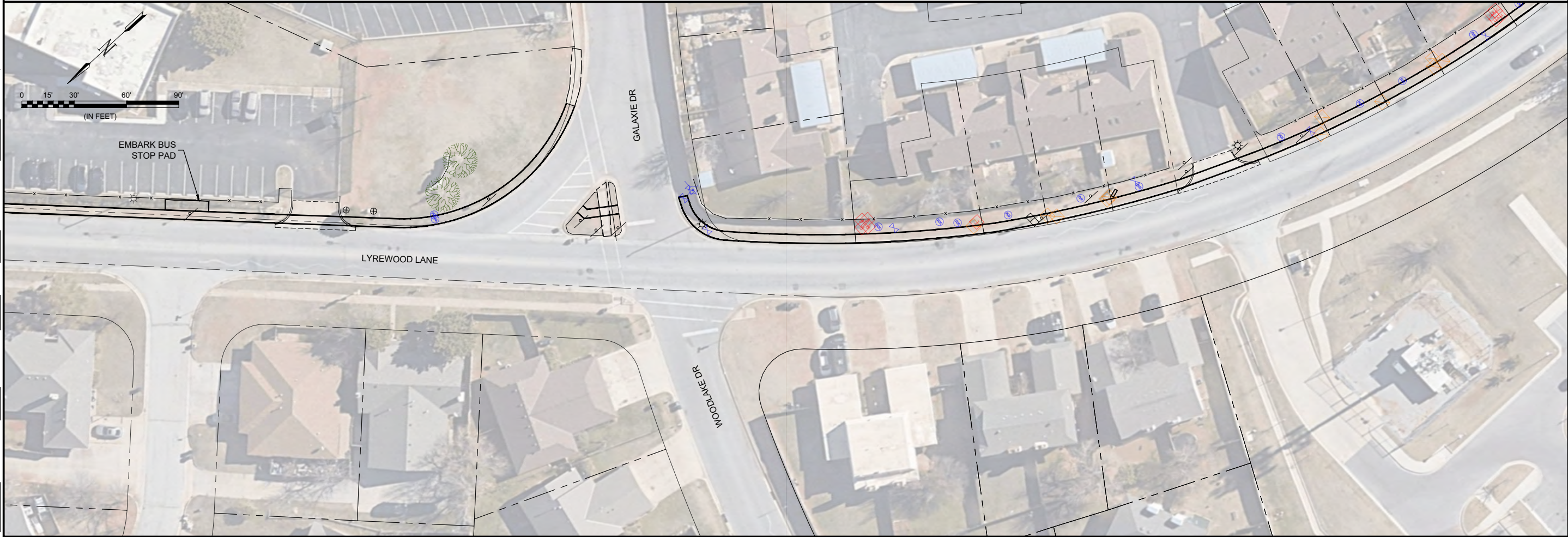
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OKLAHOMA CITY, OKLAHOMA




MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout -
Lyrewood Lane

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

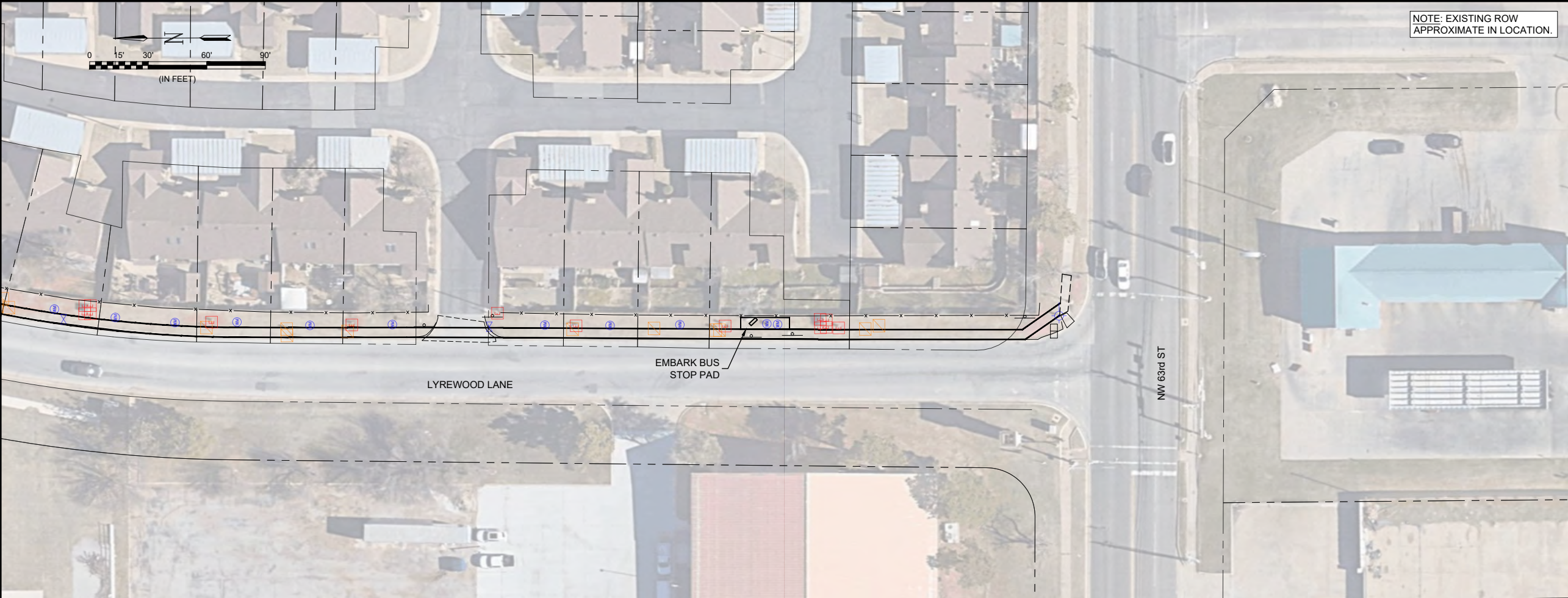
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SHEET
NUMBER 0020

