

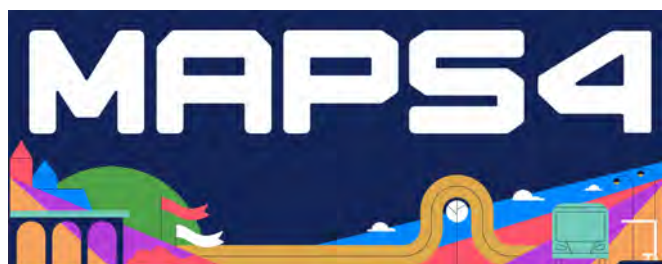


The City of
OKLAHOMA CITY

PRELIMINARY REPORT FOR

PROJECT M4-TS011 MAPS 4 SIDEWALKS AND RELATED AMENITIES PHASE 1B

Prepared for:



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THE CITY OF OKLAHOMA CITY
APPROVAL SHEET

PROJECT M4-TS011

MAPS 4 SIDEWALKS AND
RELATED AMENITIES PHASE 1B

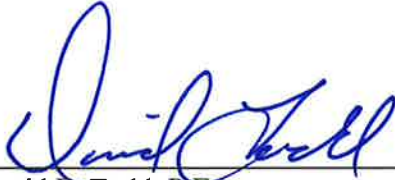
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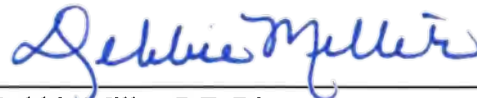
Grady Wade, P.E.



Recommended for Approval by:



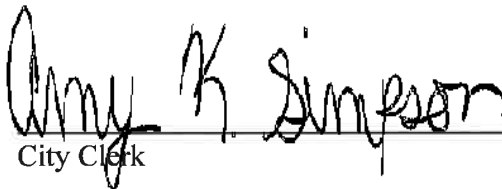
David E. Todd, P.E.
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Debbie Miller, P.E. Director
Public Works/City Engineer

APPROVED by the Council of the City of Oklahoma City this 9TH day of APRIL, 2024.
ATTEST:

THE CITY OF OKLAHOMA CITY



City Clerk



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Overview

It is the overall intention of City of Oklahoma City (OKC) to provide pedestrian and related amenity improvements throughout the City limits. Connectivity improvements were originally identified in the BikeWalkOKC plan and related community studies. This project and phase, M4-TS011 Phase 1B, will construct pedestrian upgrades in various locations spread out across southwest Oklahoma City. These locations are situated around Western Avenue near Integris Southwest Medical Center, Pennsylvania Avenue near Grant High School, and Far South OKC near Westmoore High School. Thus, the City of Oklahoma City's MAPS 4 program contracted with Smith Roberts Baldischwiler, LLC (SRB) to study these corridors and develop conceptual plans.

Design aspects to be considered and presented in this report include sidewalk construction, signalized intersection upgrades, pavement markings and signage, and other amenities which could include trees, wayfinding, landscaping, and drainage. Incidental technical elements such as topographical survey recommendations, public and private utility relocations, appropriate traffic control measures and devices, and identification of Right-of-Way acquisition needs to achieve the conceptual design will also be established and discussed.

The primary consideration for these improvements is to improve accessibility, promote walkability, and increase vehicular and pedestrian safety. This project's intention is to avoid roadway reconstruction and instead retrofit existing conditions as much as feasible to minimize costs, mitigate traffic disruptions, and alleviate hardship for nearby property owners.

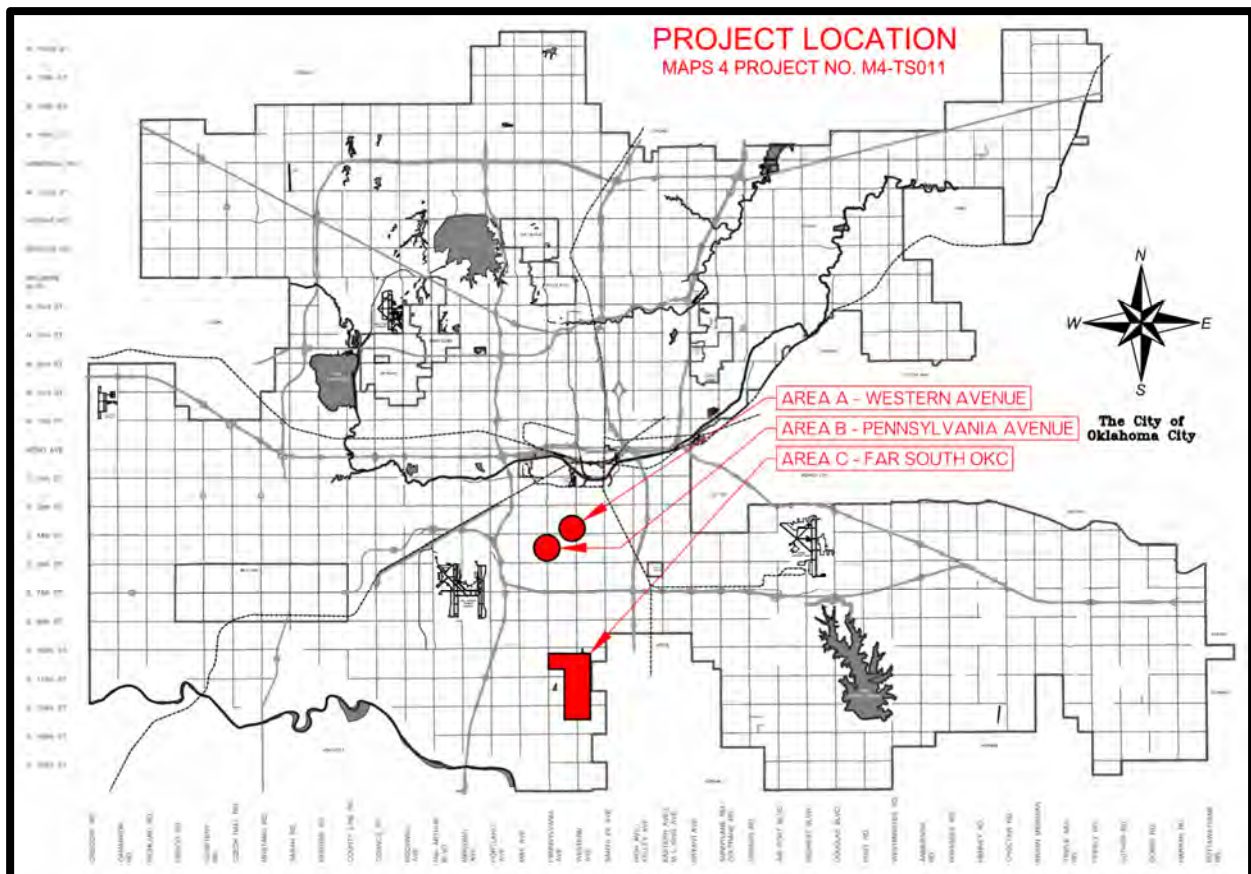


Figure 1: Project Location Map

Project Budget

- Phase 1B Construction Budget \$3,871,500
- Engineer's Opinion of Probable Construction Costs \$3,450,000

Project Schedule

The following project schedule has been developed to document the timeline of the project to date and the expected timeline for completion of the various milestones.

Task No.	Task Name	Duration (Calendar Days)	Start	Finish	2023 A S O N D	2024 J F M A M J J A S O N D	2025 J F M A M J
0	M4-TS011 Sidewalks & Related Amenities, Phase 1B	473	1/19/2023	6/7/2025			
1	Notice to Proceed	0	8/16/2023	8/16/2023			
2	30% Plans & Preliminary Engineering Report	120	8/16/2023	12/15/2023			
3	30% & PER Approval	90	12/15/2023	3/14/2024			
3	60% Plans and Review	40	3/14/2024	4/23/2024			
4	95% Plans and Review	40	4/23/2024	6/2/2024			
5	Final Engineering Drawings and Specifications (100%)	40	6/2/2024	7/12/2024			
6	Bidding	30	7/12/2024	8/11/2024			
7	Project Construction	300	8/11/2024	6/7/2025			

Figure 2: Project Schedule

Recommendation

SRB recommends proceeding with the design of the MAPS 4 Sidewalks and Related Amenities Phase 1B project.

M4-TS011 Sidewalks and Related Amenities Phase 1B	
Advantages	Disadvantages
✓ Increases walkability and safety	✓ Possible utility coordination and relocation
✓ Modernizes traffic signals and intersections	✓ Possible right-of-way acquisitions
✓ Within construction budget	✓ Traffic delays due to construction
✓ Improves or constructs over 32,000-feet of accessible pedestrian routes	

Figure 3: Decision Matrix

Sidewalk Improvements Locations

M4-TS011 extents of work for pedestrian hardscape improvements were segmented into three groupings based upon similar proximities classified hereafter as Areas A through C.

Area A – Western Avenue

This area encompasses five alignments situated around Western Avenue generally north of S.W. 44th St. and south of Grand Blvd: S.W. Grand Boulevard, S.W. 38th Street, S.W. 39th Street/Olie Avenue, S.W. 41st Street, and Douglas Avenue.

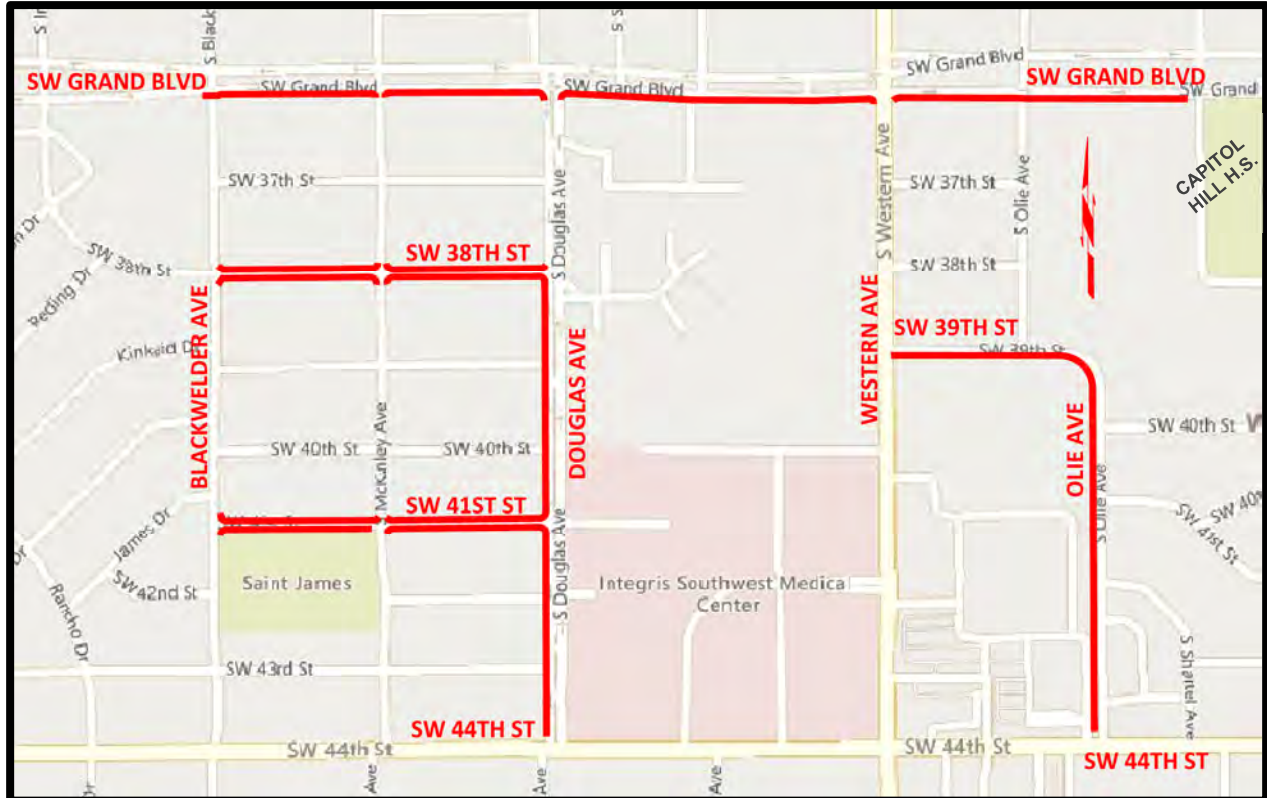


Figure 4: Project Scope, Area A (Western Ave.)

Area B – Pennsylvania Avenue

This area includes two alignments near Pennsylvania Avenue: S.W. 51st and 55th Streets.



Figure 5: Project Scope, Area B (Pennsylvania Ave.)

Area C – Far South OKC

This area encompasses various alignments near the southern City limits. This area is defined generally as Kingsridge Drive, Western Avenue, and S.W. 134th Street:

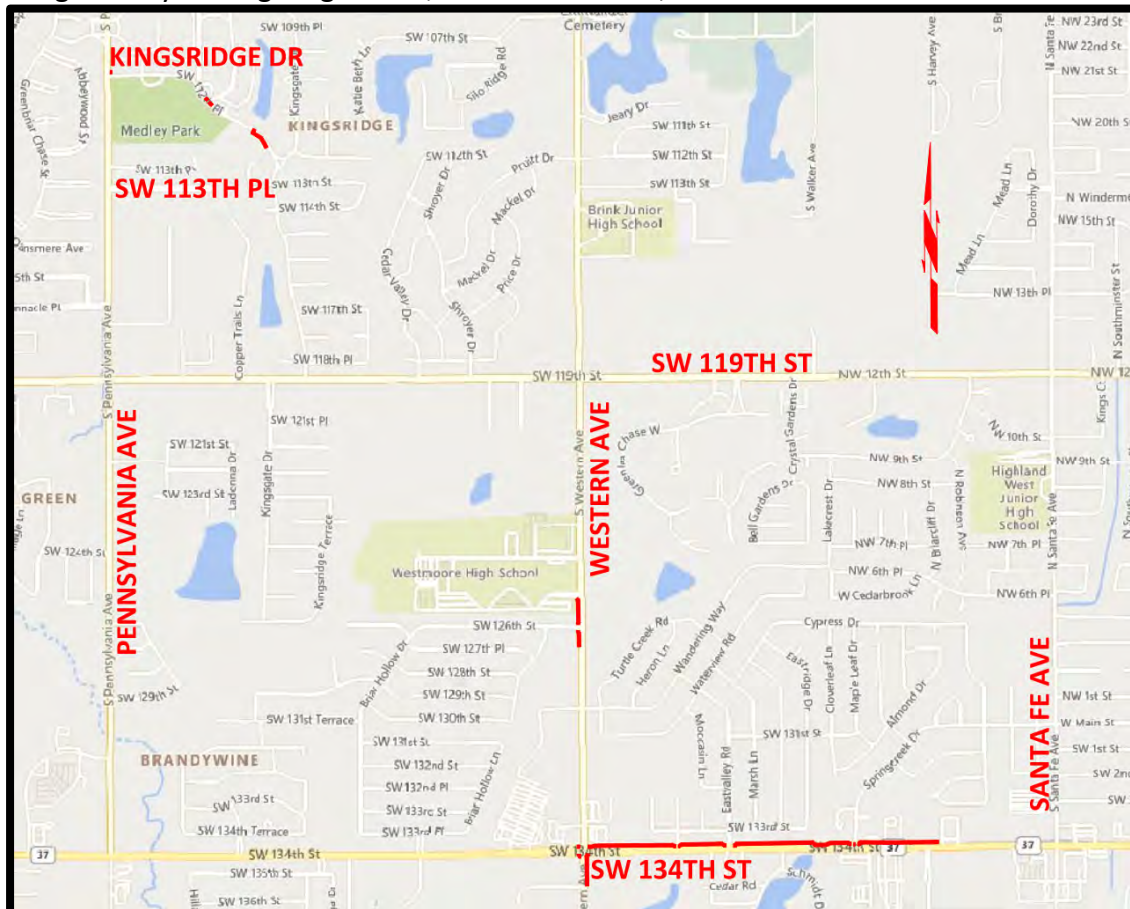


Figure 6: Project Scope, Area C (Far South OKC)



Scope of Work

Smith Roberts Baldischwiler (SRB) was contracted to develop construction plans for pedestrian improvements projects from project conception through construction completion under M4-TS011 Phase 1B. This project's understanding is for the intended improvements of sidewalk, accessible routes, pedestrian signal crossings, pavement markings and crossings, signs, and other amenities which could include trees, placemaking, landscaping, drainage, or art. The following tasks were formally contracted with Oklahoma City's MAPS 4 for M4-TS011 Phase 1B:

TASK 1B: Preliminary Engineering Services

SRB shall prepare a preliminary design report and construction plans that encompass the extents of the project. In this endeavor, the Engineer shall coordinate with MAPS and other City departments, review existing conditions, evaluate right-of-way, and provide a preliminary construction cost estimate. The Engineer must attend and participate in any Committee, Citizen Boards, City Council, and any other group as requested by the Program Manager.

TASK 2A: Final Plan Services

SRB will produce final plans, specifications, construction cost estimates, and all forms and other documentation necessary for advertisement for letting and bidding. This task shall include topographical surveys up to 25% of the total linear footage of the project, preparation of right-of-way documents, assistance in right-of-way acquisitions, and execution of utility coordination meetings. In addition, the Engineer will hold all necessary coordination and meetings with MAPS Committees, Citizen Boards, City Council, all known utilities companies and entities with facilities affected by the proposed project, and all other interested parties.

TASK 3A: Project Bidding

SRB will assist in the facilitation of the bidding and advertisement process, including but not limited to the attendance of any pre-bid meetings, active response to any requested coordination or meeting with the City and its representatives, tabulate and review all bids received, and review and make recommendations of reward of construction contract.

TASK 4A: Construction Administration Services and Representation

SRB will administer limited construction contract and represent the City in monitoring the construction project until final payment is made to the Contractor. In this endeavor, the Engineer will assist in any requested meeting or question by the City or the Contractor, help in the coordination of pre-work conferences, review and comment on all requests for information, furnish any plan changes as agreed by the City and/or Contractor, stay familiar with the progress and quality of work, audit pay claims, review change orders and amendments, maintain its own record and conduct period site reviews, take action on the approval or rejection of work that does not conform Bidding Documents, and compile a log of all created or received documents.

TASK 5A: As-Built Plans

Upon project completion and acceptance by the City, SRB will correct the original drawings and show all as-built changes based on information from Contractor red line plans, any relevant field changes and/or plan adaptations during construction, and conditions gleaned from site visitations, if necessary. SRB will submit electronic AutoCAD drawing files and GPS permanent benchmarks with the as-built drawings.

Area A – Western Avenue

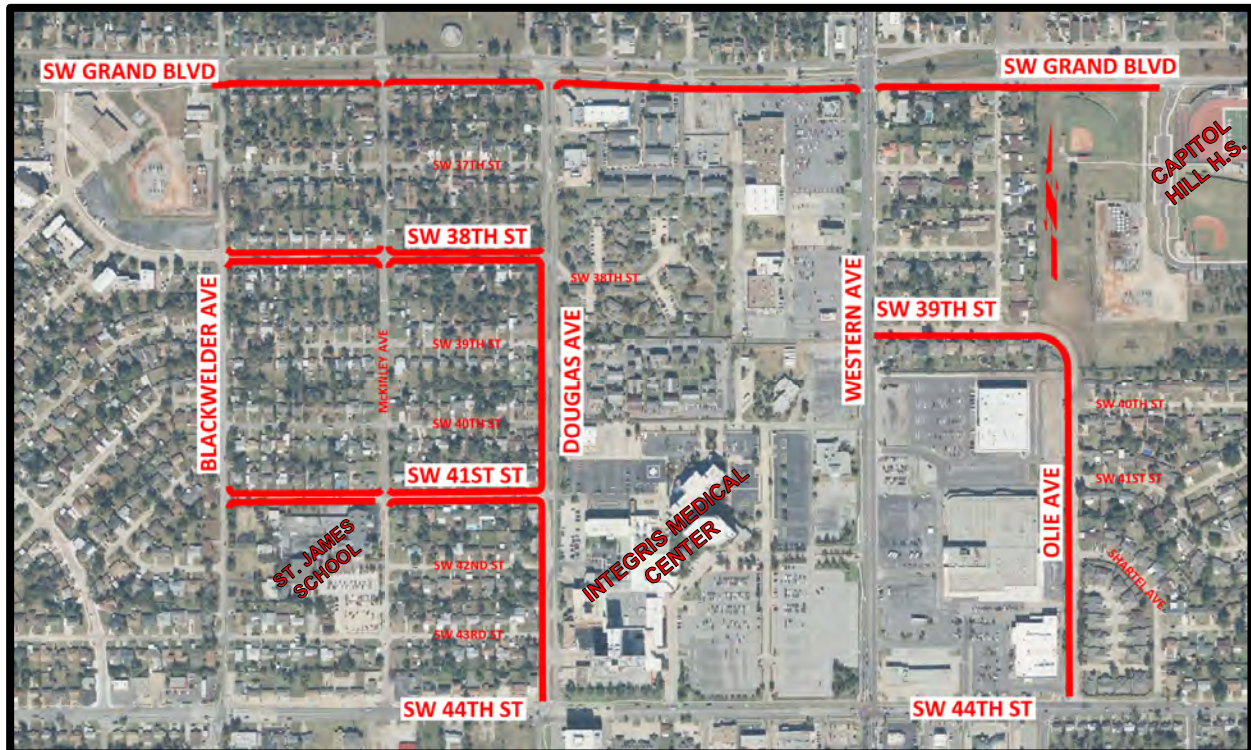


Figure 7: Aerial Scope, Area A (Western Ave.)

Existing Conditions

Area A is comprised of five alignments situated near Western Avenue in the neighborhoods of Watchful Eyes and Western Hills: S.W. Grand Boulevard, S.W. 38th Street, S.W. 39th Street/Olie Avenue, S.W. 41st Street, and Douglas Avenue. The roadways in this scope have two-driving lanes with a posted speed limit of 25-mph, except for S.W. Grand Boulevard and Douglas Avenue which have slightly higher speed limits of 35-mph and contain a median. All roadways in the area are classified as neighborhood roadways except for S.W. Grand Boulevard which is classified as a connector. Western Avenue and S.W. 44th Street are classified as major arterial roadways, however this project intends to simply start or end at these roadways by connecting to existing pedestrian hardscapes. Generally, Area A roadways service residential homes, however, there are some additional school and/or commercial uses along S.W. Grand Boulevard, Douglas Avenue, and Olie Avenue.

There is an obvious need for pedestrian improvements for these roadways. No continuous pedestrian routes currently exists along these roadway alignments with a singular exception for the multi-use trail on the median of S.W. Grand Boulevard. Connectivity improvements are necessary to provide additional pedestrian pathways to the existing South Grand Trail, existing schools, parks, and the Southwest Integris Medical Center in this scope. In addition, the Southwest Integris Medical Center area has specialized “transit-oriented” zoning focused on higher levels of transportation connectivity according to PlanOKC.org, and the construction of sidewalk improvements per this project will synergize well with any defined transit hub.



Figure 8: Existing Conditions, Area A (Western Ave.)

Right-of-Way

Preliminary investigations reveal this area with varied right-of-way widths. S.W. Grand Boulevard is a significant, wandering connector roadway with diverse zoning and use cases resulting in a complicated corridor with a right-of-way ranging from 110-feet to 200-feet. S.W. 38th and 41st Streets are residential roadways based on 60-foot right-of-ways. S.W. 39th Street and Olie Avenue are based upon a 50-foot right-of-way. Lastly, Douglas Avenue is platted within a 90-foot right-of-way.

SRB believes that sidewalk improvements can be installed in Area A without any additional acquisitions except for one possible instance along S.W. Grand Boulevard. We anticipate Grand Boulevard may lack sufficient right-of-way between Klein and Western Avenue where the eastbound roadway meanders south and was constructed very close to the property lines with southern commercial developments. Several acquisitions along the southern extents and some incidental commercial parking lot reconstruction may be required to properly construct a

pedestrian pathway, however SRB recommends that this area is first included as part of the topological survey requests to accurately define the property lines and the locations of existing hardscapes and other features. The summary of likely, proposed easement acquisitions will be discussed in more detail in the *Identification of Required Survey* report section.



Figure 9: Right-of-Way Concerns, Area A (Western Ave.)

Roadway	Typology	Typ. Right-of-Way	Description
S.W Grand Blvd.	Connector	110'-120'	R/W Verification suggested
S.W. 38 th St.	Neighborhood	60'	No issue
S.W. 39 th St./Olie Ave.	Neighborhood	50'	No issue
S.W. 41 st St.	Neighborhood	60'	No issue
Douglas Ave.	Neighborhood	90'	No issue

Table 1: Right-of-Way Summary, Area A (Western Ave.)

Signalized Intersections

There are two existing signalized intersections along this scope:

1. S.W. Grand Blvd. & Western Avenue
2. S.W. 44th Street & Douglas Avenue

Both junctions are currently ADA-compliant and have recently been improved with ramps, landings, and pedestrian crossing equipment. The first intersection at S.W. Grand Blvd. & Western Avenue has compliant hardscapes and modern equipment, thus no significant alteration is proposed. The second intersection at S.W. 44th Street & Douglas Avenue was recently improved, however the northbound and southbound signals have outdated truss style mast arms and older detection systems. We suggest the poles and detection systems be updated per current City standards. In both cases, the intention will be to limit hardscape reconstruction and to simply connect to existing pedestrian routes with minimal disturbances.