File: L:\2023\23T28050 - OKC MAPS 4 Sidewalks Amenities PH 2A 2B\Drawings\23T28050 - Plan over Plan - Lyrewood.dwg Last Save: 8/23/2024 11:03 AM Last saved by: JMS\Strahorn
Last plotted by: Strahorn, John M. Plot Style: ASBUILTS.ctb Plot Scale: 1:0.4255 Plot Date: 8/23/2024 11:51 AM Plotter used: DWG To PDF.pc3



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CA #4193 EXPIRES JUNE 30, 2026

PRELIMINARY REVIEW
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REV.	DATE	DESCRIPTION	BY

CITY OF OKLAHOMA CITY
OKLAHOMA CITY, OKLAHOMA
MAPS4
MAPS4 SIDEWALK & AMENITIES

Sidewalk Layout -
Lyrewood Lane

JOB NO.: 23T28050
DATE: SEP. 2024
DESIGNED BY: JMS
DRAWN BY: JHS

BAR IS ONE INCH ON
ORIGINAL DRAWING
0 1"
IF NOT ONE INCH ON THIS SHEET,
ADJUST SCALES ACCORDINGLY.

SHEET
NUMBER 0021



Appendix B

Preliminary Cost Estimates



Summary of Preliminary Cost Estimates



Client: City of Oklahoma City
Project: Project M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A

Date: September 2024
Client Proj. No.: M4-TS020
Garver Proj. No.: 23T28050

MAPS 4 Sidewalks and Related Amenities Phase 2 Budget: \$7,724,000

Project M4-TS020 Phase 2A Budget: \$3,784,600.00

Project M4-TS021 Phase 2B Budget: \$3,939,400.00

Option 1

Full Build Out of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths

East Area (NW 63rd Street and May Avenue) - 5' Wide Sidewalk on 2 Sides of Road	\$4,066,451.40
West Area (Lyrewood Lane) - 6' Wide Sidewalk on 1 Side of Road	\$759,240.60
Central Area (NW Expressway) - 6' Wide Sidewalk on 1 Side of Road	\$634,982.40

Estimated Construction Total = \$5,460,674.40

Phase 2A Budget = \$3,784,600.00

Phase 2A Budget Balance = (\$1,676,074.40)

Option 2 (Recommended Option)

Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths

East Area (NW 63rd Street and May Avenue) - 5' Wide Sidewalk on 1 Side of Road	\$2,188,425.00
West Area (Lyrewood Lane) - 6' Wide Sidewalk on 1 Side of Road	\$759,240.60
Central Area (NW Expressway) - 6' Wide Sidewalk on 1 Side of Road	\$634,982.40

Estimated Construction Total = \$3,582,648.00

Phase 2A Budget = \$3,784,600.00

Phase 2A Budget Balance = \$201,952.00



Preliminary Cost Estimate

Option 2 (Recommended Option)
Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths



Client: City of Oklahoma City
Project: Project M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A
Extents: East Area - NW 63rd Street and May Avenue