Figure 10: Signalized Intersections, Area A (Western Ave.)

Key Challenges

Modernization of these five alignments will be generally straight-forward and concern free except for several challenging areas. These challenges are encountered on the S.W. Grand Boulevard, S.W. 39th Street/Olie Avenue, and S.W. 41st Street alignments in the form of right-of-way concerns, physical conflicts or obstructions, steep grades, pedestrian route connectivity, and other design nuances.

Regarding S.W. Grand Boulevard, we identified possible insufficient right-of-way between Klein and Western Avenue, explained previously in this section. The roadway was constructed very close to the southern property lines, in addition a dedicated right turn lane for the commercial development was installed which further conflicts with existing right-of-way. Retrofitting this location with a pedestrian route may involve property acquisitions and parking lot reconstruction. We intend to include this area as part of our topographical survey requests to accurately verify and design this segment.

Regarding S.W. 39th Street/Olie Avenue, excessively steep slopes exist on the north/east side where the two roadways converge. The existing fields to the northeast are several feet higher than the roadway and providing a continuous sidewalk would require at least 400-feet of retaining wall. SRB proposes to avoid the need for costly retaining walls and instead install the pedestrian route on the south/west side of the roadway instead where it is flatter and has fewer intersections. Along Olie Avenue, utility poles, flumes, and steep grades exists which complicate potential designs. We suggest that the west-side of Olie Avenue is included as part of the topographical survey requests to accurately design.

Regarding S.W. 41st Street, four large curb inlets exist on each radius of the intersection with McKinley Avenue. In addition, utility poles, fire hydrants, and other utilities exist at this location as well. The position of each radial inlet and other existing utilities will influence where and how the curb ramps and crossings are proposed for this intersection. The goal for this intersection will be to minimize storm sewer network alternations and utility relocations when feasible. Therefore, SRB suggests that this intersection is included as part of the topographical survey requests to reliably layout new sidewalks.



Figure 11: Grand Blvd. (Looking East)



Figure 12: Grand Blvd. (Looking West)



Figure 13: S.W. 39th St. & Olie Ave.



Figure 14: Olie Avenue (West Side)



Figure 15: 41st St & McKinley Ave.
(Looking Southwest)

Area B – Pennsylvania Avenue



Figure 16: Aerial Scope, Area B (Pennsylvania Ave.)

Existing Conditions

Area B is comprised of two alignments near Pennsylvania Avenue: S.W. 51st Street and S.W. 55th Street. Both roadways have a posted speed limit of 25-mph and contain two driving lanes. 51st Street is classified as a connector roadway and 55th Street is classified as a neighborhood street. Pennsylvania Avenue itself is a major arterial and both roadway alignments either cross or tie to this corridor. Area B roadways are primarily residential roads, however both alignments also include public interests such as schools, apartments, and links to larger arterial corridors.

There is a need for pedestrian improvements along these alignments. No continuous sidewalk routes exist along these roadways despite several schools and various commercial businesses in close proximity.



Figure 17: Existing Conditions, Area B (Pennsylvania Ave.)

Right-of-Way

Investigations reveal Area B as standard residential streetscapes and right-of-ways. Both S.W. 51st and 55th Streets are mostly based upon 50-foot right-of-ways with one exception along S.W. 55th Street between Villa Avenue and Youngs Boulevard where the right-of-way widens to 55-feet. SRB believes that sidewalk improvements can be installed along these streets without any additional acquisitions due to plentiful space availability within existing right-of-way.

Key Challenges

Area B's singular challenging situation will be at northwest quadrant S.W. 51st Street & Kentucky Avenue. A recently constructed softball field dugout and protective bollards were erected closely along S.W. 51st Street's northern curb line leaving insufficient room for a City-compliant sidewalk without significant alterations. To provide an accessible route through this area, removal of the protective bollards with some street and curb replacement would in addition to topographical survey be necessary to improve this segment.

Instead of alterations to the roadway and brand new athletic facility hardscapes plus costly surveying, SRB proposes instead that no sidewalk be installed in this brief area and to focus on connectivity on the south side of S.W. 51st Street instead.

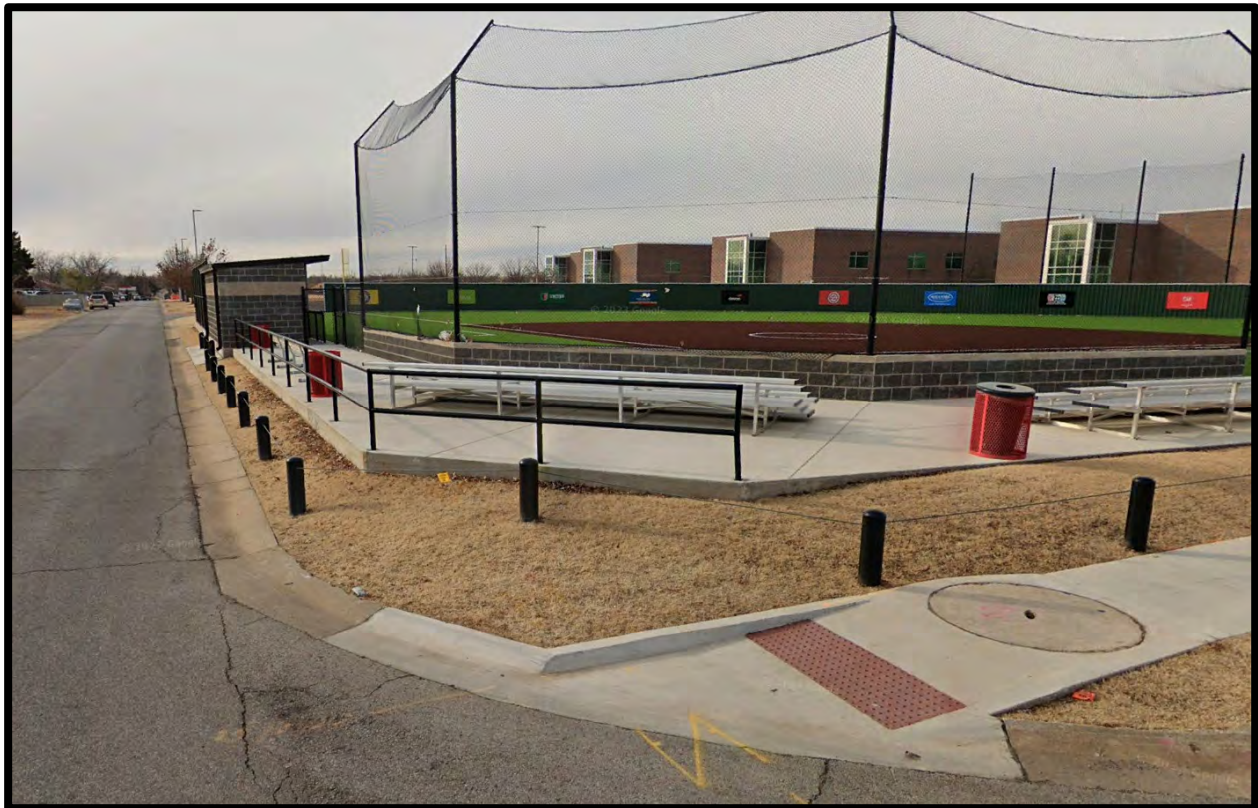


Figure 18: N.W. Quadrant of S.W. 51st St. & Kentucky Ave., Area B (Pennsylvania Ave.)

Area C – Far South OKC

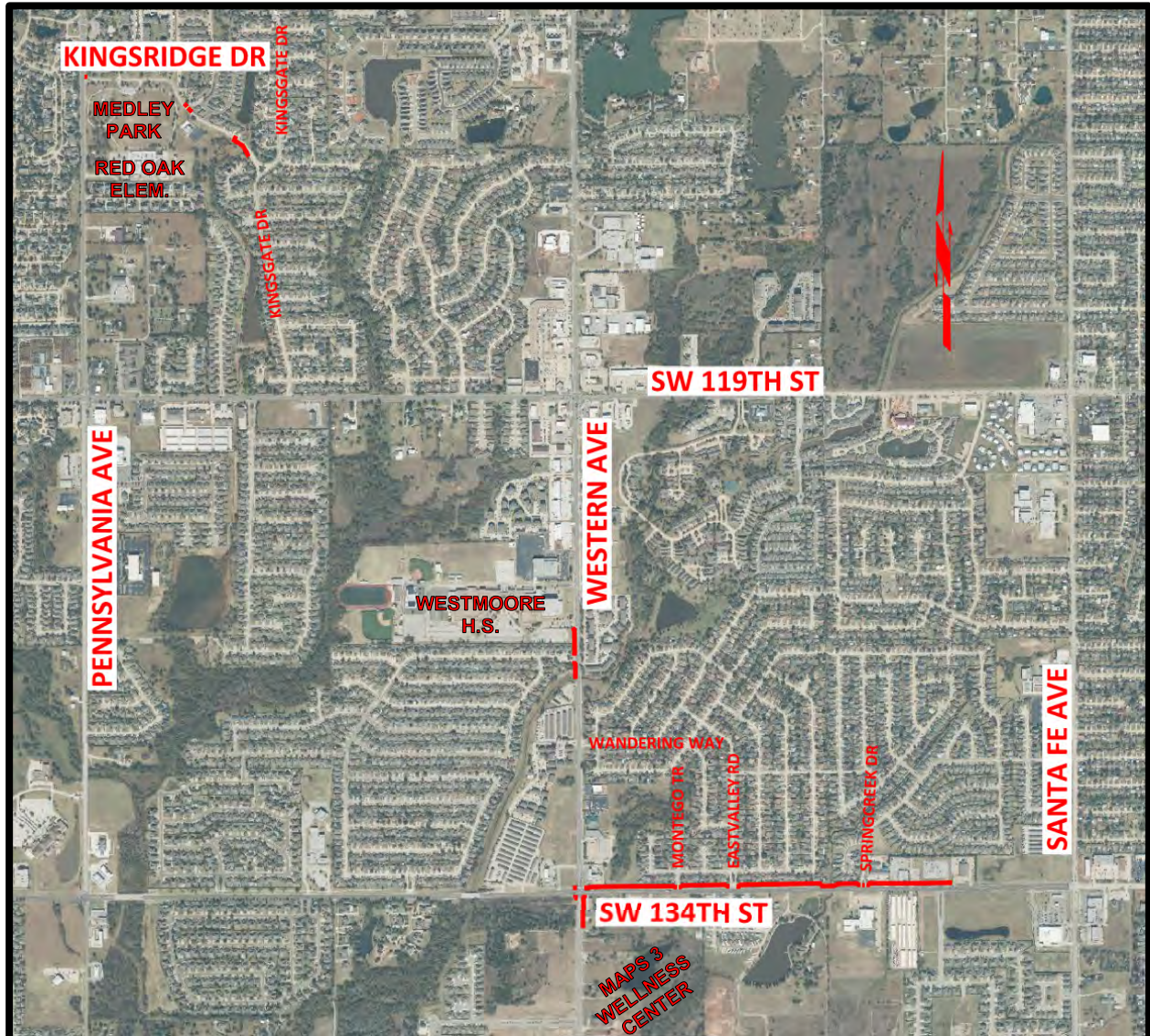


Figure 19: Aerial Scope, Area C (Far South OKC)

Existing Conditions

Area C is comprised of three alignments: Kingsridge Drive, Western Avenue, and S.W. 134th Street. The locations and conditions in this section vary widely. Kingsridge Drive is a minor two-lane residential street, whereas Western Avenue and S.W. 134th Street are four-lane major arterials. These alignments primarily service residences, schools, parks, and businesses. Importantly, the MAPS 3 Wellness Center No. 4 is currently under construction just south of the S.W. 139th Street & Western Avenue intersection, and this project intends to provide connectivity to the new development. Originally, sidewalk improvements at Red Oak Elementary School and Westmoore High School were initially envisioned as part of this project, however recent City guidance to avoid any encroachment onto school properties resulted in the removal of these segments. See Table 2 summarizing existing descriptions for these alignments.

There is a warranted need for sidewalk and pedestrian improvements in this area. Along the north side of Kingsridge Drive, some sidewalk segments already exist, however it is not ADA-compliant due to discontinuous route segments and missing curb ramps at some intersections. For Western Avenue between Westmoore Way and S.W. 134th Street, a continuous ADA-compliant pathway exists on the east side and there are some two disconnected segments along the west side limited to business frontages. For Western Avenue south of the S.W. 134th Street intersection, a compliant accessible route exists along the east side of the roadway starting about 350-feet south of the intersection and continuing over a mile, which this project intends to connect to. For S.W. 134th Street, no continuous sidewalks exist on either side of the roadway.

Roadway	Lanes	Speed Limit	Typology	Use Description
Kingsridge Dr.	2	25	Neighborhood	Residential
Westmoore H.S.	4	45	Major Arterial	Commercial, residential, school, church
S.W. 134 th St.	4	40	Major Arterial	Commercial, residential, parks, schools

Table 2: Roadway Summary, Area C (Far South OKC)



Figure 20: Existing Conditions, Area C (Far South OKC)

Right-of-Way

Preliminary investigations reveal this area with varied right-of-way widths. Kingsridge Avenue is a residential roadway with an abutting public park, Merrel-Medley Park, and is currently defined as a 60-foot right-of-way. Western Avenue is a major arterial with neighborhood developments, schools, and businesses nearby and is currently platted with a 110-foot right-of-way. Lastly, S.W. 134th Street is also a major arterial similar to Western Avenue and possesses a 117-foot right-of-way. Sidewalk improvements at Westmoore High School and Red Oak Elementary were originally envisioned as part of this project, however these segments had to be removed due to guidance to avoid encroachment onto school properties.

SRB believes that sidewalk improvements can be installed along these streets without any additional acquisitions due to plentiful space availability within existing right-of-way.

Signalized Intersections

There are two existing signalized intersections along this scope:

1. Westmoore Way & Western Avenue
2. S.W. 134th Street & Western Avenue

The first signal at Westmoore Way is 3-legged and currently ADA-compliant with a singular east-west pedestrian crossing on the south leg of the intersection across Western Avenue. The detection system is a special-case and does not need updating. This project will attempt to simply connect to existing pedestrian hardscapes and minimize reconstruction.

The second signal at S.W. 134th Street is 4-legged and contains two pedestrian crossings on the north and east legs of the intersection; however, the northwest and southeast quadrants are not ADA-compliant due to missing or unsuitable curb ramps and improper push button placement. In addition, the northeast quadrant might be partially ADA-complaint with suitable curb ramps, landings, and sidewalk surfaces, however the pedestrian push buttons share the same pole which is not compliant per current City standards requiring push buttons be placed on separate poles about 10-feet away from each other. The detection system has been recently modernized. The design intention will be to improve this intersection in the form of compliant curb ramps and landings, proper inclusions of pedestrian push buttons, and relevant crosswalk striping and signaling on all four quadrants.

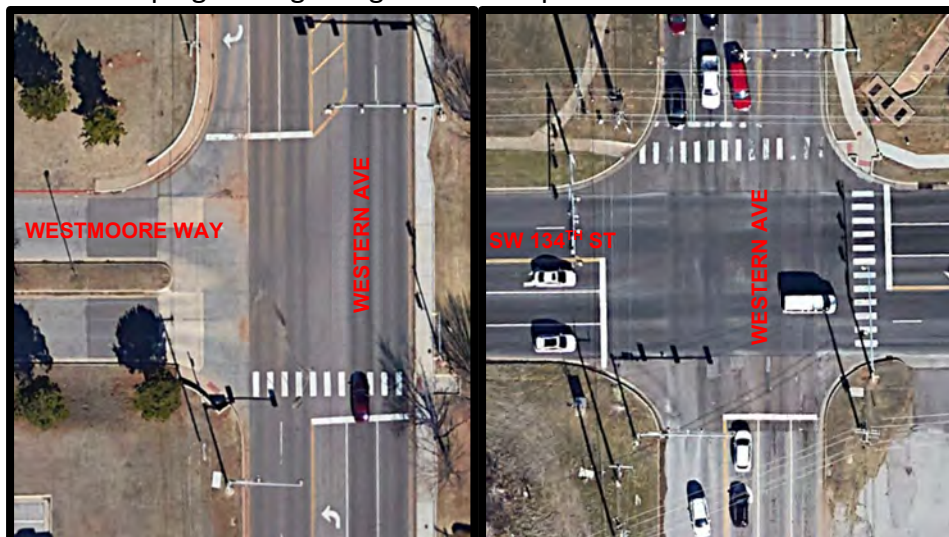


Figure 21: Signalized Intersections, Area C (Far South OKC)

Key Challenges

Improvements along these three alignments will be generally straight-forward except for a handful of challenging areas. These challenges were encountered in the form of physical conflicts or obstructions, property lines and right-of-way concerns, steep grades, pedestrian route connectivity, or other design nuances. These identified challenges are summarized herein.

For Kingsridge Drive, a reinforced concrete box with pedestrian sidewalk and protective railing already exist along our alignment. We intend to not disturb the existing drainage structure and simply connect to on either side. We will request this area as part of the topographical survey requests to verify conditions and design more accurately.

For Western Avenue, the landscaping behind the curb along Westmoore High School area is very steep and houses multiple utilities such as utility pools. In addition, an existing reinforced concrete box and railing leaves little space for adding a sidewalk at this location. SRB intends to request that this area along Western Avenue near Westmoore High School be included in topographical surveys to provide retaining walls along hilly segments and to properly design across the existing drainage structure with compliant sidewalk and protective handrailing.

For S.W. 134th Street, we identified several concerns along the north side primarily due to the existing ditches and drainage structures. First, there is a paved ditch and steep grading between Montego Terrace and Eastvalley Road. Most significantly, an unpaved ditch, reinforced concrete box, and numerous driveways complicate the design between Lakeview Drive and where the project ends at the U.S. Postal Office west of Harvey Avenue. We propose that these two areas be included as part of the topographical survey requests to design around the existing hydraulic features, verify right-of-way, ensure proper inclusions of landscaping curbs or pedestrian railing, check compliant paths through driveways, and avoid existing utilities or other conflicts.



Figure 22: Kingsridge Dr. (Looking East)

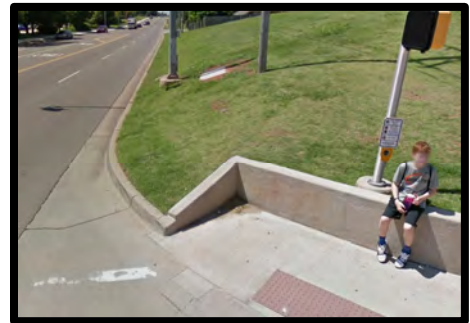


Figure 23: Western Ave. & Westmoore Way



Figure 24: Western Ave. RCB (Looking North)



Figure 25: S.W. 134th St. Paved Ditch



Figure 26: S.W. 134th St. Unpaved Ditch, RCB, & Driveways

Identification of Topographical Survey

The utilization of topographic surveys will be necessary to provide accurate engineering solutions with respect to property boundaries, right-of-way verification, American with Disabilities Act compliance, obstruction avoidance, and geological alteration. No more than 25% of the total linear footage of this project shall be requested, and it is estimated that the requests below do not exceed this ratio. The candidates for topographic survey requests were discussed in the previous sections of this report and are summarized in Table 3 below.

Main Roadway	Intersecting Roadway	Survey Description
S.W. Grand Blvd.	Western Ave.	Signalized intersection, parking lot, right-of-way
S.W. 41 st St.	McKinley Ave.	Existing curb inlets & utilities
Douglas Ave.	S.W. 44 th St.	Signalized intersection
Olie Ave.	S.W. 44 th St.	Utility poles, right-of-way
Kingsridge Dr.	Kingsgate Dr.	Reinforced concrete box (RCB) crossing
Western Ave.	S.W. 134 th St.	Signalized intersection
Western Ave.	Westmoore Way.	Signalized intersection, steep grades, RCB, R/W
S.W. 134 th St.	Montego Tr. to Eastvalley Dr.	Paved ditched, steep grades, utilities
S.W. 134 th St.	Lakeview Dr. to Harvey Ave.	Unpaved ditch, steep grades, utilities, RCB crossing

Table 3: Identification of Topographical Survey

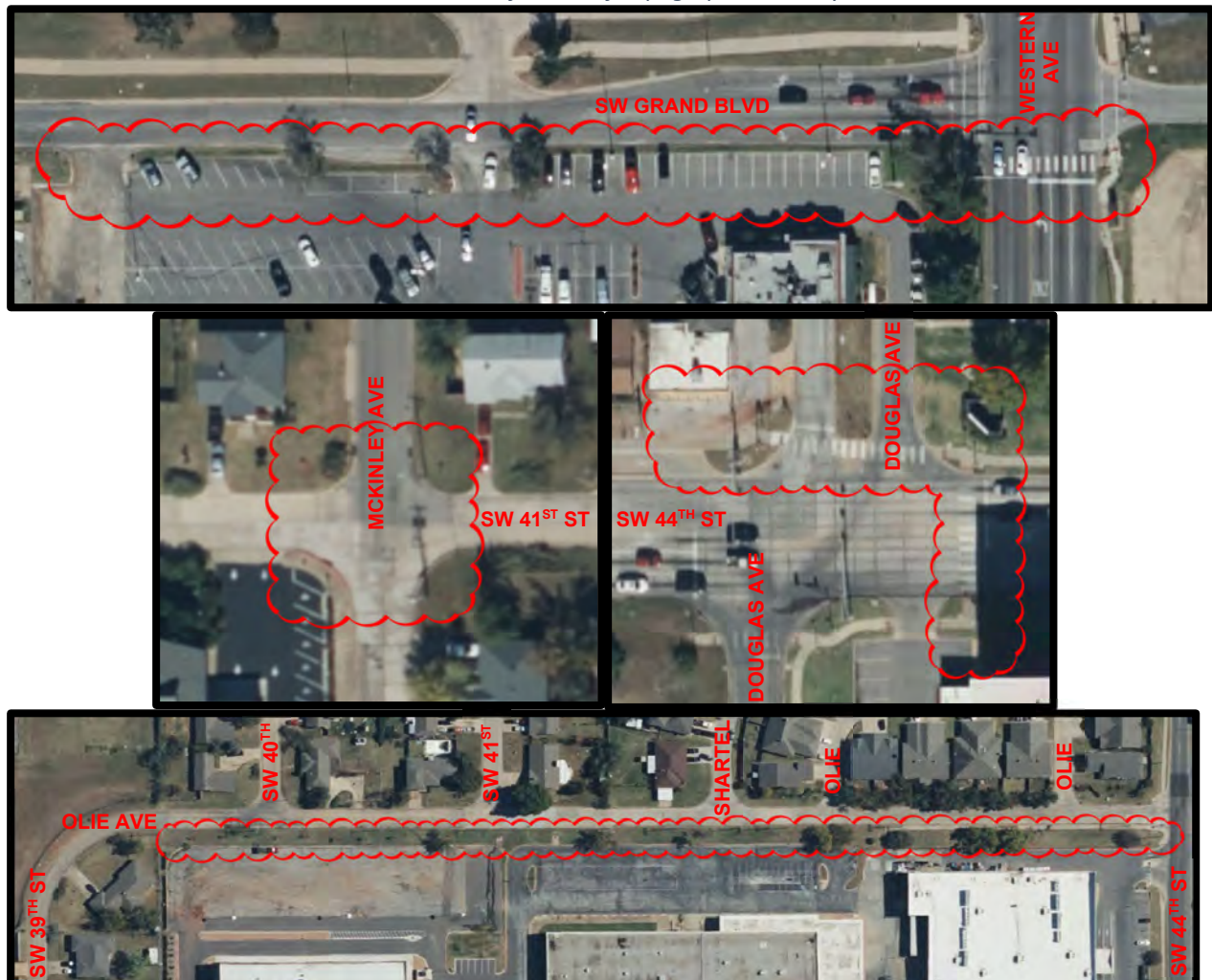


Figure 27: Identification of Topographical Surveys, Areas A-B

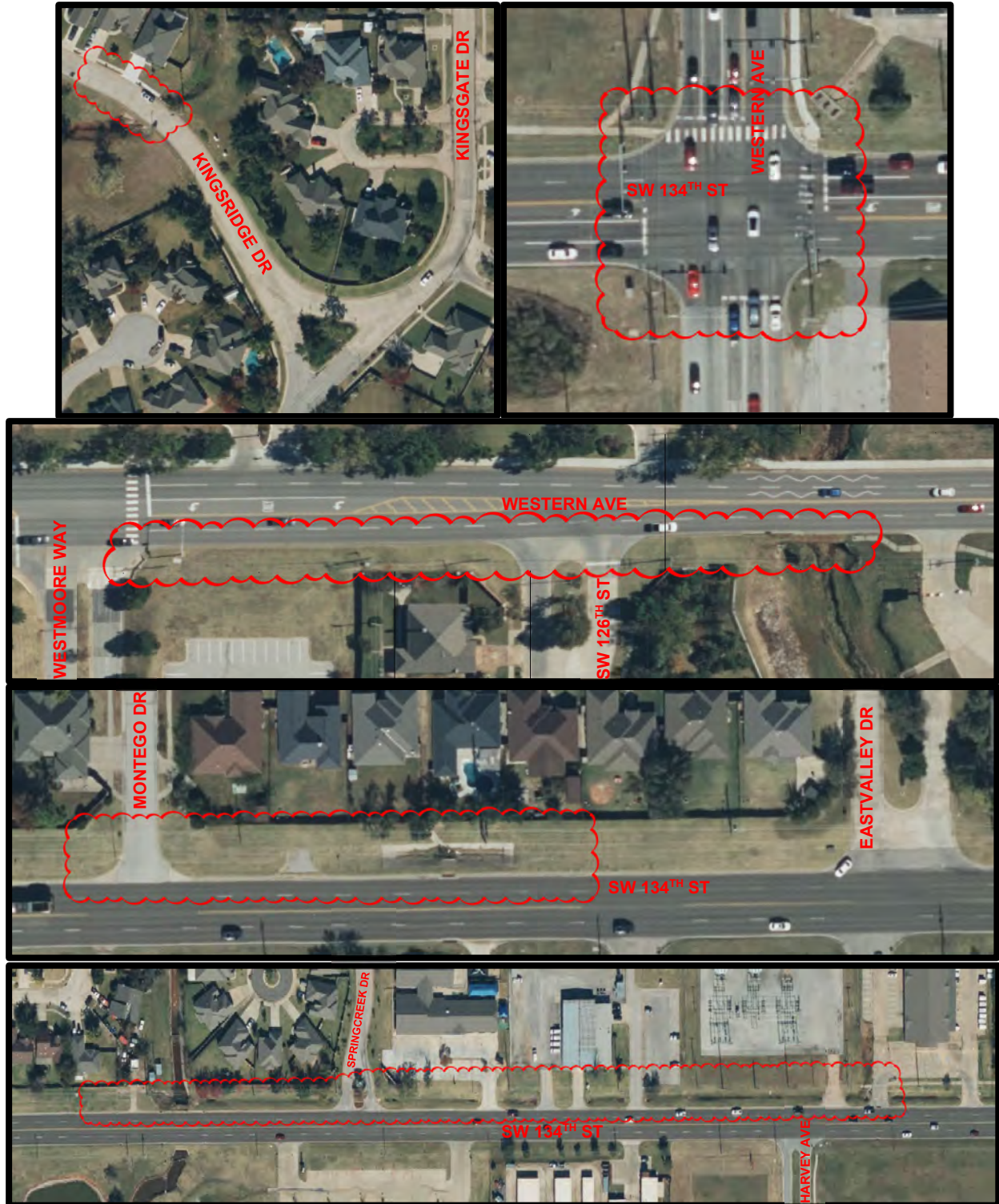


Figure 28: Identification of Topographical Surveys, Area C

Conclusion and Recommendations

This Preliminary Report for pedestrian enhancements at Western Avenue nearby Integris Southwest Medical Center, Pennsylvania Avenue nearby U.S. Grant High School, and Far South OKC nearby Westmoore High School sought to incorporate all available information from plat research, geospatial and geographical information, preliminary surveys, and City design standards and preferences. The goal of this report is to provide conceptual design, identify Right-of-Way verification and acquisition needs, establish topographical survey requirements, anticipate required utility relocations, determine special challenges or considerations, and introduce preliminary solutions for the improvements related to this project. The recommendations in this report are based on best engineering practices and align with the needs of and the desires for these areas.