Date: September 2024
Client Proj. No.: M4-TS020
Garver Proj. No.: 23T28050

Item	Spec.	Description	Unit	Quantity	Unit Price	Sub-Total Cost
1	200	Earthwork	L.SUM	1.00	\$ 25,000.00	\$ 25,000.00
2	305	Header Curb	L.F.	2,028.00	\$ 50.00	\$ 101,400.00
3	403	Retaining Wall	S.Y.	211.00	\$ 875.00	\$ 184,625.00
4	314	P.C. Concrete Pavement (Placement)	S.Y.	28.00	\$ 120.00	\$ 3,360.00
5	735	Traffic Stripe (Plastic)(24" Wide)(White)	L.F.	3,024.00	\$ 15.00	\$ 45,360.00
6	801	Construction Staking (Construction Survey)	LSUM	1.00	\$ 27,500.00	\$ 27,500.00
7	802	Construction Signing and Traffic Control	L.SUM	1.00	\$ 35,000.00	\$ 35,000.00
8	809	Mobilization	L.SUM	1.00	\$ 150,000.00	\$ 150,000.00
9	810	Clearing and Grubbing	L.SUM	1.00	\$ 55,000.00	\$ 55,000.00
10	812	Remove Concrete Pavement	S.Y.	42.00	\$ 12.00	\$ 504.00
11	813	Remove Driveway	S.Y.	2,924.00	\$ 12.00	\$ 35,088.00
12	819	Adjust Existing Structure (Meters)	EA.	82.00	\$ 500.00	\$ 41,000.00
13	819	Adjust Existing Structure (Hydrant)	EA.	7.00	\$ 5,000.00	\$ 35,000.00
14	819	Adjust Existing Structure (Valves)	EA.	30.00	\$ 500.00	\$ 15,000.00
15	819	Adjust Existing Structure (Manholes)	EA.	2.00	\$ 2,500.00	\$ 5,000.00
16	819	Adjust Existing Structure (Light Poles)	EA.	4.00	\$ 7,500.00	\$ 30,000.00
17	819	Adjust Existing Structure (Drainage Structures)	EA.	1.00	\$ 7,500.00	\$ 7,500.00
18	819	Remove and Relocate Sign	EA.	64.00	\$ 500.00	\$ 32,000.00
19	823	Concrete Sidewalk (4")	S.Y.	7,845.00	\$ 65.00	\$ 509,925.00
20	823	Concrete Driveway (6")	S.Y.	2,924.00	\$ 75.00	\$ 219,300.00
21	830	Tactile Markers/Truncated Domes	S.F.	960.00	\$ 35.00	\$ 33,600.00
22	840	Solid Slab Sodding	S.Y.	11,134.00	\$ 4.50	\$ 50,103.00
23	900	Rock Bag Inlet Barrier	L.F.	240.00	\$ 8.00	\$ 1,920.00
24	900	Filter Fabric Silt Fence - Complete in Place	L.F.	14,315.00	\$ 3.50	\$ 50,102.50
25	982	Pipe Railing	L.F.	652.00	\$ 200.00	\$ 130,400.00

Estimated Sub-Total Cost: \$ 1,823,687.50

20% Design Contingency: \$ 364,737.50

Estimated Project Total Cost: \$ 2,188,425.00



Preliminary Cost Estimate

Option 2 (Recommended Option)
Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths



Client: City of Oklahoma City
Project: Project M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A
Extents: West Area - Lyrewood Lane

Date: September 2024
Client Proj. No.: M4-TS020
Garver Proj. No.: 23T28050

Item	Spec.	Description	Unit	Quantity	Unit Price	Sub-Total Cost
1	200	Earthwork	L.SUM	1.00	\$ 5,000.00	\$ 5,000.00
2	305	Header Curb	L.F.	563.00	\$ 50.00	\$ 28,150.00
3	314	P.C. Concrete Pavement (Placement)	S.Y.	134.00	\$ 80.00	\$ 10,720.00
4	404	RCB Extension	L.SUM	1.00	\$ 60,000.00	\$ 60,000.00
5	735	Traffic Stripe (Plastic)(24" Wide)(White)	L.F.	263.00	\$ 15.00	\$ 3,945.00
6	801	Construction Staking (Construction Survey)	LSUM	1.00	\$ 12,500.00	\$ 12,500.00
7	802	Construction Signing and Traffic Control	L.SUM	1.00	\$ 20,000.00	\$ 20,000.00
8	809	Mobilization	L.SUM	1.00	\$ 50,000.00	\$ 50,000.00
9	810	Clearing and Grubbing	L.SUM	1.00	\$ 25,000.00	\$ 25,000.00
10	812	Remove Sidewalk	S.Y.	22.00	\$ 10.00	\$ 220.00
11	813	Remove Driveway	S.Y.	734.00	\$ 12.00	\$ 8,808.00
12	819	Adjust Existing Structure (Meters)	EA.	16.00	\$ 500.00	\$ 8,000.00
13	819	Adjust Existing Structure (Hydrant)	EA.	5.00	\$ 5,000.00	\$ 25,000.00
14	819	Adjust Existing Structure (Valves)	EA.	10.00	\$ 500.00	\$ 5,000.00
15	819	Adjust Existing Structure (Manholes)	EA.	1.00	\$ 2,500.00	\$ 2,500.00
16	819	Adjust Existing Structure (Pullbox)	EA.	1.00	\$ 1,000.00	\$ 1,000.00
17	819	Remove and Relocate Sign	EA.	16.00	\$ 500.00	\$ 8,000.00
18	823	Concrete Sidewalk (4")	S.Y.	3,748.00	\$ 65.00	\$ 243,620.00
19	823	Concrete Driveway (6")	S.Y.	734.00	\$ 75.00	\$ 55,050.00
20	830	Tactile Markers/Truncated Domes	S.F.	120.00	\$ 35.00	\$ 4,200.00
21	840	Solid Slab Sodding	S.Y.	4,372.00	\$ 4.50	\$ 19,674.00
22	900	Rock Bag Inlet Barrier	L.F.	80.00	\$ 8.00	\$ 640.00
23	900	Filter Fabric Silt Fence - Complete in Place	L.F.	5,621.00	\$ 3.50	\$ 19,673.50
24	982	Pipe Railing	L.F.	80.00	\$ 200.00	\$ 16,000.00