For Western Avenue near Integris Southwest Medical Center, the recommended improvements depend on the alignment per the original City intentions. A single pedestrian route on one side of the roadway is recommended along S.W. Grand Boulevard, S.W. 39th Street/Olie Avenue, and Douglas Avenue, and two pedestrian routes on both sides of the roadway are proposed along S.W. 38th and 41st Streets. Right-of-Way verification and some possible acquisitions may be required to construct new sidewalks along the southwest quadrant of S.W. Grand Boulevard and Western Avenue due to the roadway being constructed very close to the plat limits.

For Pennsylvania Avenue near Grant High School, the typical recommended improvement for this area are two pedestrian routes on both sides of the roadway along S.W. 51st and 55th Streets.

For Far South Oklahoma City, the typical recommended improvement for this scope is to ensure at least one fully compliant accessible route is provided along alignments identified by the City. For the Kingsridge Drive and Western Avenue alignments, existing pedestrian hardscapes already exist along the project scope and this project's goal is to connect existing disjointed sidewalk segments with new sidewalks for improved connectivity. For S.W. 134th Street, no pedestrian paths exist on either side of the roadway thus a new pedestrian route will be proposed along the north side of the roadway.



CONCLUSION AND RECOMMENDATIONS

Engineer's Opinion of Probable Cost

With respect to the recommendations listed above, a probable cost estimate for the proposed improvements is shown in the figure below. The estimated cost is \$3,450,000.

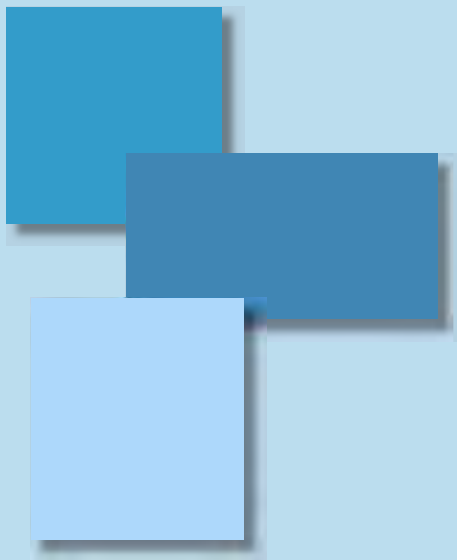
M4-TS011 PHASE 1B						
ITEM NO.	SPEC. NO.	ITEM	UNIT	ESTIMATED QUANTITY	UNIT BID	AMOUNT
1	225	AGGREGATE BASE (TYPE A) W/ EXCAVATION	C.Y.	1,377.11	\$120.00	\$ 165,253.33
2	305	CURB & GUTTER (2FT-8IN) (6IN BARRIER)	L.F.	100.00	\$70.00	\$ 7,000.00
3	305	INTEGRAL CURB (6IN BARRIER)	L.F.	400.00	\$20.00	\$ 8,000.00
4	403	RETAINING WALL	L.F.	200.00	\$750.00	\$ 150,000.00
5	454	REBUILD INLET	EA.	2.00	\$6,000.00	\$ 12,000.00
6	734	TRAFFIC STRIPE (MULTI-POLY) (4 INCH WIDE)	L.F.	6,100.00	\$1.00	\$ 6,100.00
7	734	TRAFFIC STRIPE (MULTI-POLY) (ARROWS) (SINGLE)	EA.	8.00	\$300.00	\$ 2,400.00
8	745	TRAFFIC SIGNALS IMPROVEMENTS	L.S.	1.00	\$431,333.97	\$ 431,333.97
9	801	G.P.S. AS-BUILT SURVEY	L.S.	1.00	\$25,000.00	\$ 25,000.00
10	801	CONSTRUCTION STAKING (CONSTRUCTION SURVEY)	L.S.	1.00	\$25,000.00	\$ 25,000.00
11	801	COLOR AUDIO/VIDEO RECORDING, PRE & POST CONSTRUCTION (RECORDED DIGITALLY ON DVD)	L.S.	1.00	\$10,000.00	\$ 10,000.00
12	802	CONSTRUCTION SIGNING AND TRAFFIC CONTROL	L.S.	1.00	\$50,000.00	\$ 50,000.00
13	809	MOBILIZATION	L.S.	1.00	\$136,000.00	\$ 136,000.00
14	810	CLEARING AND GRUBBING	L.S.	1.00	\$30,000.00	\$ 30,000.00
15	812	REMOVE PAVEMENT (VARIES)	S.Y.	0.00	\$25.00	\$ -
16	812	REMOVE SIDEWALK	S.Y.	100.00	\$15.00	\$ 1,500.00
17	812	REMOVE CURB AND GUTTER	L.F.	100.00	\$20.00	\$ 2,000.00
18	813	REMOVE DRIVEWAY	S.Y.	5,146.00	\$15.00	\$ 77,190.00
19	819	ADJUST EXISTING STRUCTURE (MANHOLES)	EA.	5.00	\$5,000.00	\$ 25,000.00
20	819	ADJUST EXISTING STRUCTURE (HYDRANT)	EA.	3.00	\$5,000.00	\$ 15,000.00
21	819	ADJUST EXISTING STRUCTURE (METERS)	EA.	10.00	\$1,000.00	\$ 10,000.00
22	819	ADJUST EXISTING STRUCTURE (VALVES)	EA.	10.00	\$1,000.00	\$ 10,000.00
23	819	ADJUST EXISTING STRUCTURE (CLEAN OUT)	EA.	5.00	\$1,000.00	\$ 5,000.00
24	819	ADJUST EXISTING STRUCTURE (PULL BOX)	EA.	10.00	\$1,250.00	\$ 12,500.00
25	819	REMOVE & RESET SIGN	EA.	20.00	\$500.00	\$ 10,000.00
26	823	SIDEWALK (4IN)	S.Y.	13,864.00	\$65.00	\$ 901,160.00
27	823	DRIVEWAY (6IN)	S.Y.	5,146.00	\$100.00	\$ 514,600.00
28	823	ADA CURB RAMP	S.Y.	632.00	\$140.00	\$ 88,480.00
29	828	REMOVE AND RECONSTRUCT FENCE	L.F.	1,000.00	\$50.00	\$ 50,000.00
30	830	TACTILE WARNING DEVICE	S.F.	920.00	\$40.00	\$ 36,800.00
31	840	SOLID SLAB SODDING	S.Y.	11,000.00	\$3.00	\$ 33,000.00
32	900	SEDIMENT AND EROSION CONTROL	L.S.	1.00	\$20,000.00	\$ 20,000.00
20% Contingency						\$ 579,682.70
Total						\$ 3,450,000.00

Figure 29: Cost Estimate

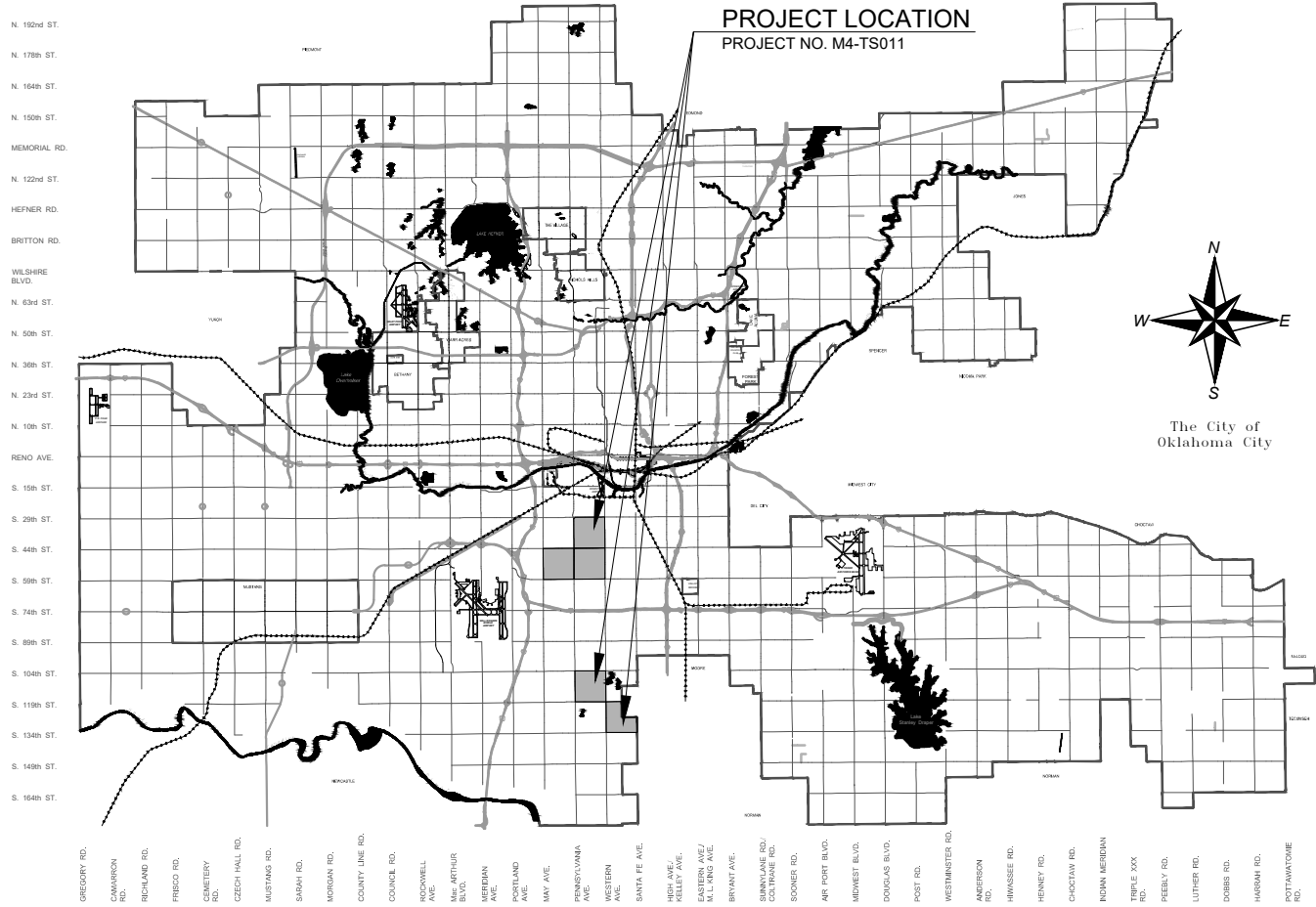


APPENDIX A

CONCEPTUAL ENGINEERING PLANS



2/27/2024 10:28 AM BRANDON LEVERHART \\FILES\PROJECTS\116822 MAPS4 SIDEWALKS PHASE 1A 1B_PHASE 1B\DRAWINGS\116822-TITLE.DWG



PROJECT M4-TS011
MAPS 4 SIDEWALKS AND RELATED
AMENITIES PHASE 1B

30% DESIGN
FEBRUARY 27, 2024

MAPS4

OWNER

City of Oklahoma City
MAPS Office
420 West Main Street, Suite 400
Oklahoma City, OK 73102
Phone: (405) 297-3461
Project Manager: Brannon Nelson



ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

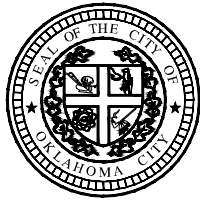
TRAFFIC



6000 S. Western Ave., Ste. 300
Oklahoma City, OK 73139
Phone: (405) 73139

DRAWING LIST

DRAWING NO.	DESCRIPTION
	COVER SHEET
CD-01 - CD-02	TYPICAL SECTIONS
G-01	GENERAL NOTES
G-02	SUMMARY OF PAY QUANTITIES AND NOTES
LM-2A, LM-2B, LM-2C	SITE MAPS
RA1-1 - RA1-3	SW GRAND BLVD
RA2-1	SW 38TH ST
RA3-1 - RA3-2	SW 39TH ST - S OLIE AVE
RA4-1	SW 41ST ST
RA5-1 - RA5-2	S DOUGLAS AVE
RB1-1 - RB1-5	SW 51ST ST
RB1-1 - RB1-2	SW 55TH ST
RC1-1 - RC1-2	KINGSRIDGE DR
RC2-1	S WESTERN AVE
RC3-1 - RC3-4	SW 134TH ST
RC3-4	S WESTERN AVE
EA-1, EB-1, EC-1	STORMWATER POLLUTION PREVENTION PLAN
A1-1 - A1-3	SW GRAND BLVD
A2-1	SW 38TH ST
A3-1 - A3-2	SW 39TH ST - S OLIE AVE
A4-1	SW 41ST ST
A5-1 - A5-2	S DOUGLAS AVE
B1-1 - B1-5	SW 51ST ST
B2-1 - B2-2	SW 55TH ST
C1-1 - C1-2	KINGSRIDGE DR
C2-1 - C2-2	S WESTERN AVE
C3-1 - C3-4	SW 134TH ST
	TRAFFIC SIGNAL SUMMARY OF PAY QUANTITIES
	TRAFFIC SIGNAL PAY QUANTITIES NOTES
	TRAFFIC SIGNAL MODIFICATIONS
	SIGNING & STRIPING PLANS
	TYPICAL LANE CLOSURE
	TYPICAL SIGNAL TRAFFIC CONTROL
	STORM WATER EROSION AND SEDIMENT CONTROL PROCEDURES
	STANDARD TYPICAL SECTIONS ASPHALTIC PAVING
	STANDARD TYPICAL SECTIONS P.C. CONCRETE PAVING
	STANDARD TYPICAL SECTIONS DRIVEWAY DETAILS
	STANDARD DETAILS FOR CUL-DE-SACS
	STANDARD TYPICAL SECTIONS MISCELLANEOUS DETAILS
	STANDARD CHANNEL LINER & FLUME DETAILS
	PEDESTRIAN HANDRAIL DETAIL
	GUARDRAIL EXTRUDER TERMINAL
	ADA CURB RAMP DETAILS
	ADA CURB RAMP DETAILS
	ADA SIDEWALK AT FLUME DETAILS
	STANDARD REPAIR DETAILS
	STANDARD STORM SEWER INLETS DESIGN 2 INLET WITH CAST STEEL HOODS
	GRADED STREET INLET DETAIL
	STANDARD INLET DETAILS DESIGN #5 (SINGLE GRATING) AND BOX TYPE INLET
	STANDARD INLETS DESIGN #6 & DESIGN #7
	STANDARD INLET FRAME DETAIL FOR DESIGN #2 & #5
	STORM SEWER INLET GRATE DESIGN #1 - #6 & #7
	BICYCLE SAFE GRATE
	CURB / CATCH BASIN INLET FRAME DETAILS
	SLOTTED DRAIN INLET & TRENCH WIDTH PIPE WARPING FOR STORM SEWER
	STORM WATER QUALITY EROSION CONTROL DRAWINGS
	STORM WATER QUALITY EROSION CONTROL DRAWINGS
	STORM WATER QUALITY EROSION CONTROL DRAWINGS
	SANITARY SEWER STANDARD DETAILS
	SANITARY SEWER STANDARD DETAILS
	SANITARY SEWER STANDARD DETAILS
	SANITARY SEWER STANDARD DETAILS
	WATER STANDARD DETAILS
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	WATER STANDARD DETAILS



The City of
OKLAHOMA CITY
Public Works Department

DAVID HOLT Mayor

COUNCIL MEMBERS

BRADLEY CARTER	Ward 1
JAMES COOPER	Ward 2
BARBARA PECK	Ward 3
TODD STONE	Ward 4
MATT HINKLE	Ward 5
JOBETH HAMON	Ward 6
NIKKI NICE	Ward 7
MARK K. STONECIPHER	Ward 8

CRAIG FREEMAN City Manager

DEBBIE MILLER, P.E. City Engineer

PREPARED BY

GRADY J. WADE, P.E.

DATE



RECOMMENDED FOR APPROVAL

DAVID E. TODD, P.E.,
MAPS PROGRAM MANAGER

DATE

DEBBIE MILLER, P.E., DIRECTOR
PUBLIC WORKS / CITY ENGINEER

DATE

APPROVED AS FINAL PLANS

MAYOR

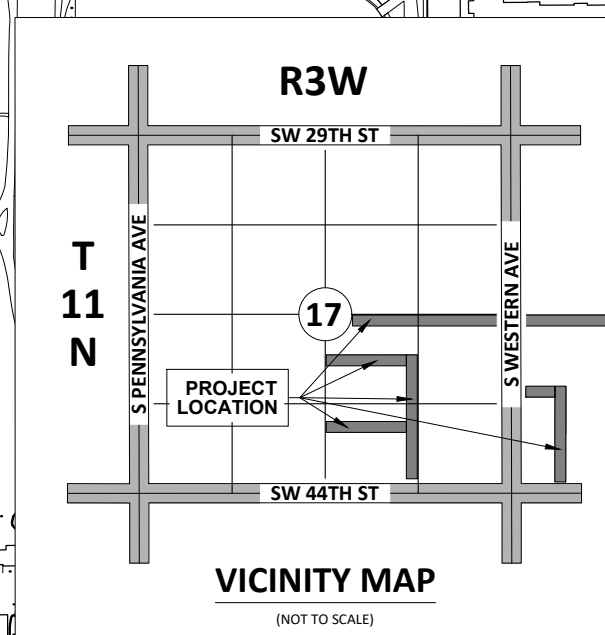
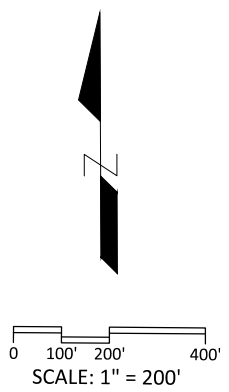
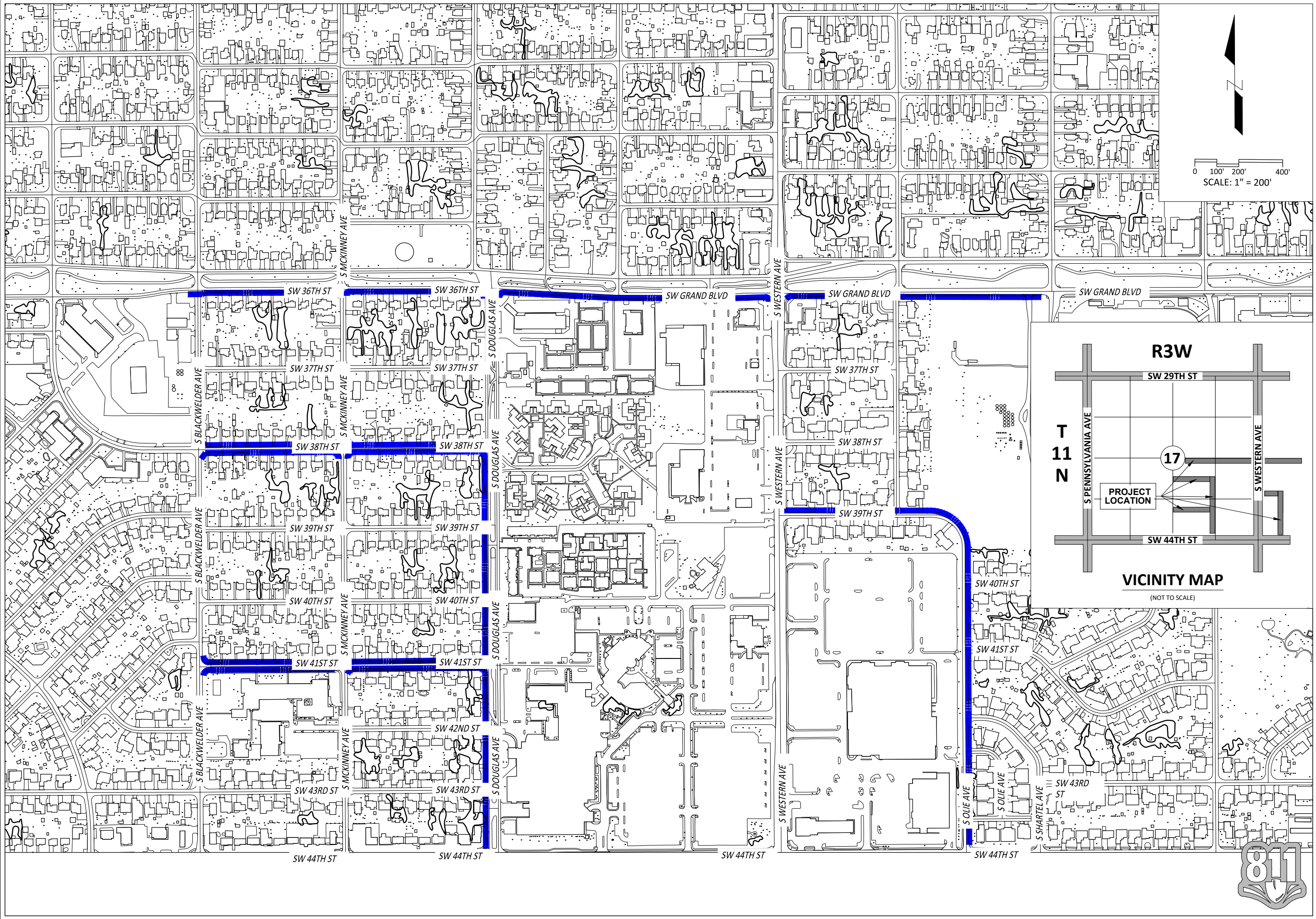
DATE

CITY CLERK

DATE

PROJECT NO. M4-TS011

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ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

REVISIONS	
NO.	DATE

CITY OF OKLAHOMA CITY, OKLAHOMA

MAPS 4 SIDEWALK AND RELATED AMENITIES

LOCATION MAPS

2A - MAP

DATE: 12.13.23

DESIGN BY: GJW

DRAWN BY: DHB

CHECKED BY: BE

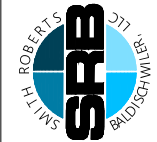
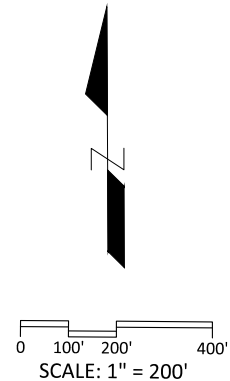
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SHEET NUMBER

LM-2A

Project #: M4-TS011




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[illegible]

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
LOCATION MAPS
2B - MAP

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

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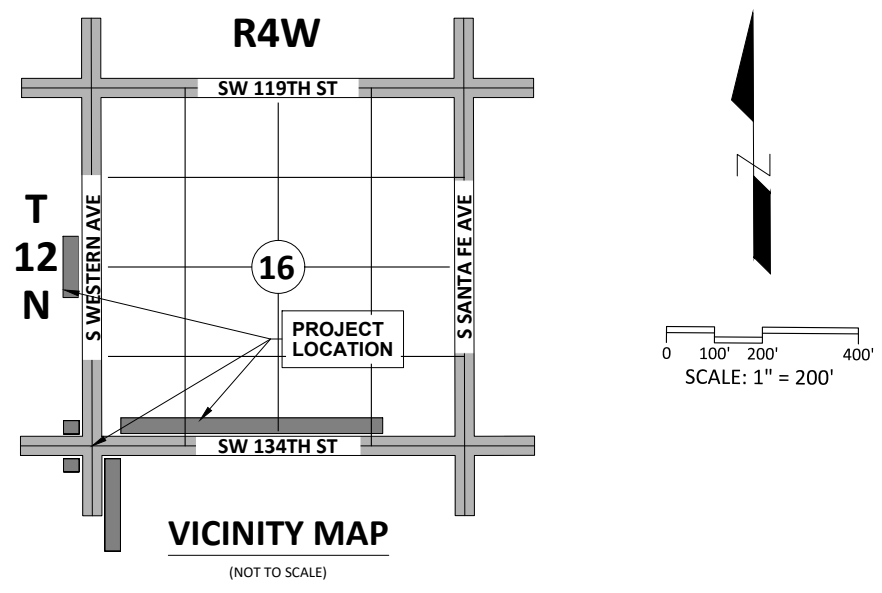
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SHEET NUMBER
LM-2B

Project #: M4-TS011

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CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
LOCATION MAPS
2C - MAP 1

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: XXX
CHECKED BY: BE

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SHEET NUMBER
LM-2C.A

Project #: M4-TS011


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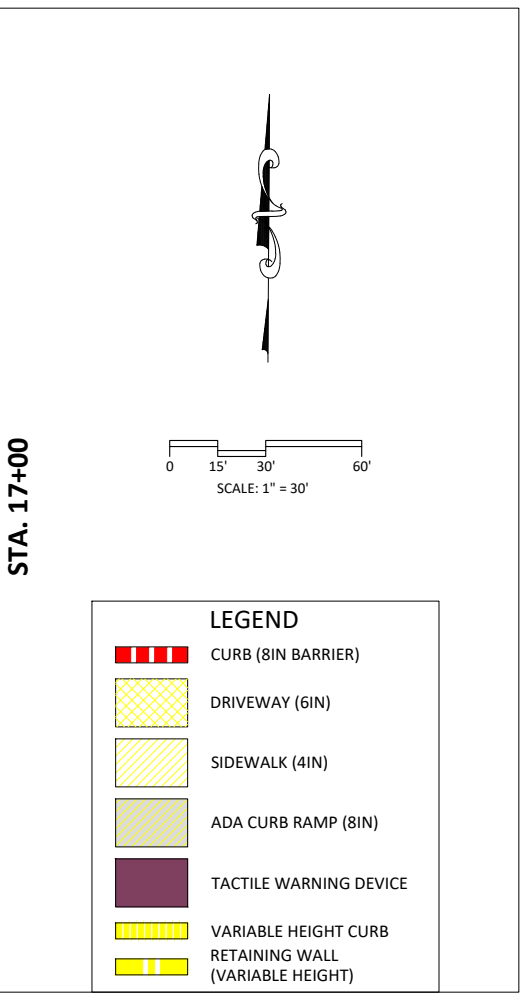
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SURVEYING
PLANNING

SRB
SANTA FE
ROBERTS
BUTLER
SMITH

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025



CITY OF OKLAHOMA CITY, OKLAHOMA MAPS 4 SIDEWALK AND RELATED AMENITIES LOCATION MAPS 2C - MAP 2	
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DRAWN BY:	XXX
CHECKED BY:	BE
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<u>SHEET NUMBER</u> LM-2C.B	
Project #: M4-TS0101	




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DRAWN BY:	DHB
CHECKED BY:	BE

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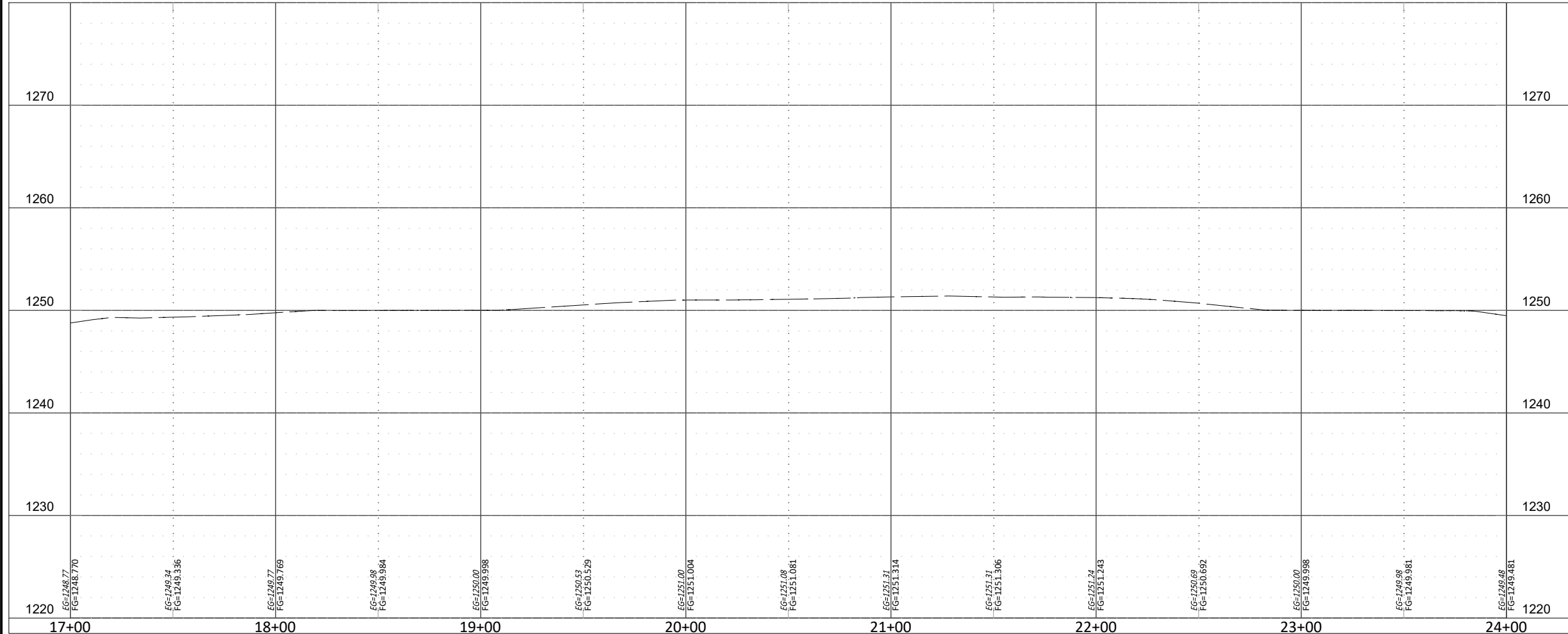
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SHEET NUMBER
A1-1

Project #: M4-TS011

	NO.	DATE

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SHEET NUMBER
A1-2

Project #: M4-TS011



LEGEND

CURB (8IN BARRIER)

DRIVEWAY (6IN)

SIDEWALK (4IN)

ADA CURB RAMP (8IN)

TACTILE WARNING DEVICE

VARIABLE HEIGHT CURB

RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

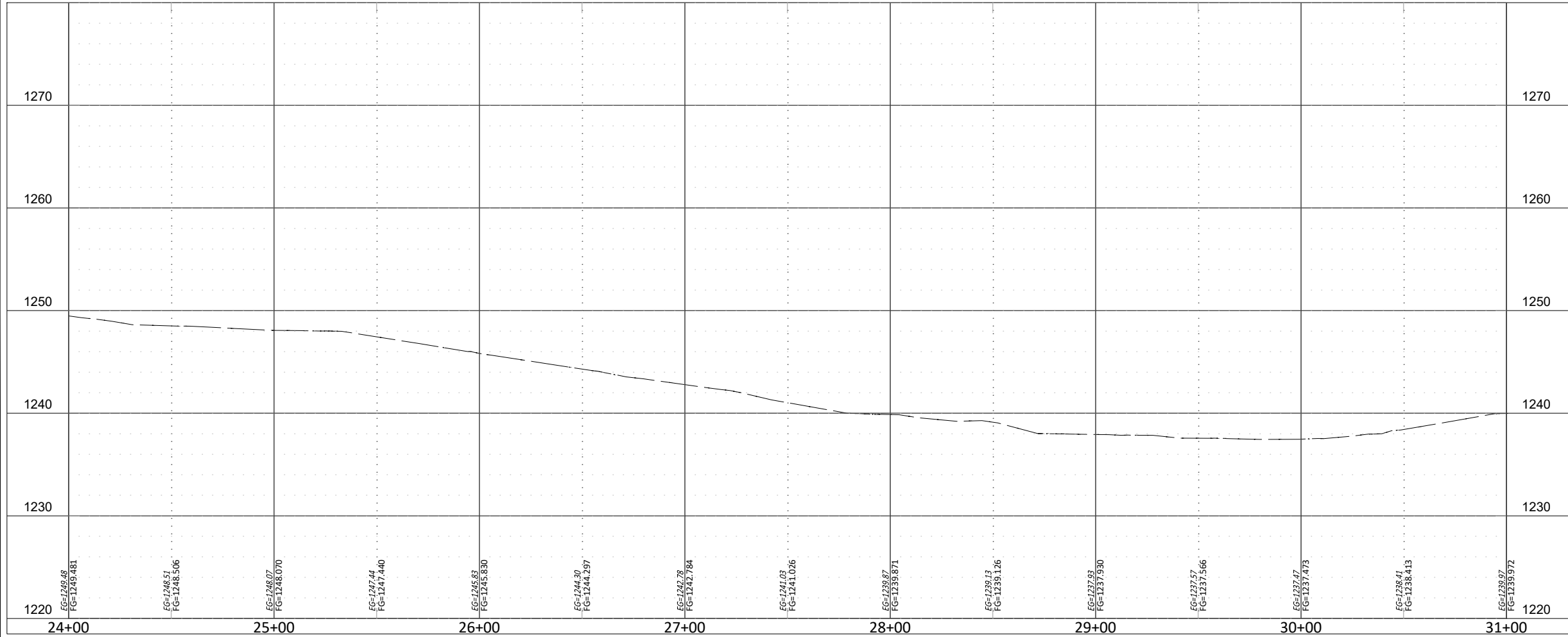
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
36TH - P&P(2)

ENGINEERING
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SRB

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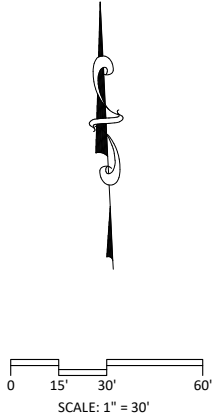


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DRAWN BY:	DHB
CHECKED BY:	BE

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SHEET NUMBER
A1-3

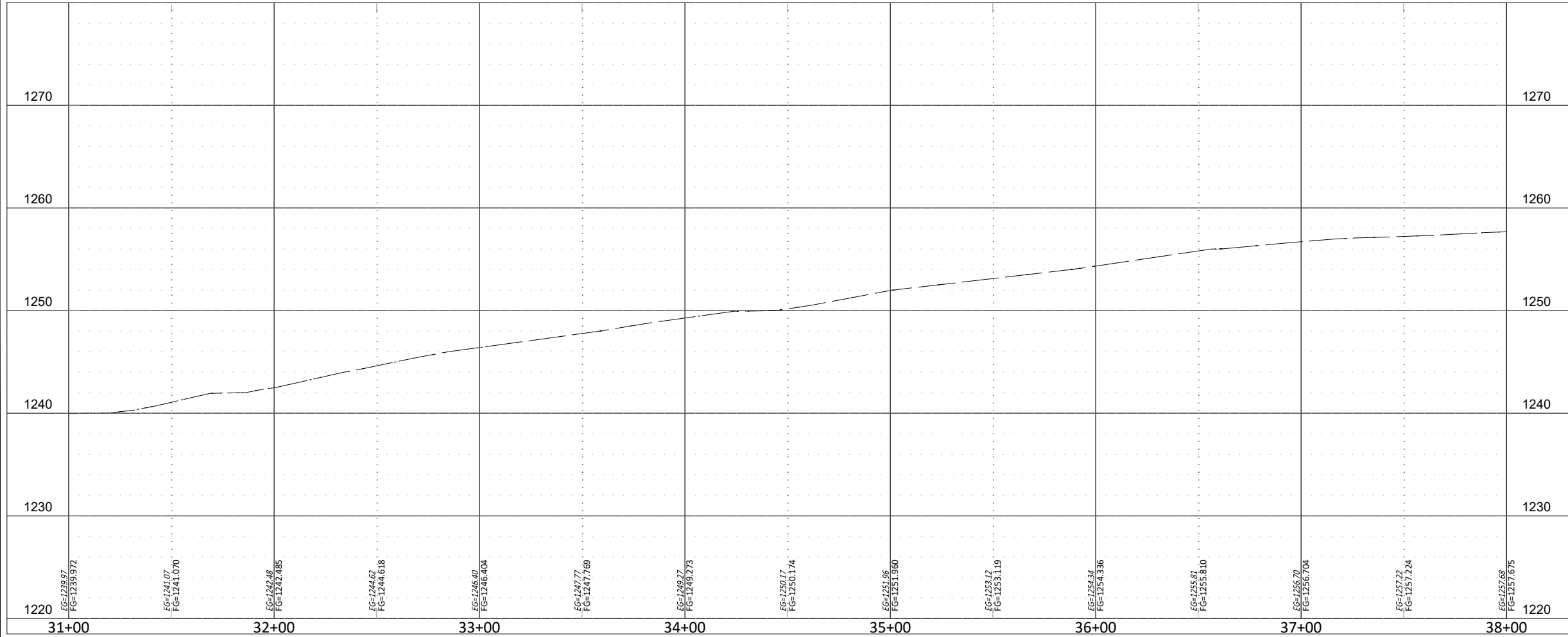
Project #: M4-TS011



REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
36TH - P&P(3)

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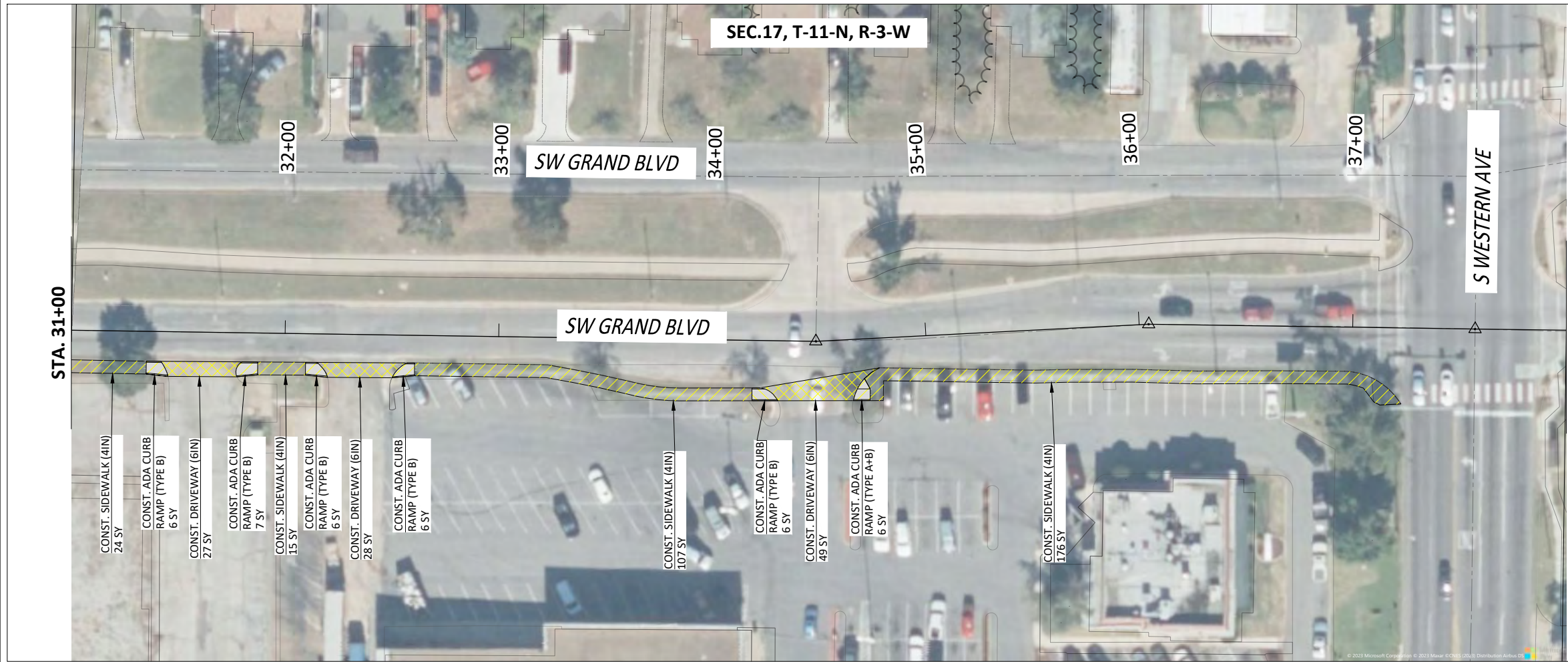


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MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
36TH - P&P(4)

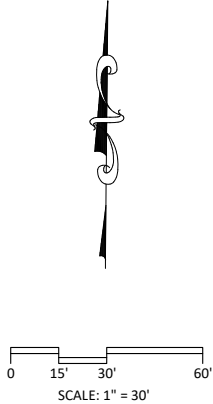
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BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A1-4
Project #: M4-TS011



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)



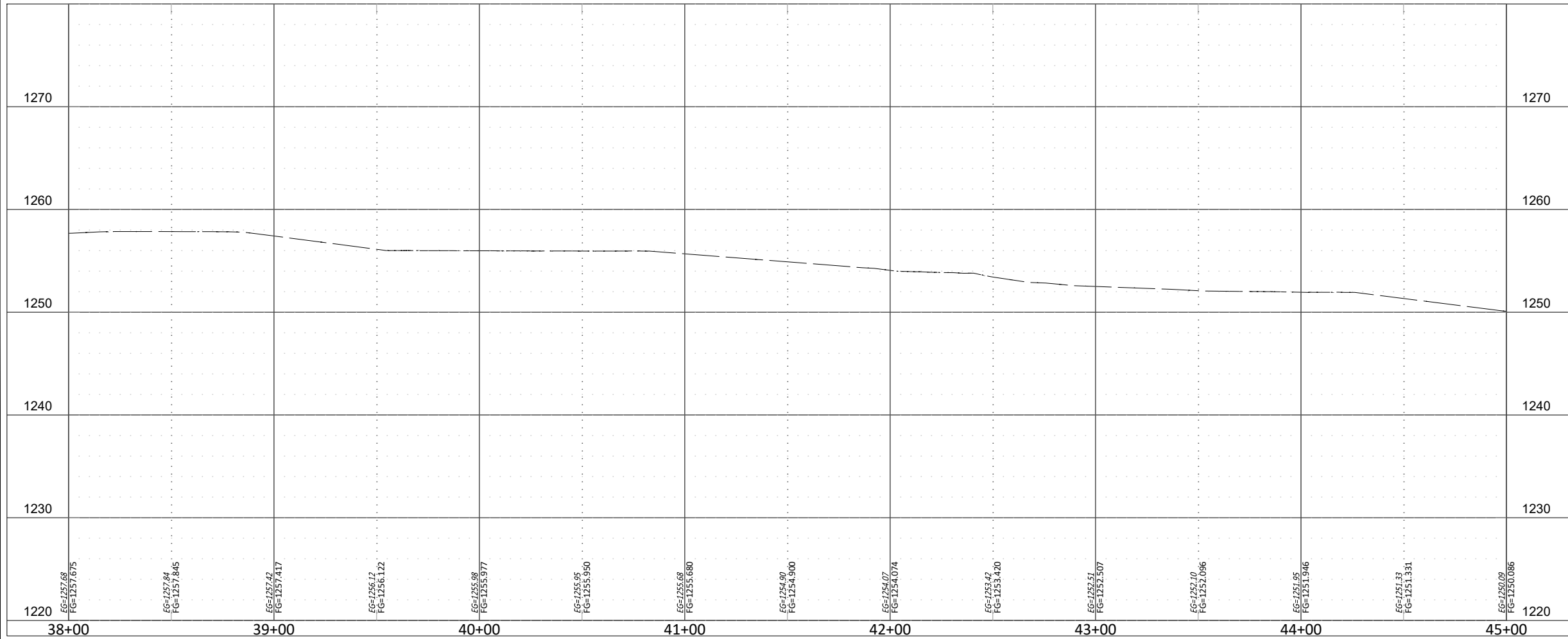
REVISIONS	
NO.	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

SRB

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

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- 2. A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.



DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

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SHEET NUMBER
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Project #: M4-TS011

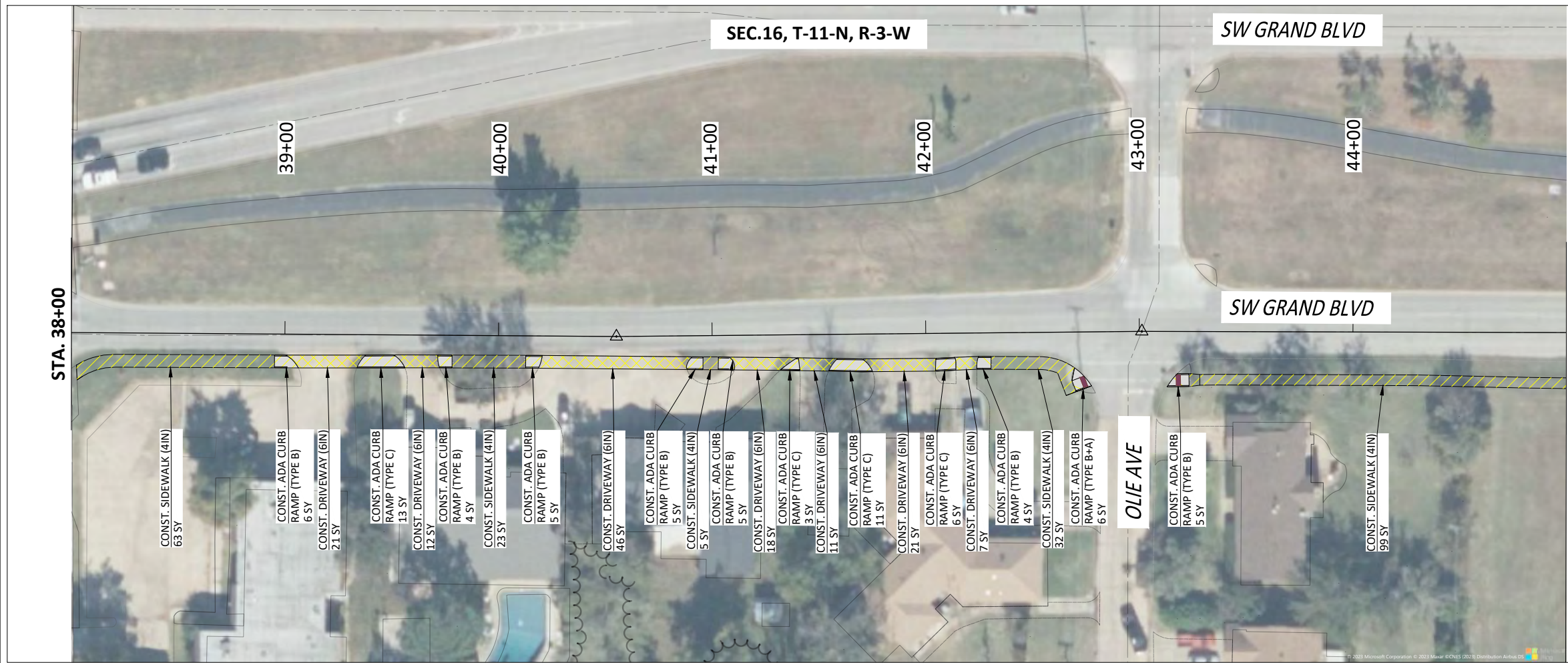
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
36TH - P&P(5)

NO.	DATE	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

SMITH
ROBERTS
SRB
BUDISCH WILSON

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025



LEGEND

CURB (8IN BARRIER)

DRIVEWAY (6IN)

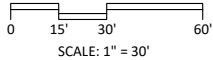
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ADA CURB RAMP (8IN)

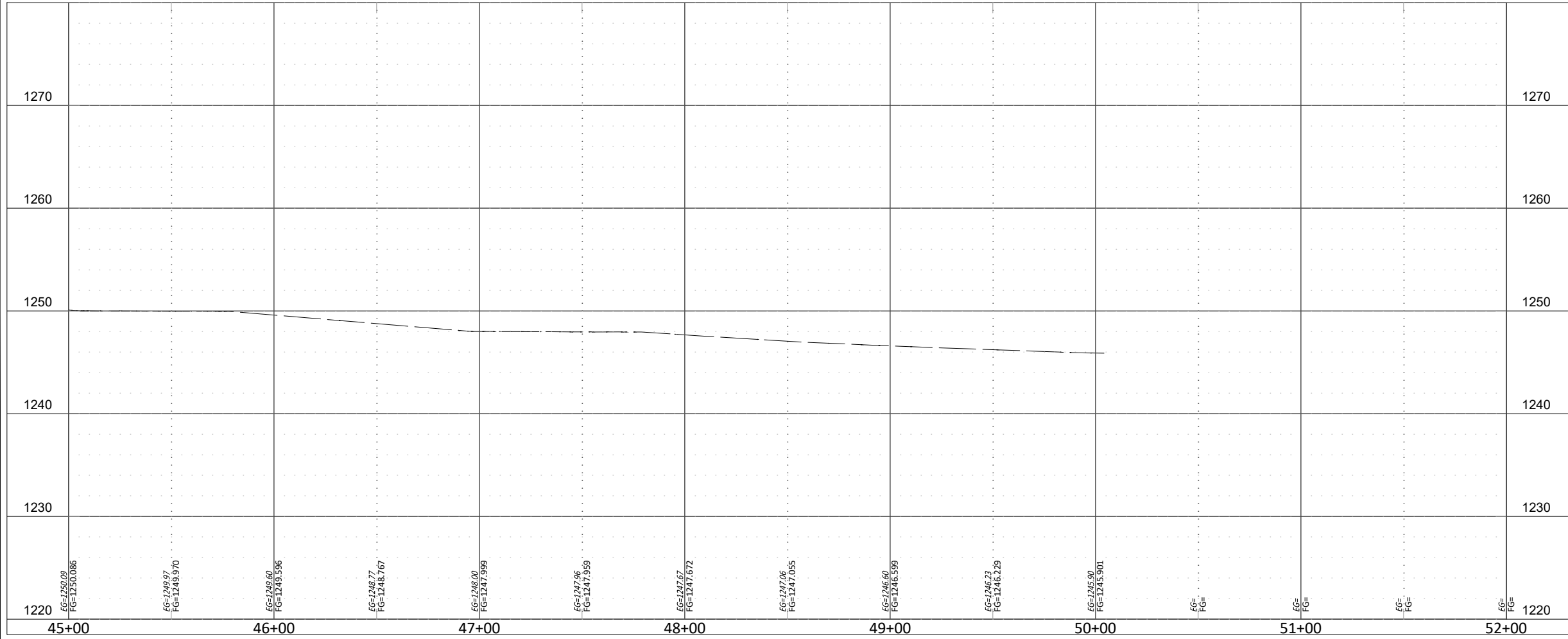
TACTILE WARNING DEVICE

VARIABLE HEIGHT CURB

RETAINING WALL (VARIABLE HEIGHT)



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GENERAL NOTES:

- CENTERLINE PROFILE CREATED BY USING 2 FT. CONTOUR FROM OKC OPEN DATA PORTAL.
- A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.