Estimated Sub-Total Cost: \$ 632,700.50

20% Design Contingency: \$ 126,540.10

Estimated Project Total Cost: \$ 759,240.60



Preliminary Cost Estimate

Option 2 (Recommended Option)

Reduced Extents of BikeWalkOKC Master Plan w/ Maximum Sidewalk Widths



Client: City of Oklahoma City
Project: Project M4-TS020 MAPS 4 Sidewalks and Related Amenities Phase 2A
Extents: Central Area - NW Expressway

Date: September 2024
Client Proj. No.: M4-TS020
Garver Proj. No.: 23T28050

Item	Spec.	Description	Unit	Quantity	Unit Price	Sub-Total Cost
1	200	Earthwork	L.SUM	1.00	\$ 10,000.00	\$ 10,000.00
2	305	Header Curb	L.F.	1,000.00	\$ 50.00	\$ 50,000.00
3	403	Retaining Wall	S.Y.	40.00	\$ 875.00	\$ 35,000.00
4	451	Reinforced Concrete Pipe (30")	L.F.	20.00	\$ 160.00	\$ 3,200.00
5	451	Reinforced Concrete Pipe (36")	L.F.	50.00	\$ 200.00	\$ 10,000.00
6	451	Culvert End Section (30")	EA.	1.00	\$ 1,500.00	\$ 1,500.00
7	451	Culvert End Section (36")	EA.	1.00	\$ 2,500.00	\$ 2,500.00
8	735	Traffic Stripe (Plastic)(24" Wide)(White)	L.F.	924.00	\$ 15.00	\$ 13,860.00
9	801	Construction Staking (Construction Survey)	LSUM	1.00	\$ 12,500.00	\$ 12,500.00
10	802	Construction Signing and Traffic Control	L.SUM	1.00	\$ 30,000.00	\$ 30,000.00
11	809	Mobilization	L.SUM	1.00	\$ 50,000.00	\$ 50,000.00
12	810	Clearing and Grubbing	L.SUM	1.00	\$ 5,000.00	\$ 5,000.00
13	813	Remove Driveway	S.Y.	587.00	\$ 12.00	\$ 7,044.00
14	819	Adjust Existing Structure (Meters)	EA.	5.00	\$ 500.00	\$ 2,500.00
15	819	Adjust Existing Structure (Valves)	EA.	3.00	\$ 500.00	\$ 1,500.00
16	819	Adjust Existing Structure (Manholes)	EA.	6.00	\$ 2,500.00	\$ 15,000.00
17	819	Adjust Existing Structure (Pullbox)	EA.	2.00	\$ 1,000.00	\$ 2,000.00
18	819	Adjust Existing Structure (Drainage Structures)	EA.	2.00	\$ 7,500.00	\$ 15,000.00
19	819	Remove and Relocate Sign	EA.	10.00	\$ 500.00	\$ 5,000.00
20	823	Concrete Sidewalk (4")	S.Y.	2,221.00	\$ 65.00	\$ 144,365.00
21	823	Concrete Driveway (6")	S.Y.	587.00	\$ 75.00	\$ 44,025.00
22	830	Tactile Markers/Truncated Domes	S.F.	240.00	\$ 35.00	\$ 8,400.00
23	840	Solid Slab Sodding	S.Y.	2,591.00	\$ 4.50	\$ 11,659.50
24	900	Rock Bag Inlet Barrier	L.F.	180.00	\$ 8.00	\$ 1,440.00
25	900	Filter Fabric Silt Fence - Complete in Place	L.F.	3,331.00	\$ 3.50	\$ 11,658.50
26	982	Pipe Railing	L.F.	180.00	\$ 200.00	\$ 36,000.00

Estimated Sub-Total Cost: \$ 529,152.00

20% Design Contingency: \$ 105,830.40

Estimated Project Total Cost: \$ 634,982.40