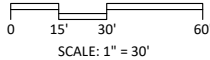
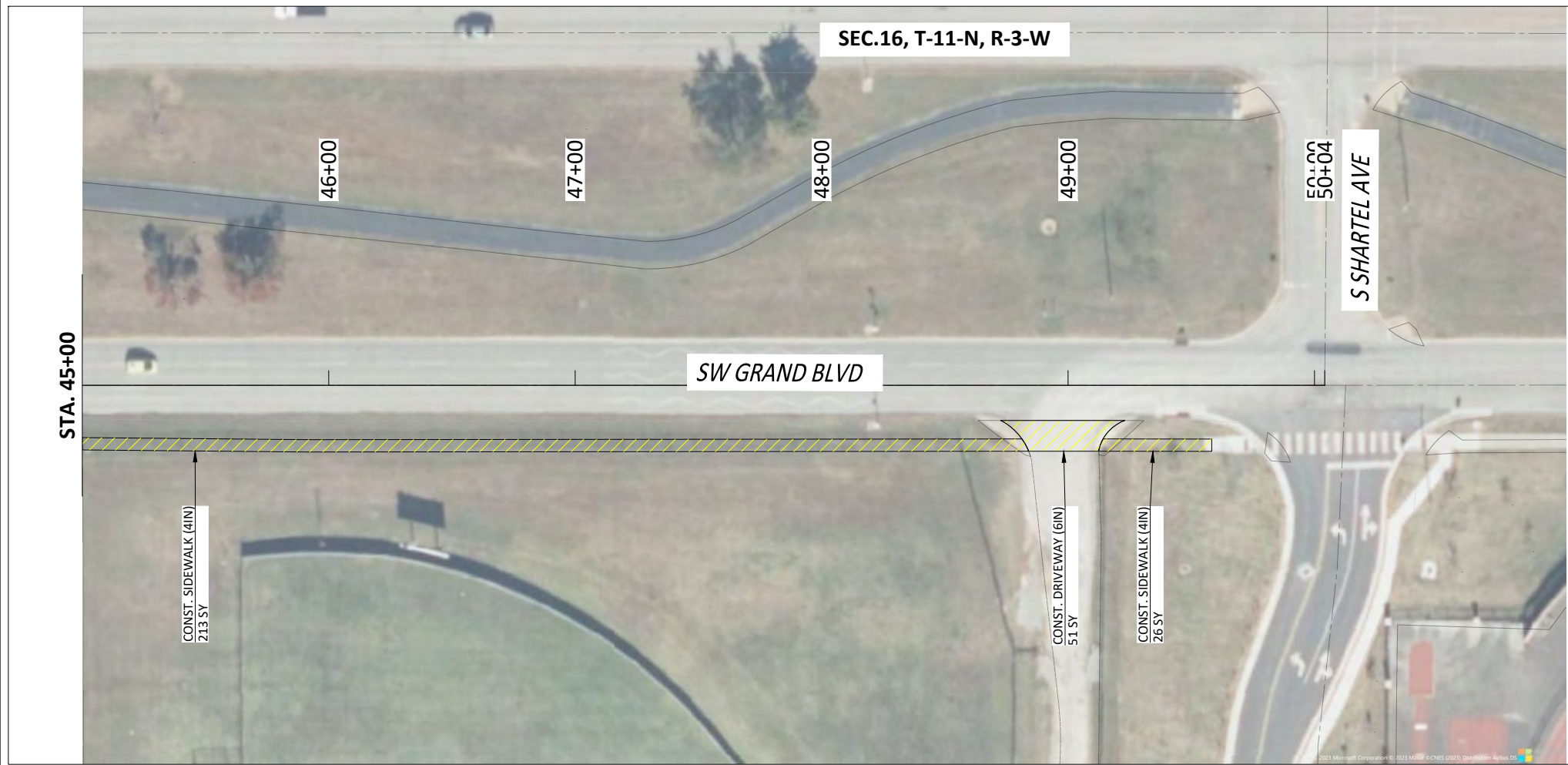
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SHEET NUMBER
A1-6

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
36TH - P&P(6)



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

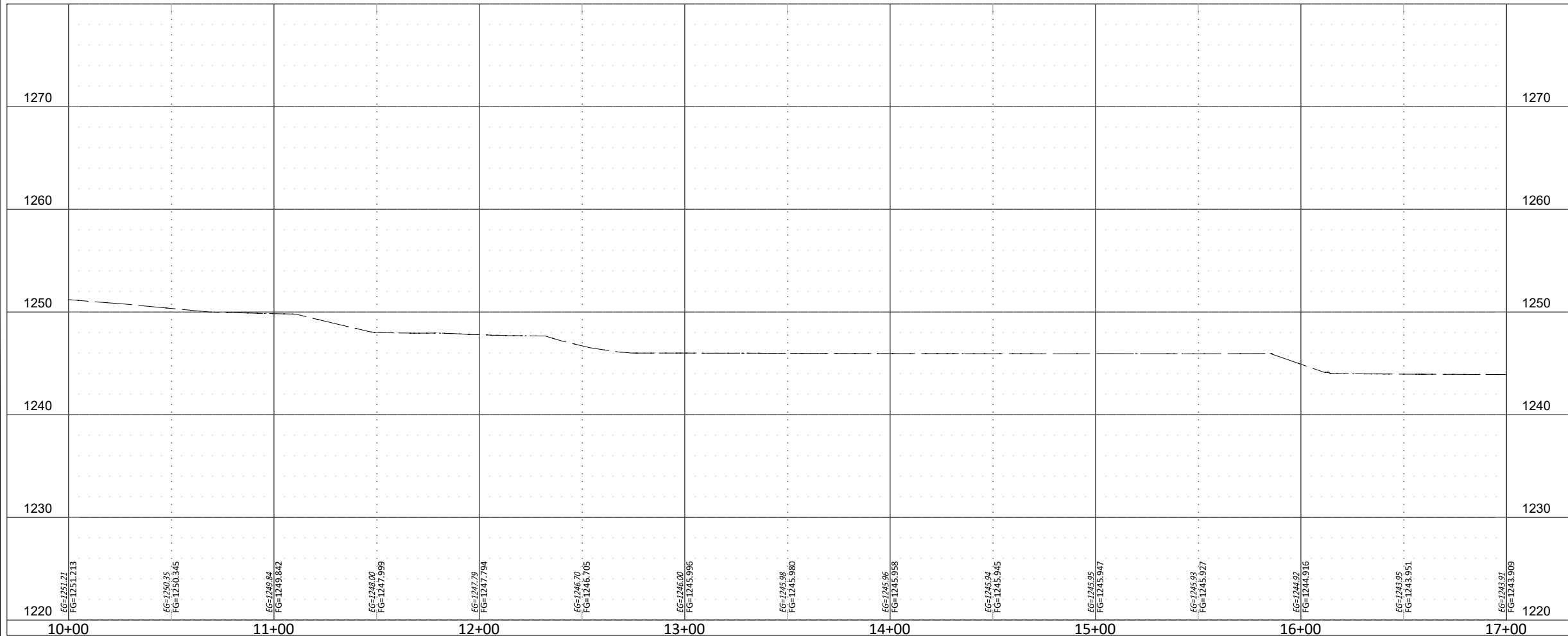
ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

SMITH
ROBERTS
SRB
BUDISCH
WILLIAMS

DTI
BERT
BERNARD

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



GENERAL NOTES:

- 1. CENTERLINE PROFILE CREATED BY USING 2 FT. CONTOUR FROM OKC OPEN DATA PORTAL.
- 2. A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.



DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

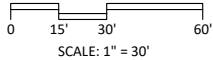
SHEET NUMBER
A2-1

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)



REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
38TH - P&P(1)

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
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BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

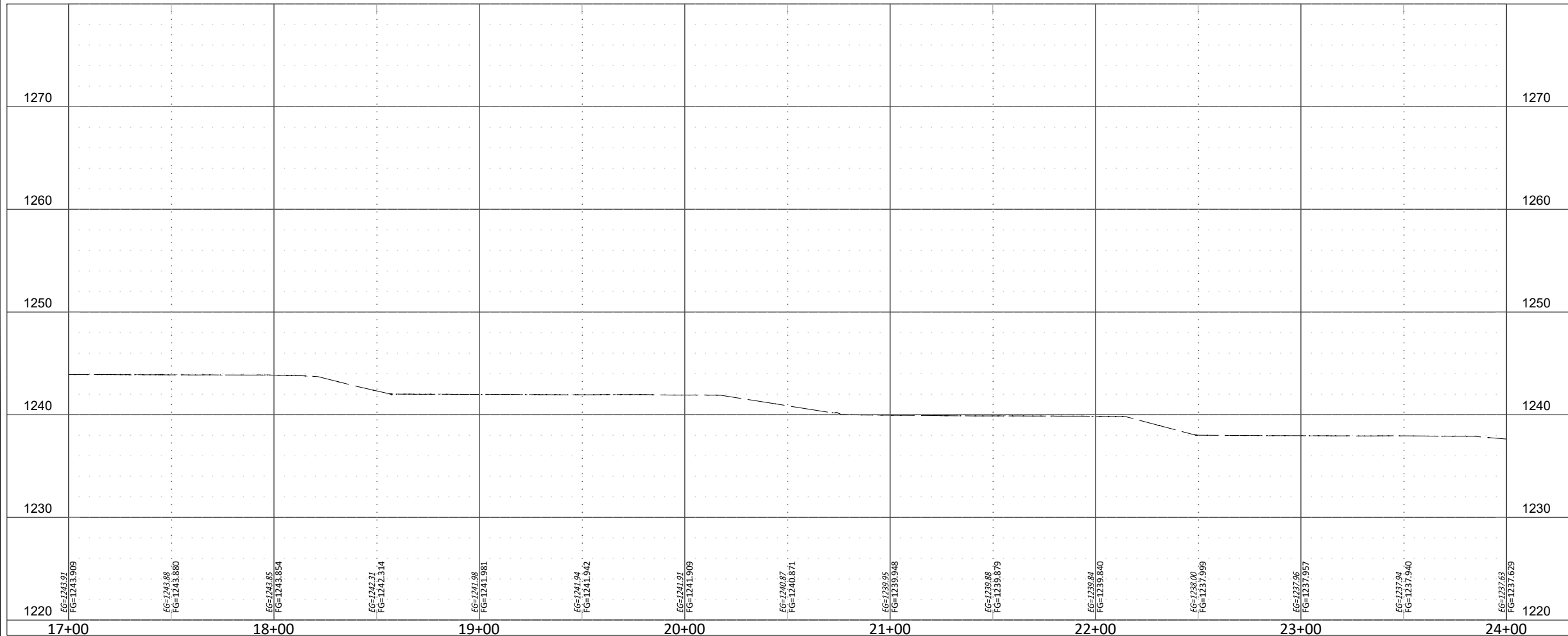
SHEET NUMBER
A2-1

Project #: M4-TS011

ENGINEERING
SURVEYING
PLANNING

SRB
SMITH, ROBERTS & BUDEN
CITY OF OKLAHOMA
CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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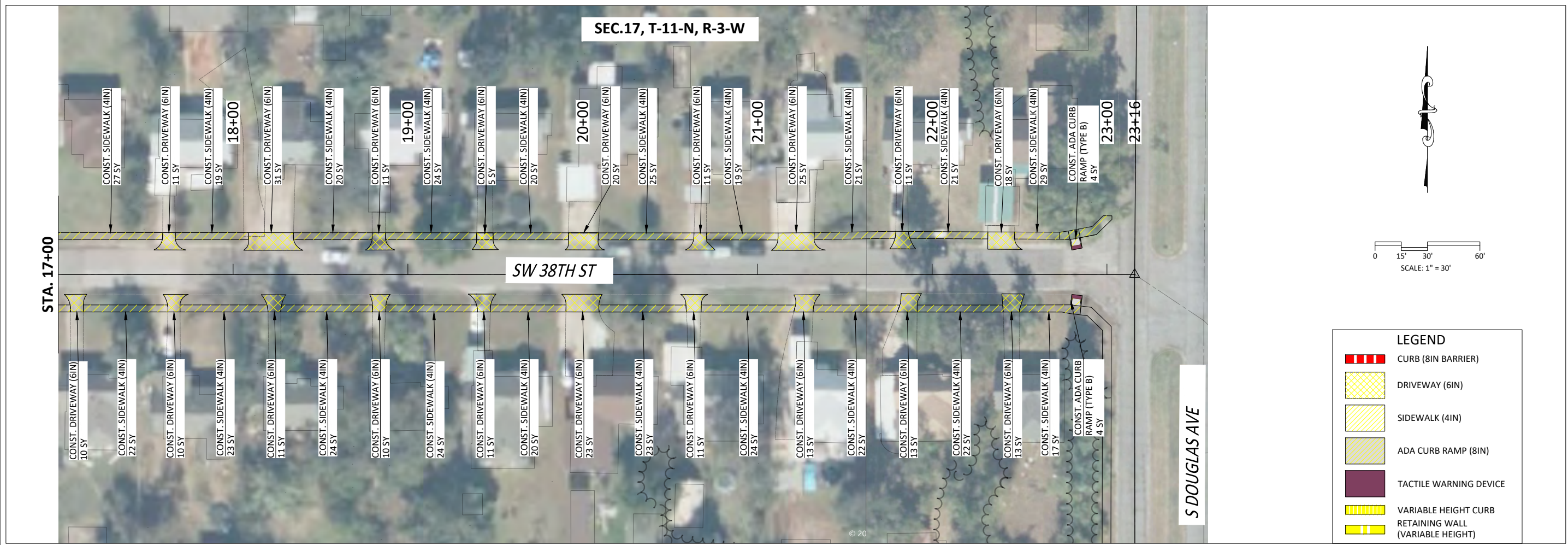
DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
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SHEET NUMBER
A2-2

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
38TH - P&P(2)

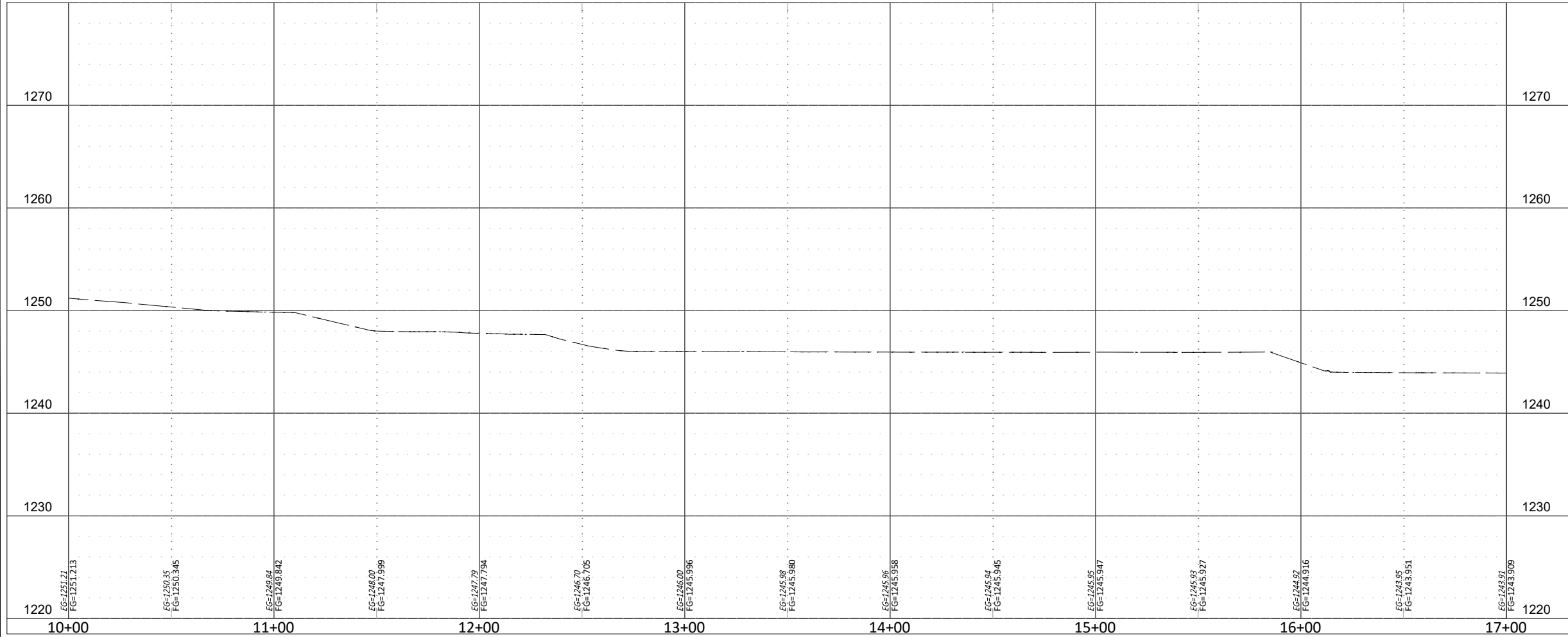


REVISIONS
DESCRIPTION

NO.	DATE	DESCRIPTION



12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



GENERAL NOTES:

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CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
39TH - P&P(1)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
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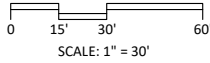
SHEET NUMBER
A3-1

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)



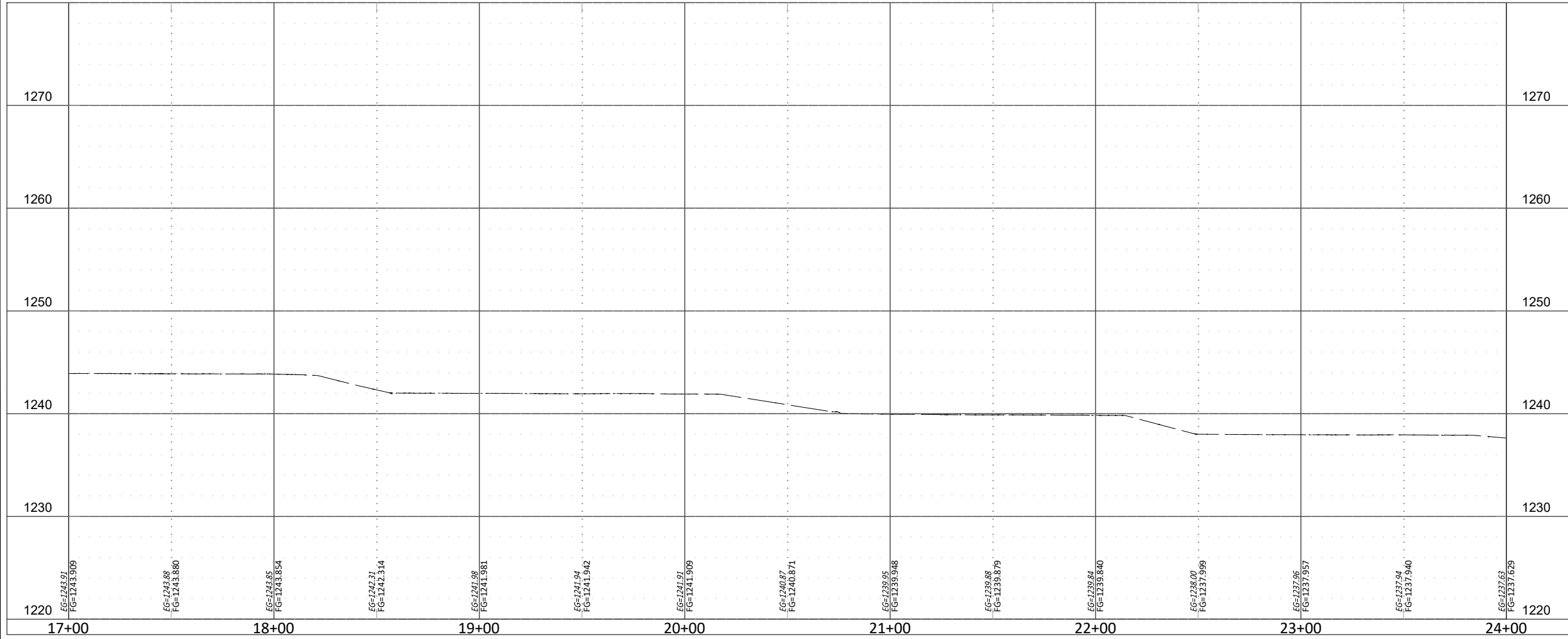
REVISIONS	
NO.	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

SMITH ROBERTS BUILDING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
39TH - P&P(2)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A3-2

Project #: M4-TS011



REVISIONS	
NO.	DESCRIPTION

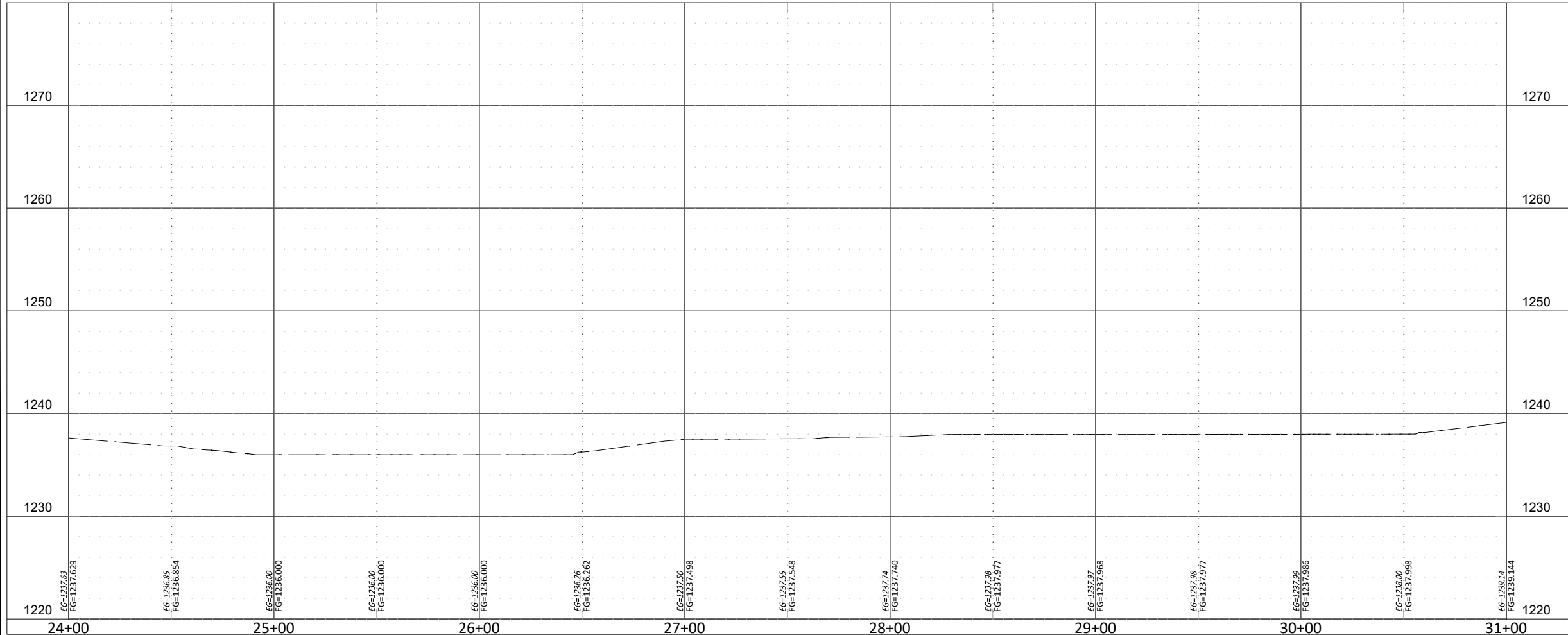
ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

SMITH
ROBERTS
BUDISCH
CITY

SRB

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



GENERAL NOTES:

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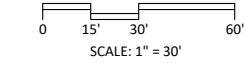
DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A3-3

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
39TH - P&P(3)



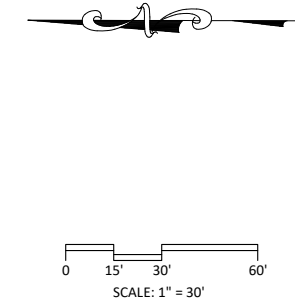
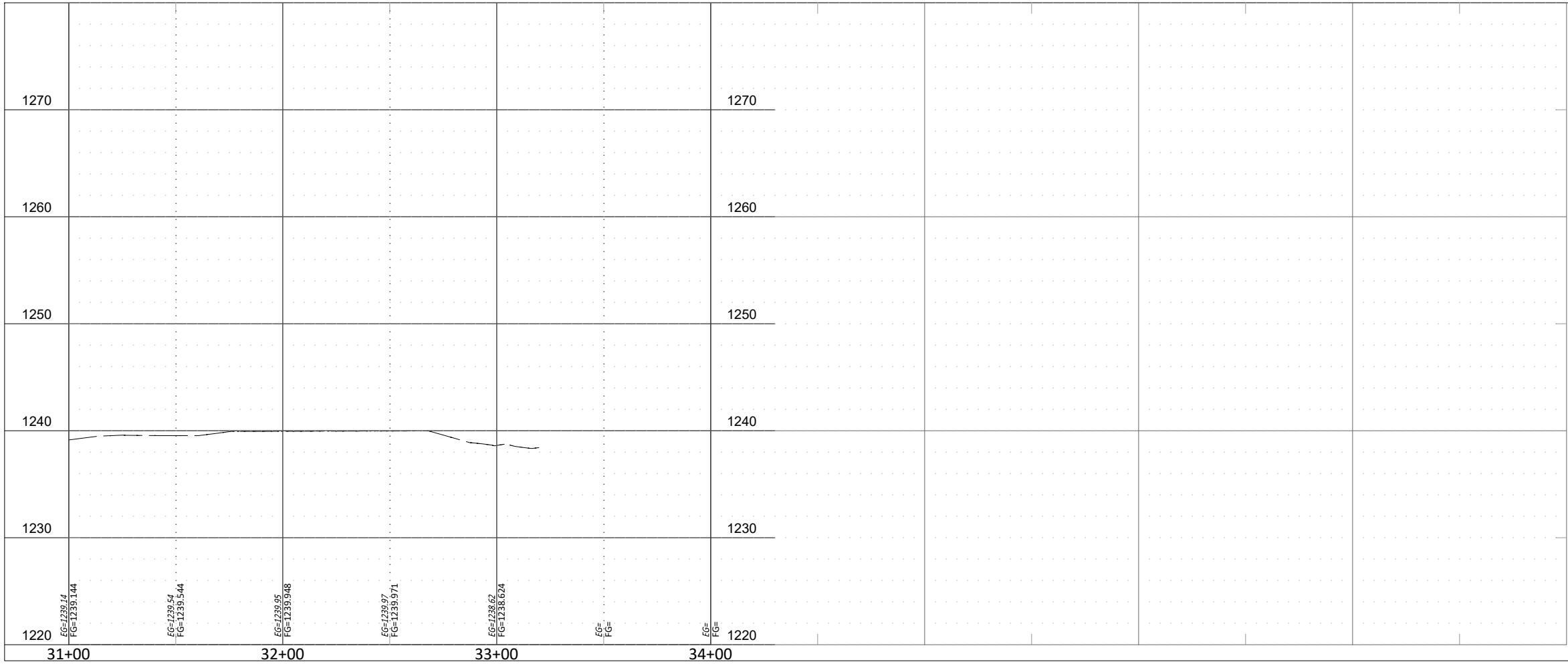
LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

NO.	DATE	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

GENERAL NOTES:

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CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
39TH - P&P(4)

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
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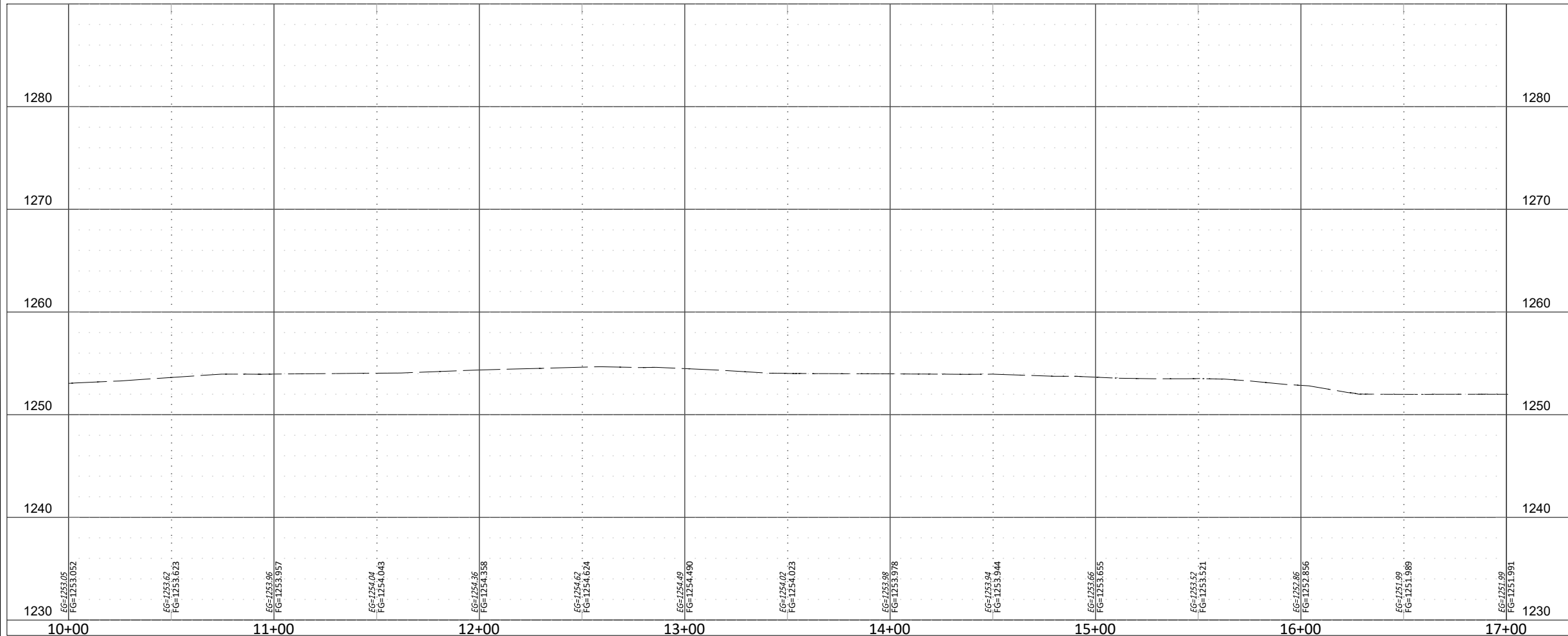
SHEET NUMBER
A3-4

Project #: M4-TS011

REVISIONS
DESCRIPTION

NO. DATE

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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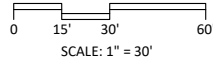
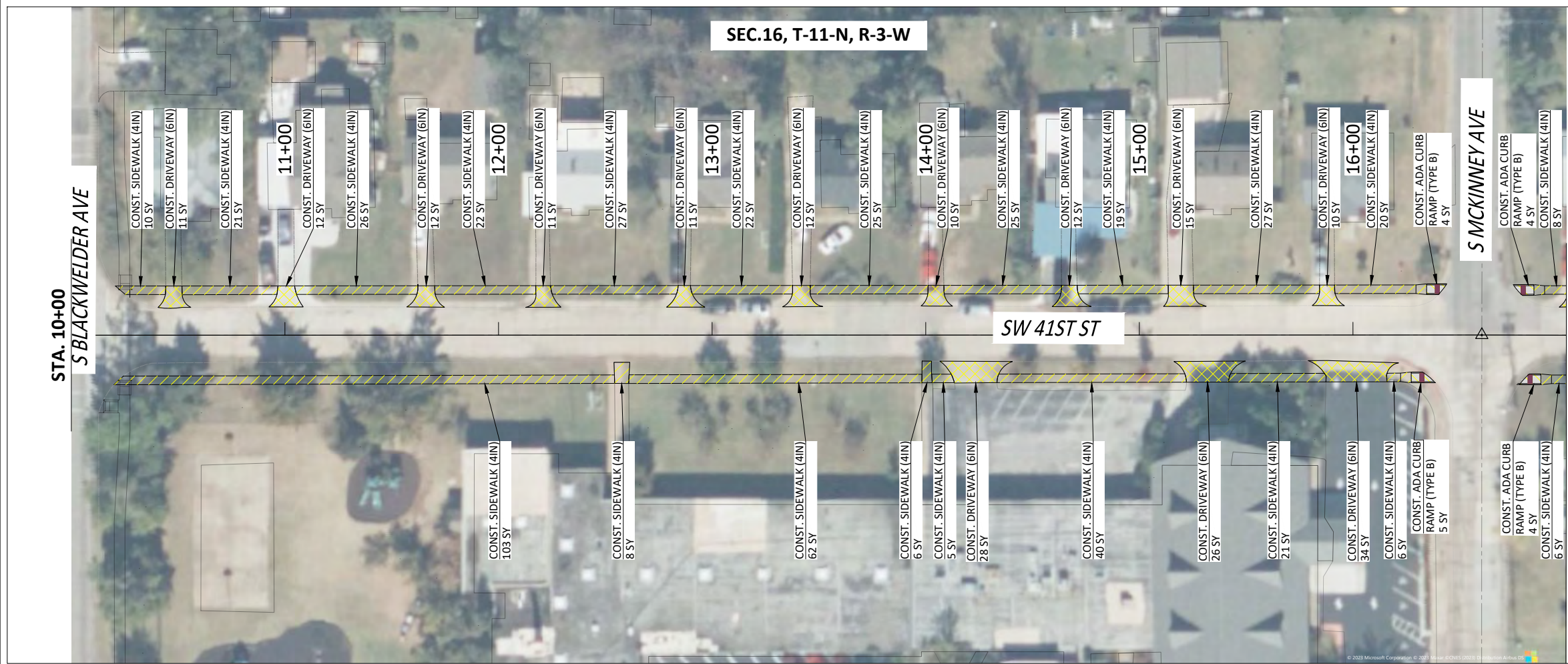
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VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A4-1

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
41ST - P&P(1)



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

NO.	DATE	DESCRIPTION

REVISIONS

DESCRIPTION

NO.

DATE

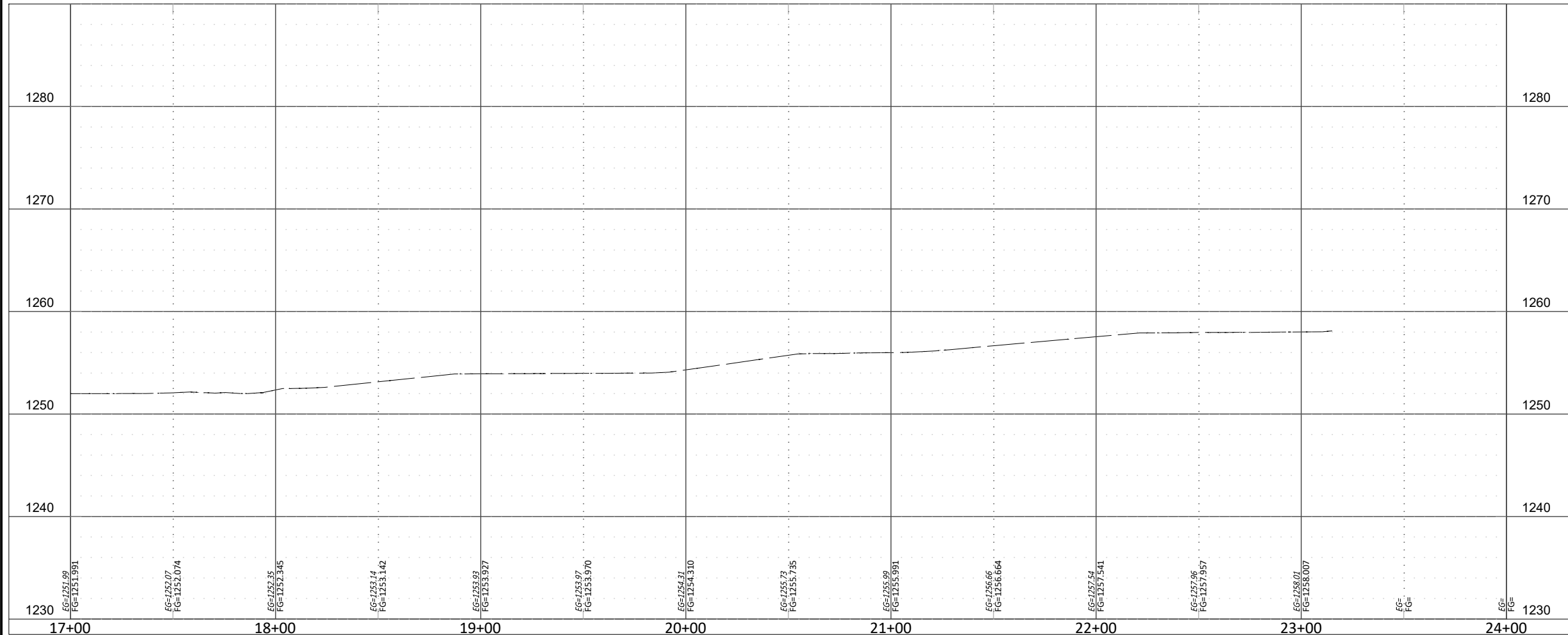
DESCRIPTION

ENGINEERING
SURVEYING
PLANNING



CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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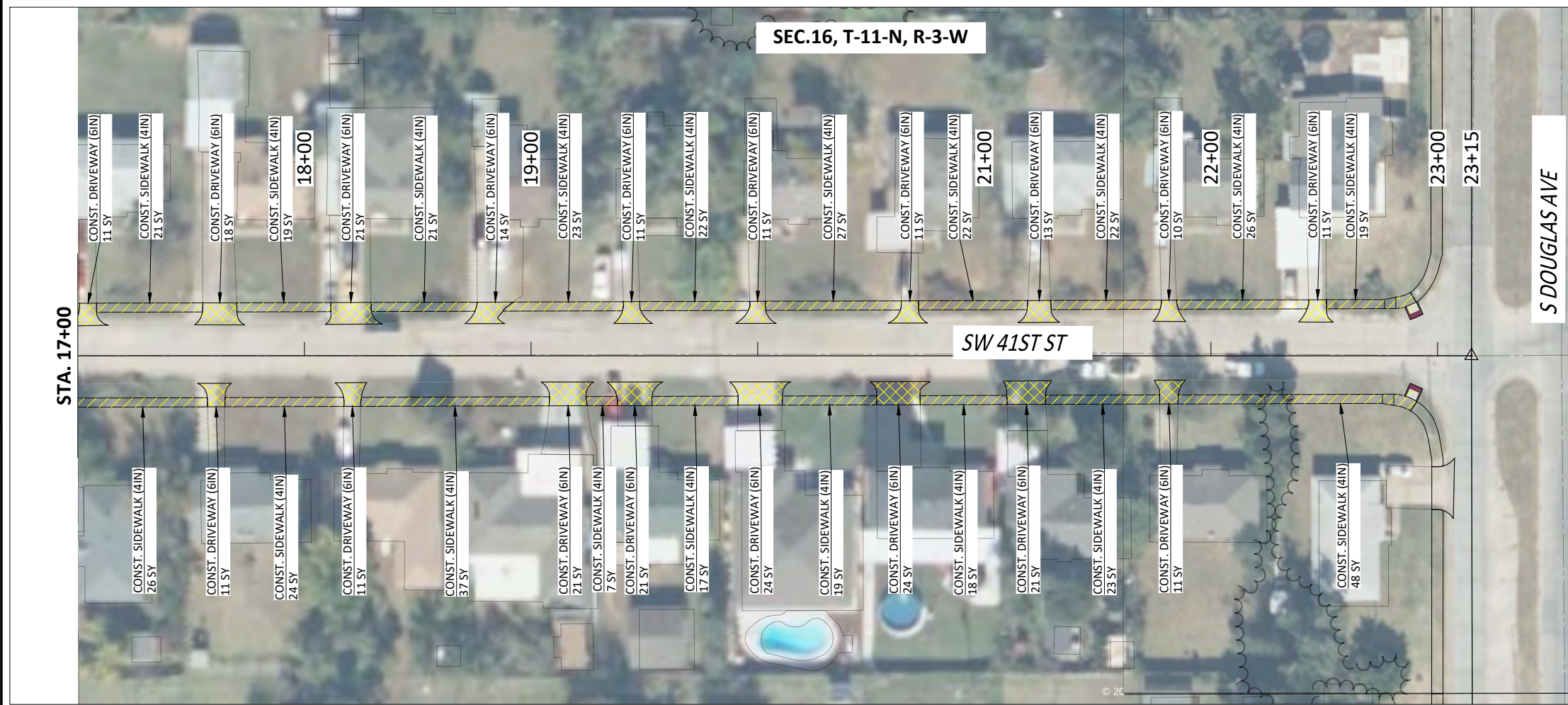
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DESIGN BY:	GJW
DRAWN BY:	DHB
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VERIFY SCALE
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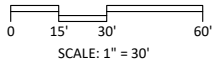
SHEET NUMBER
A4-2

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
41ST - P&P(2)



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)



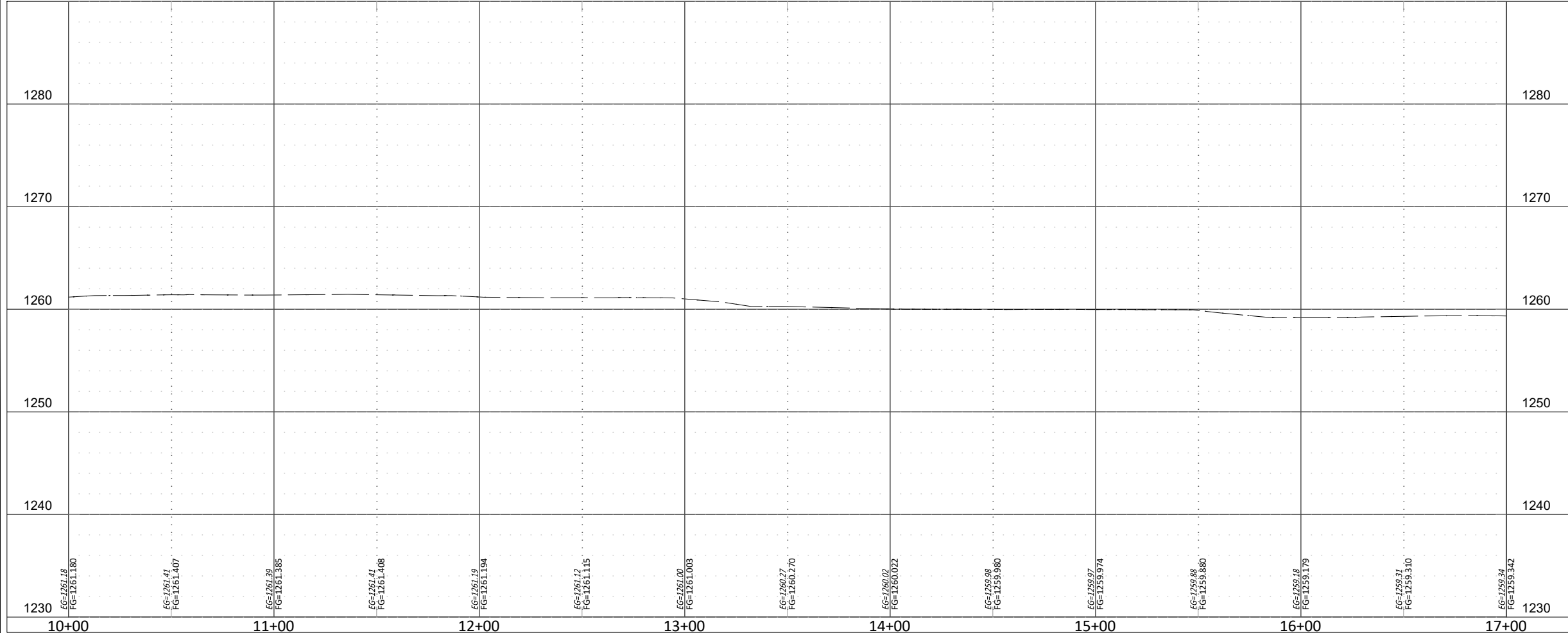
REVISIONS	
NO.	DESCRIPTION

SMITH, ROBERTS & BUDDESMITH, LLC

**ENGINEERING
SURVEYING
PLANNING**

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



GENERAL NOTES:

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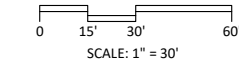


DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A5-1

Project #: M4-TS011



SCALE: 1" = 30'



LEGEND

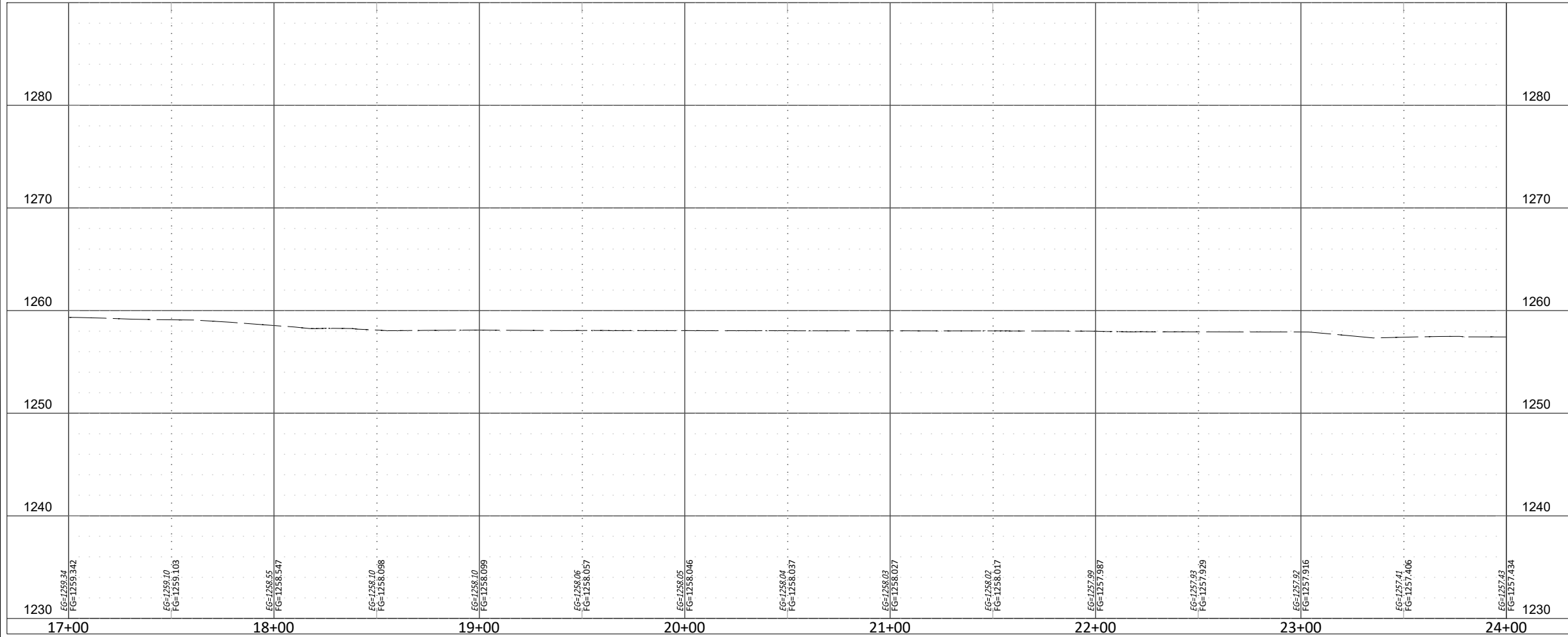
- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
DOUGLAS - P&P(1)

REVISIONS
DESCRIPTION

NO. DATE

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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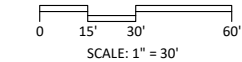
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VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
A5-2

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
DOUGLAS - P&P(2)



LEGEND

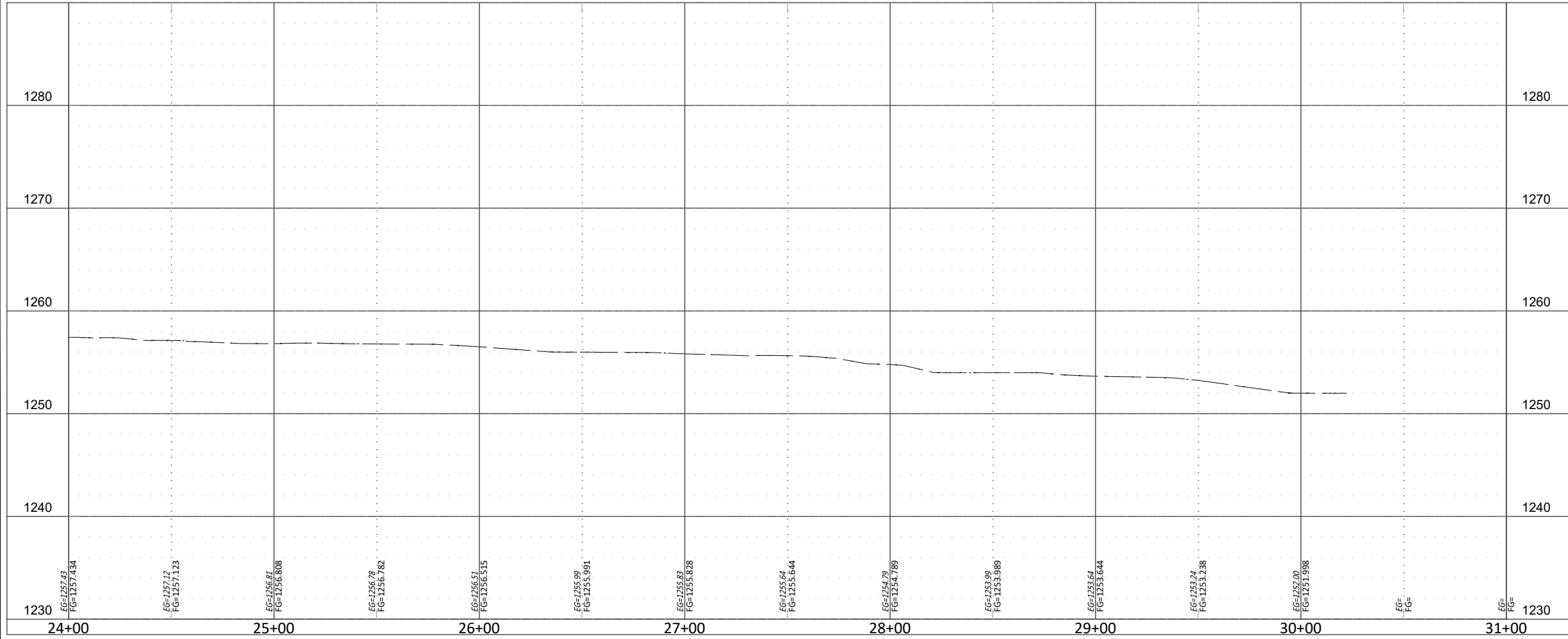
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- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS

DESCRIPTION

NO. DATE

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2A-PROF.DWG



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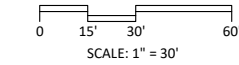
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DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
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SHEET NUMBER
A5-3

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
DOUGLAS - P&P(3)



LEGEND

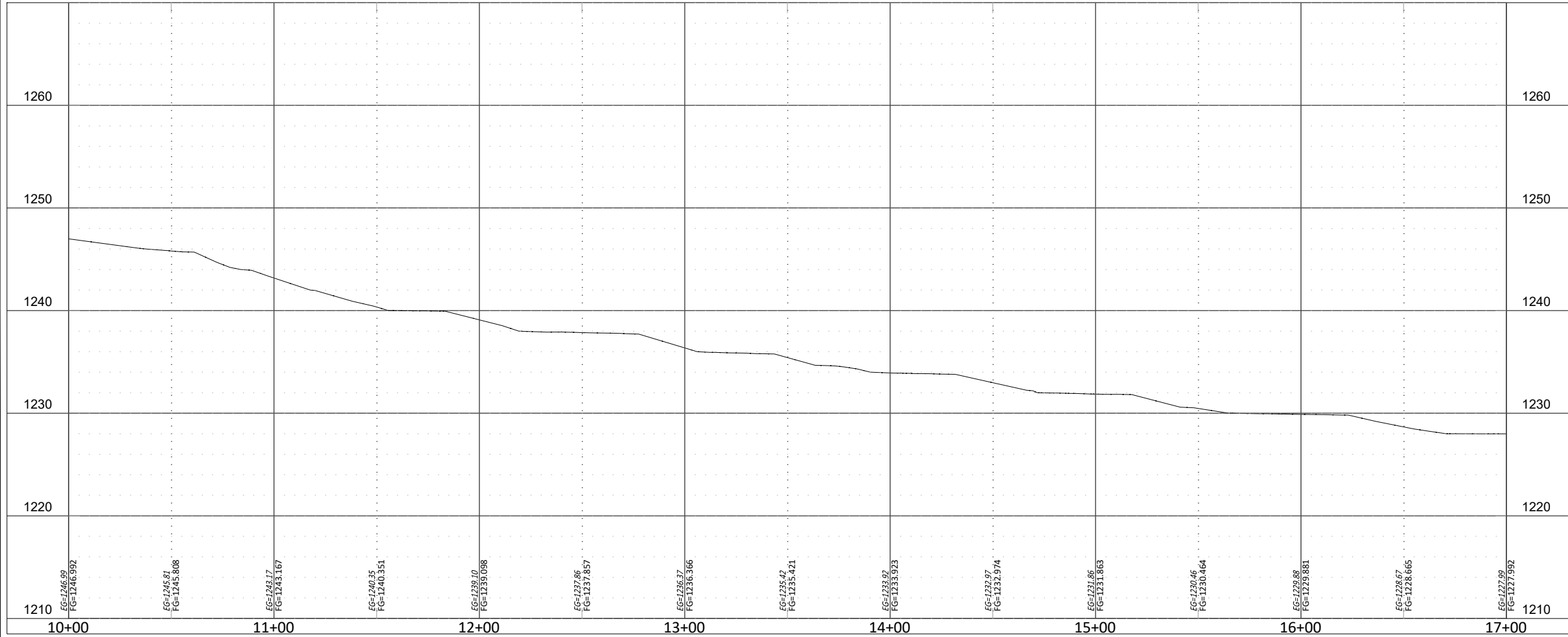
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- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS
DESCRIPTION

NO. DATE



12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



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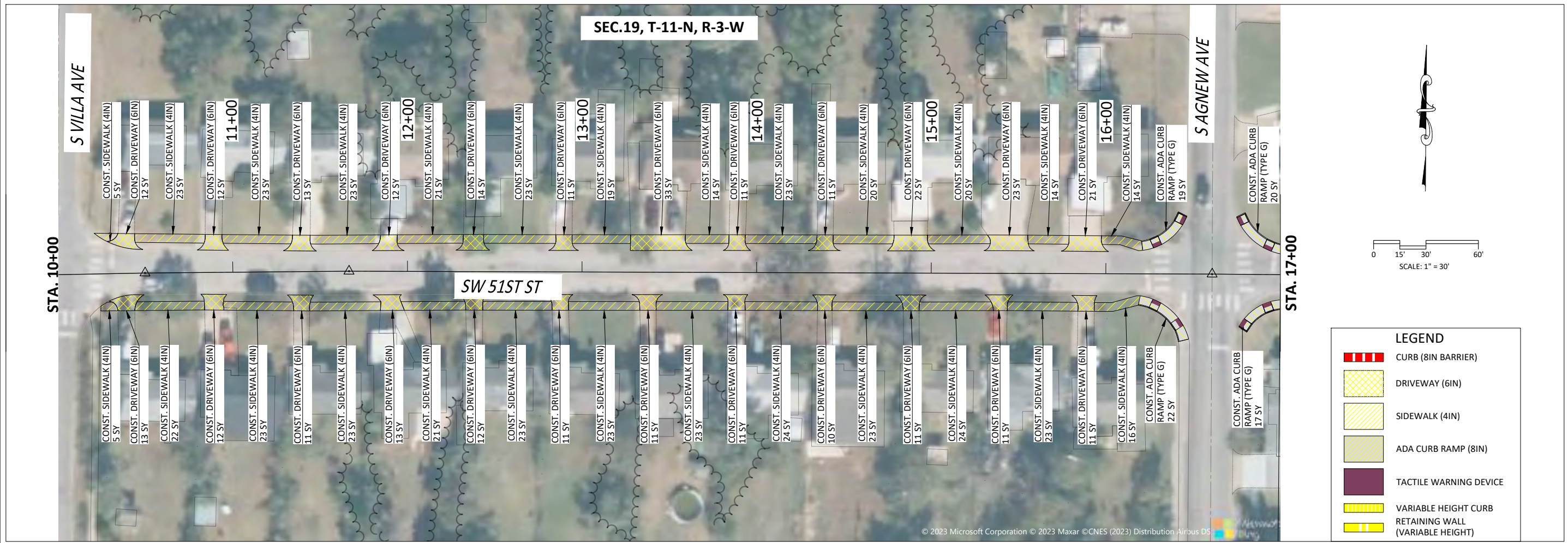
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST - P&P(1)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-1

Project #: M4-TS011



0 15' 30' 60'
SCALE: 1" = 30'

LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS
DESCRIPTION

NO. DATE

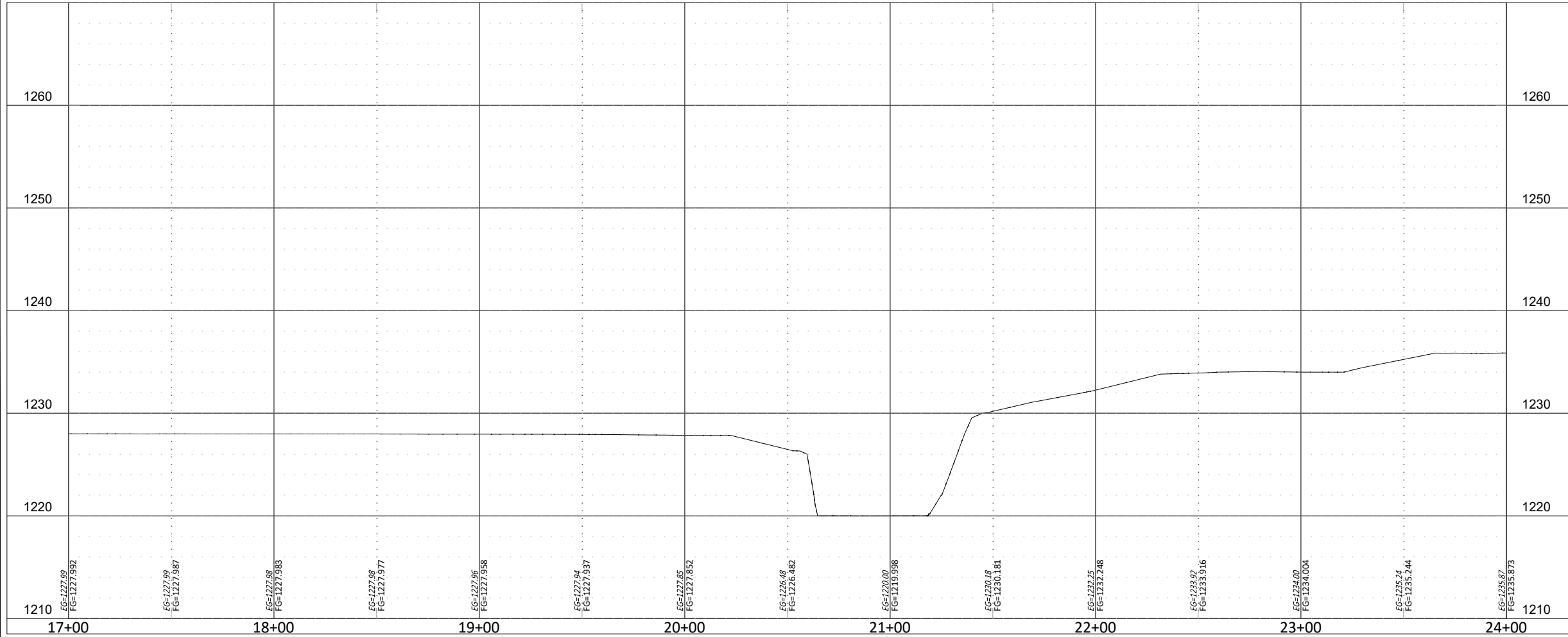
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DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-1

Project #: M4-TS011

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



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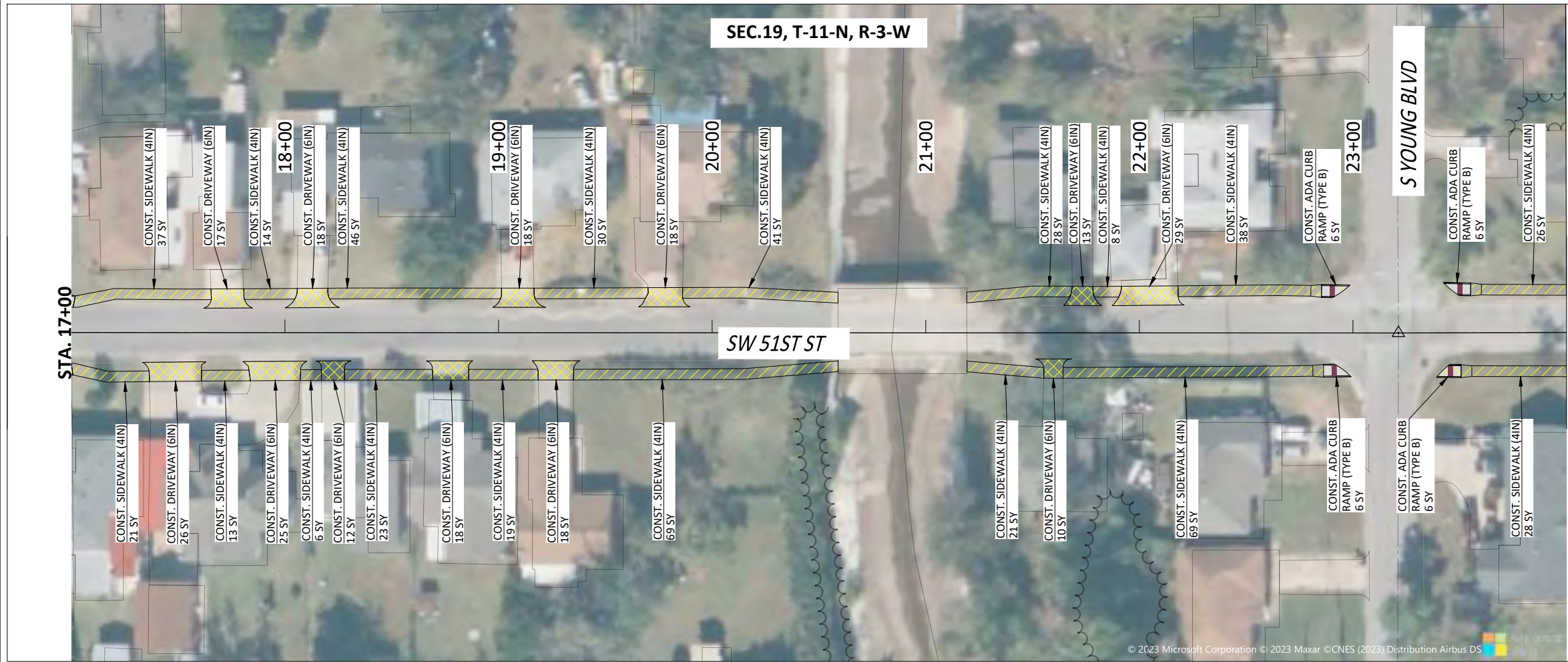
DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-2

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST - P&P(2)



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

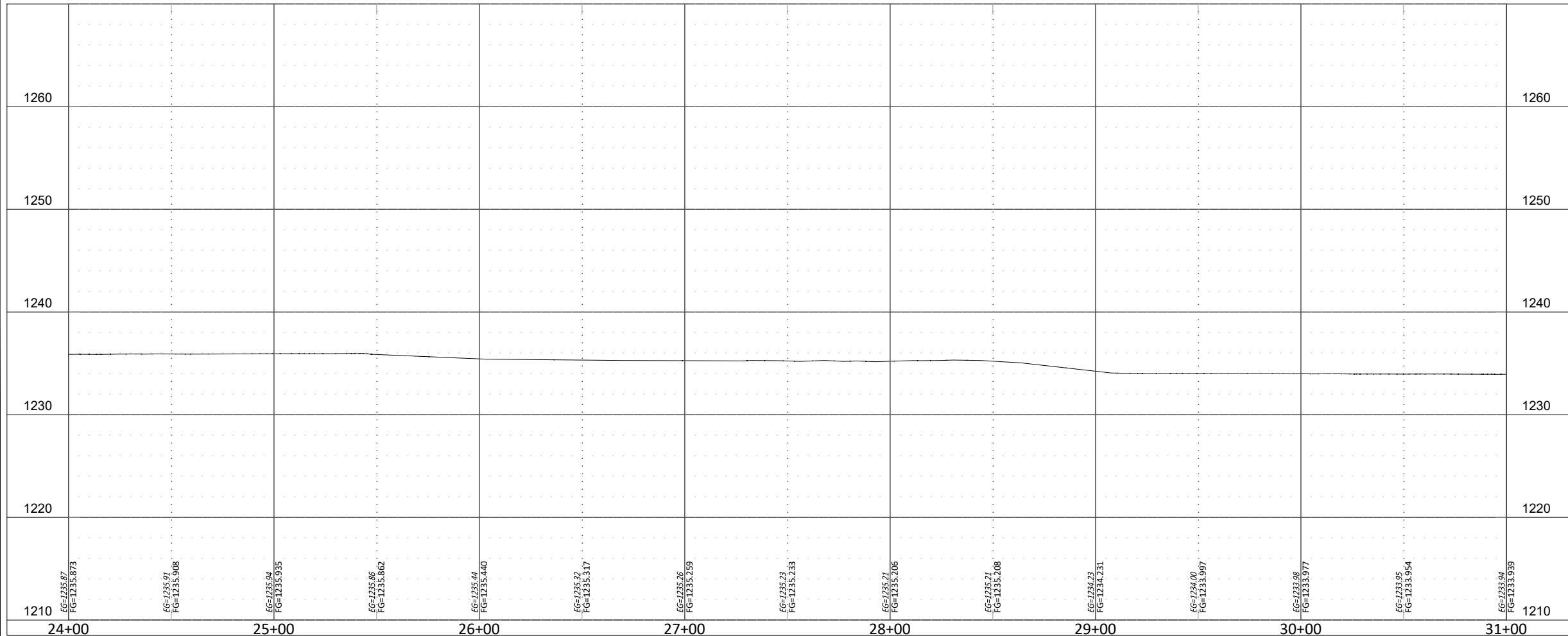
ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

SMITH
ROBERTS
SRB
BUILDING

DTT
BELL

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



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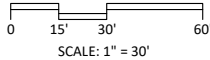
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST- P&P(3)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-3

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS
DESCRIPTION

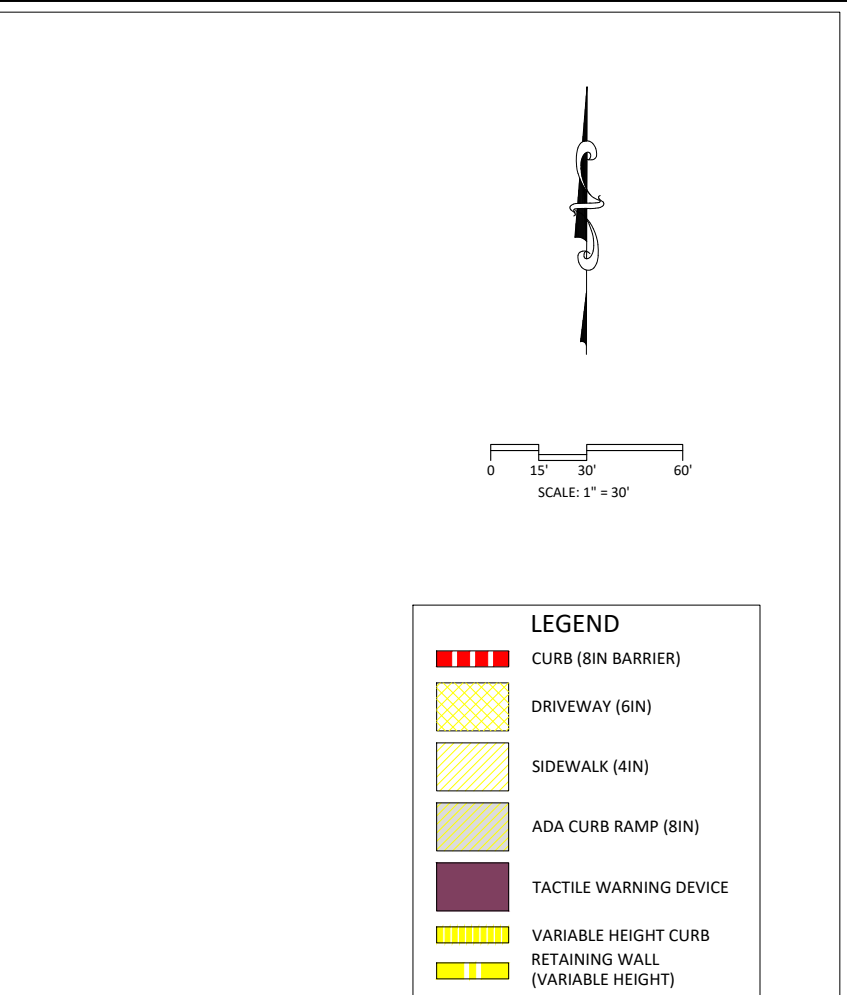
NO. DATE

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-3

Project #: M4-TS011



1. CENTERLINE PROFILE CREATED BY USING 2 FT. CONTOUR FROM OKC OPEN DATA PORTAL.
2. A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.

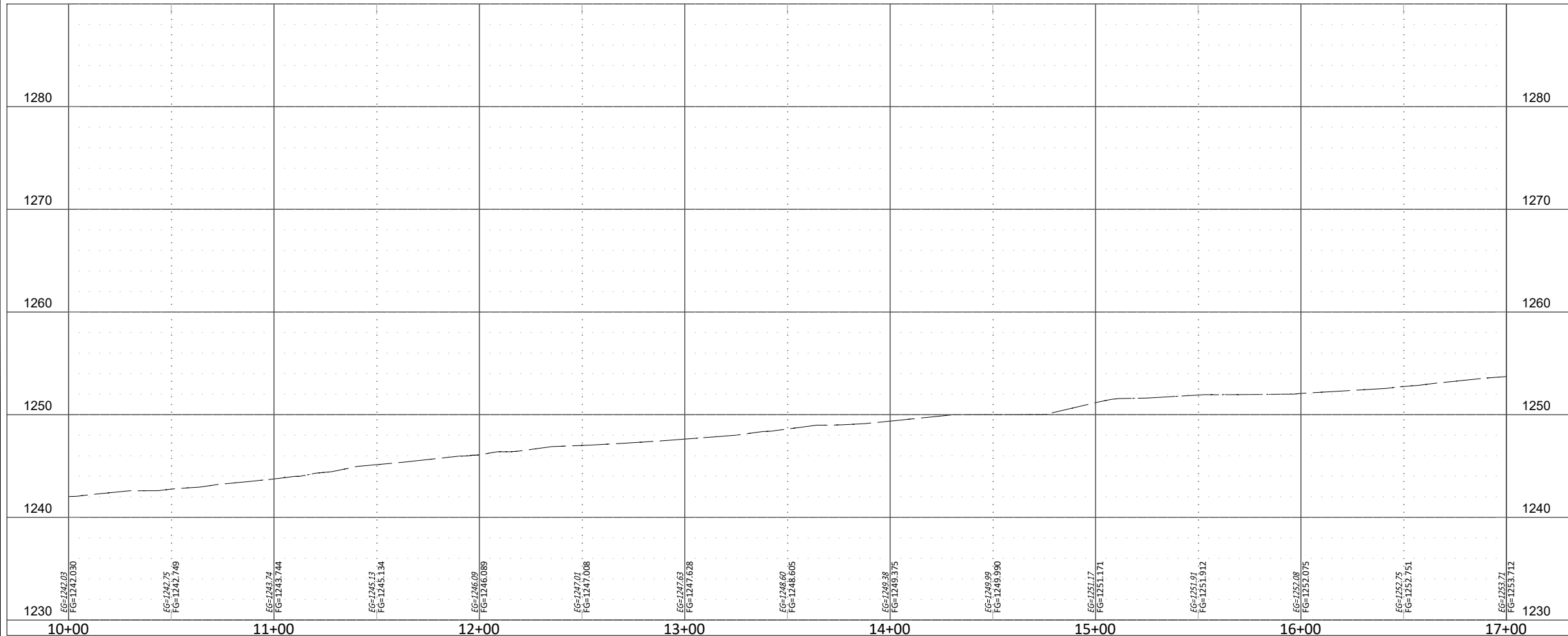
Project #: M4-TS011[illegible]

**CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST- P&P(4)**



CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



GENERAL NOTES:

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DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

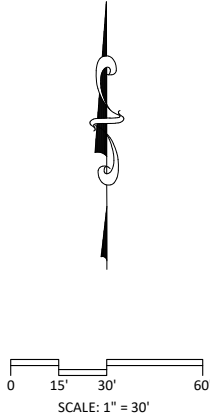
VERIFY SCALE
0 1" 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-5

Project #: M4-TS011



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

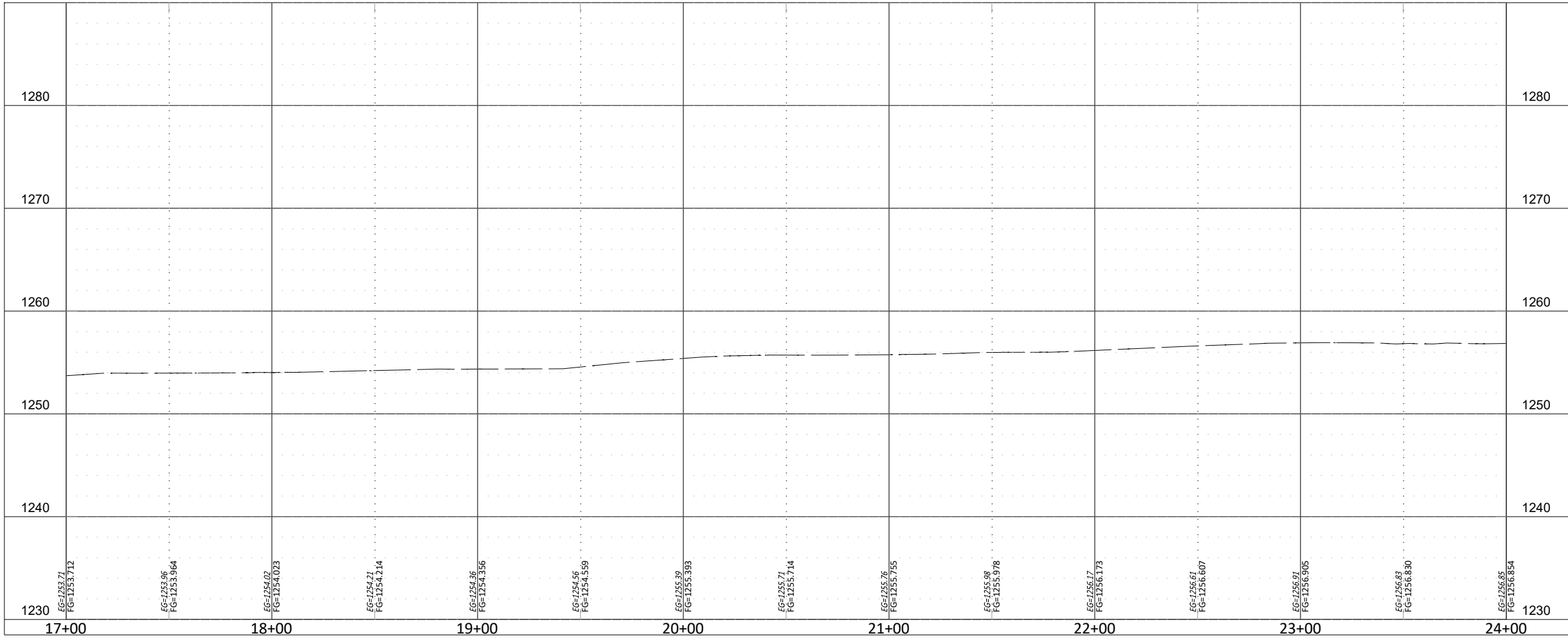


REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST-P&P(5)



12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



GENERAL NOTES:

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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B1-6

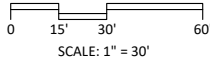
Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST- P&P(6)



STA. 24+00

LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)



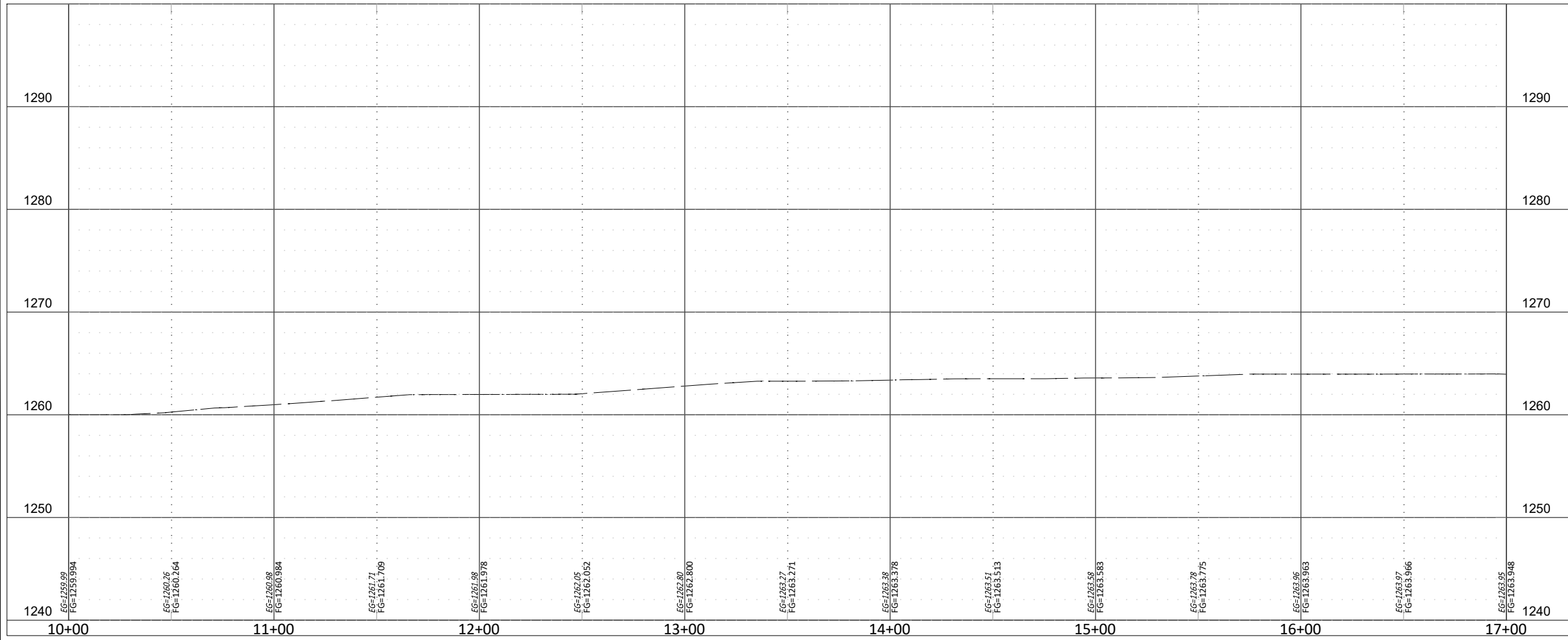
REVISIONS	
NO.	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

SMITH
ROBERTS
SRB
BUDGET
CITY

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



GENERAL NOTES:

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DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
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SHEET NUMBER
B1-8

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST - P&P(8)



NO.	DATE	DESCRIPTION

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



GENERAL NOTES:

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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE

0 1"

BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
51ST- P&P(9)



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

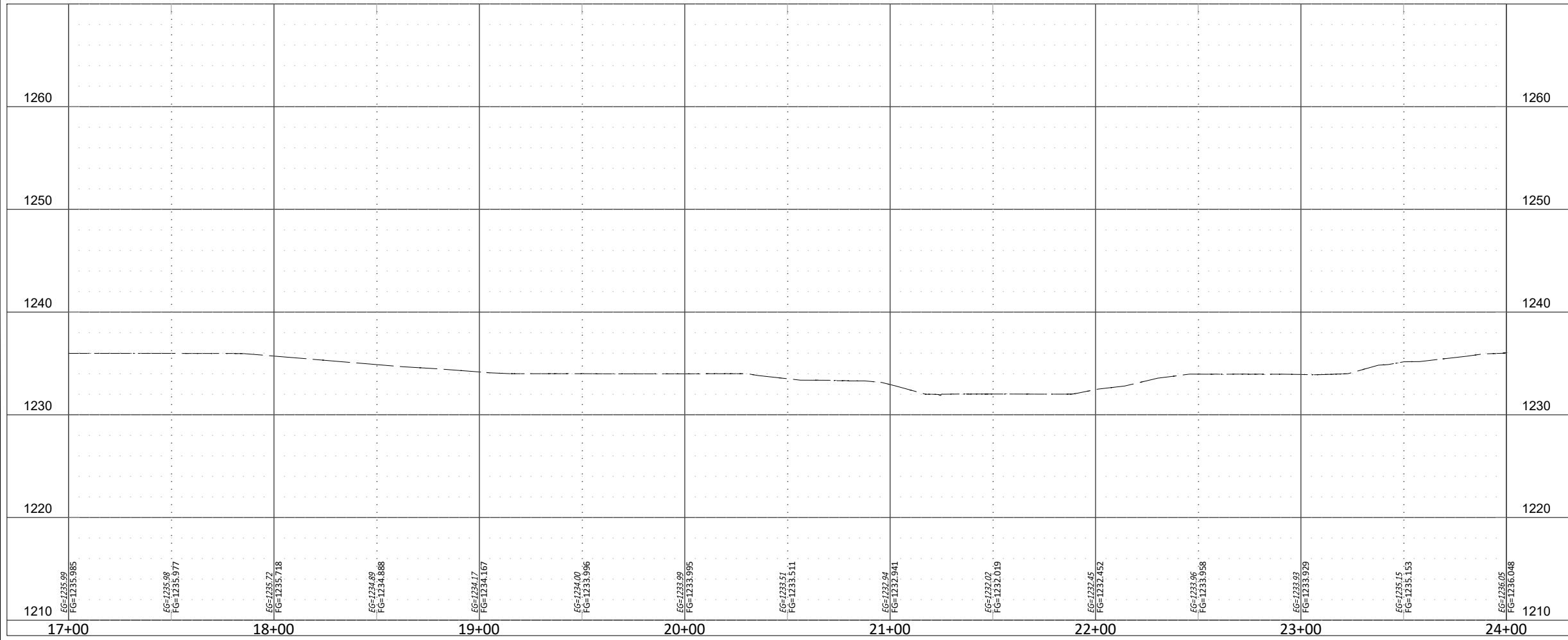
REVISIONS	
NO.	DESCRIPTION



1. CENTERLINE PROFILE CREATED BY USING 2 FT. CONTOUR FROM OKC OPEN DATA PORTAL.
2. A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.



12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



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CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
55TH - P&P(2)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

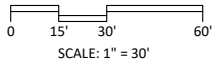
SHEET NUMBER
B2-2

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)



REVISIONS	
NO.	DESCRIPTION

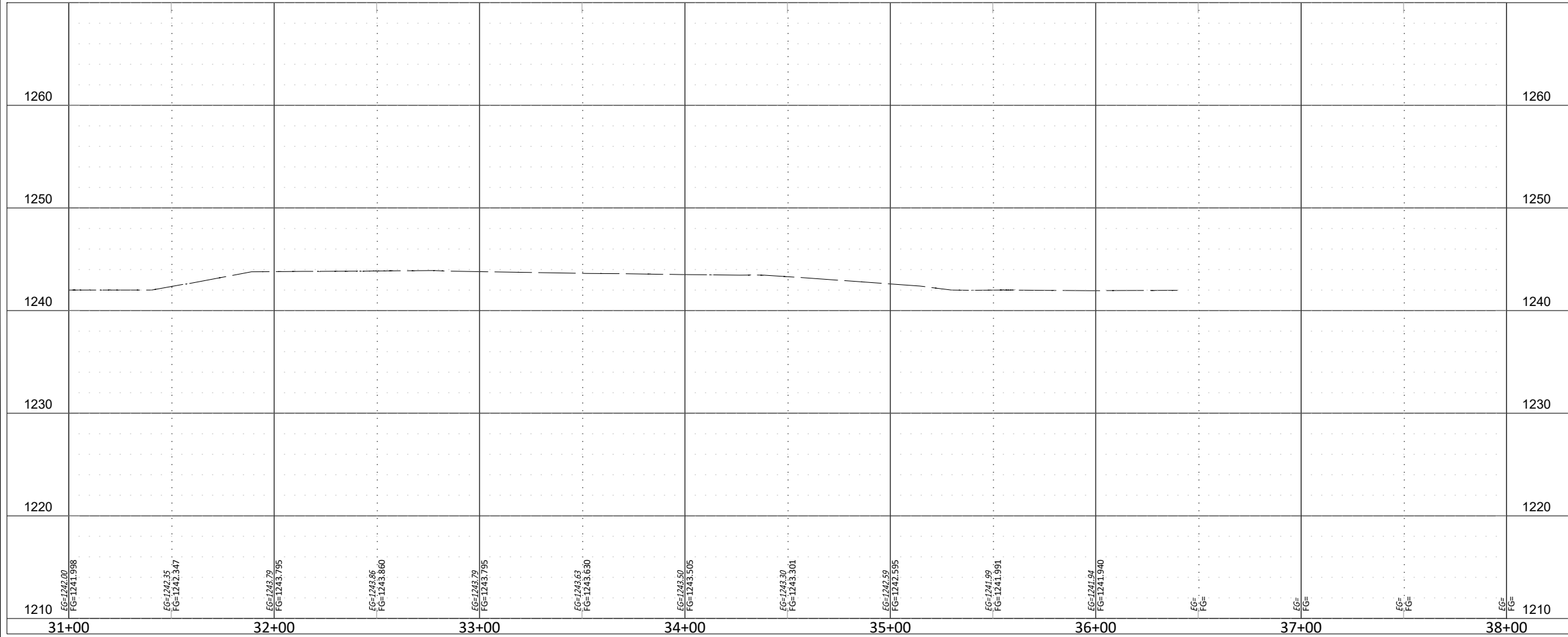
ENGINEERING
SURVEYING
PLANNING

SRB

SMITH, HILL, ROBERTS & BUDENCO, LLC

CITY OF OKLAHOMA
CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2B-PROF.DWG



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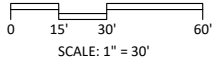
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
55TH - P&P(4)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
B2-4

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS
DESCRIPTION

NO. DATE

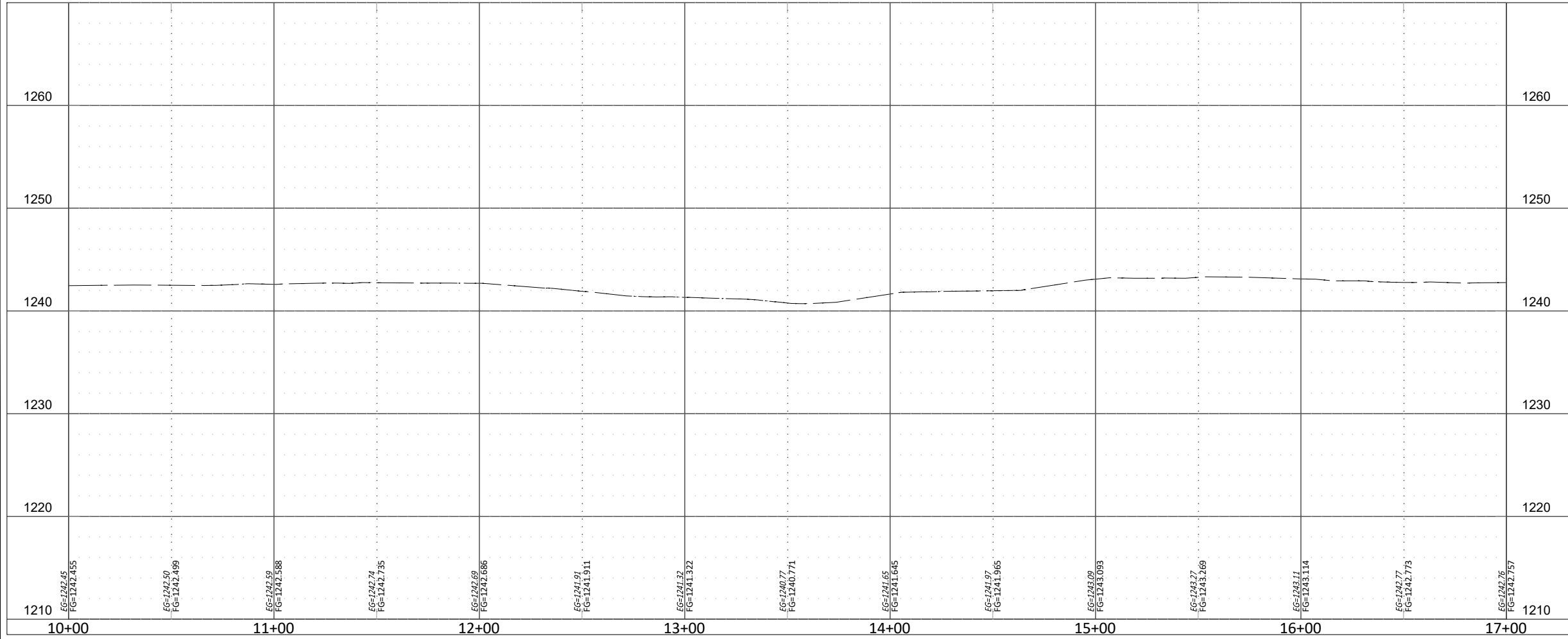
DATE: 12.13.23
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DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
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SHEET NUMBER
B2-4

Project #: M4-TS011

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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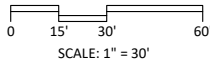
DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C1-1

Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
KINGRIDGE DR-P&P - (1)



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

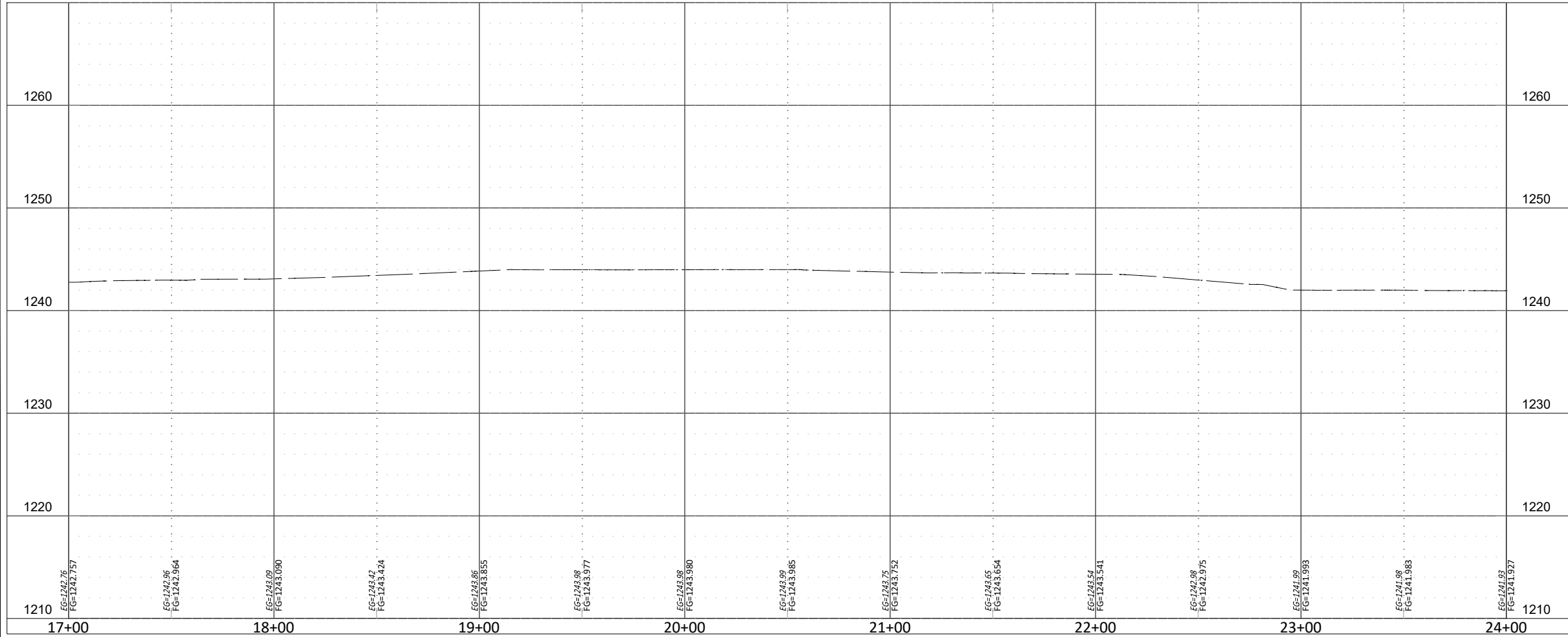
REVISIONS
DESCRIPTION

NO. DATE



CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C1-2
Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
KINGRIDGE DR-P&P - (2)



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

NO.	DATE	DESCRIPTION

REVISIONS

DESCRIPTION

DATE

NO.

DATE:

DESIGN BY:

DRAWN BY:

CHECKED BY:

VERIFY SCALE

0 1"

BAR IS ONE INCH ON ORIGINAL

DRAWING. IF NOT ONE INCH ON

THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER

C1-2

Project #:

M4-TS011

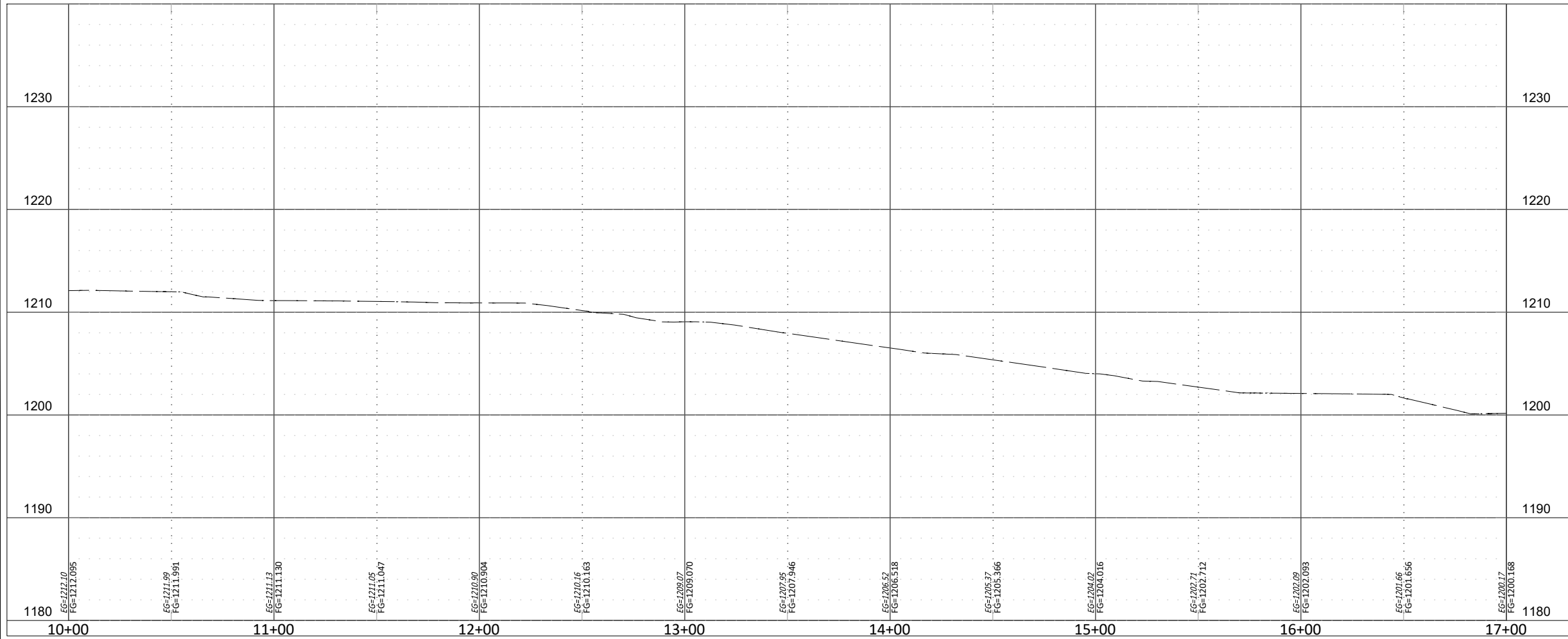
CITY OF OKLAHOMA CITY, OKLAHOMA

MAPS 4 SIDEWALK AND RELATED AMENITIES

CIVIL - PLAN & PROFILE

KINGRIDGE DR-P&P - (2)

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C2-1

Project #: M4-TS011



STA. 17+00

STA. 10+00



0 15' 30' 60'
SCALE: 1" = 30'

LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
WESTERN AVE-P&P - (8)

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C2-1

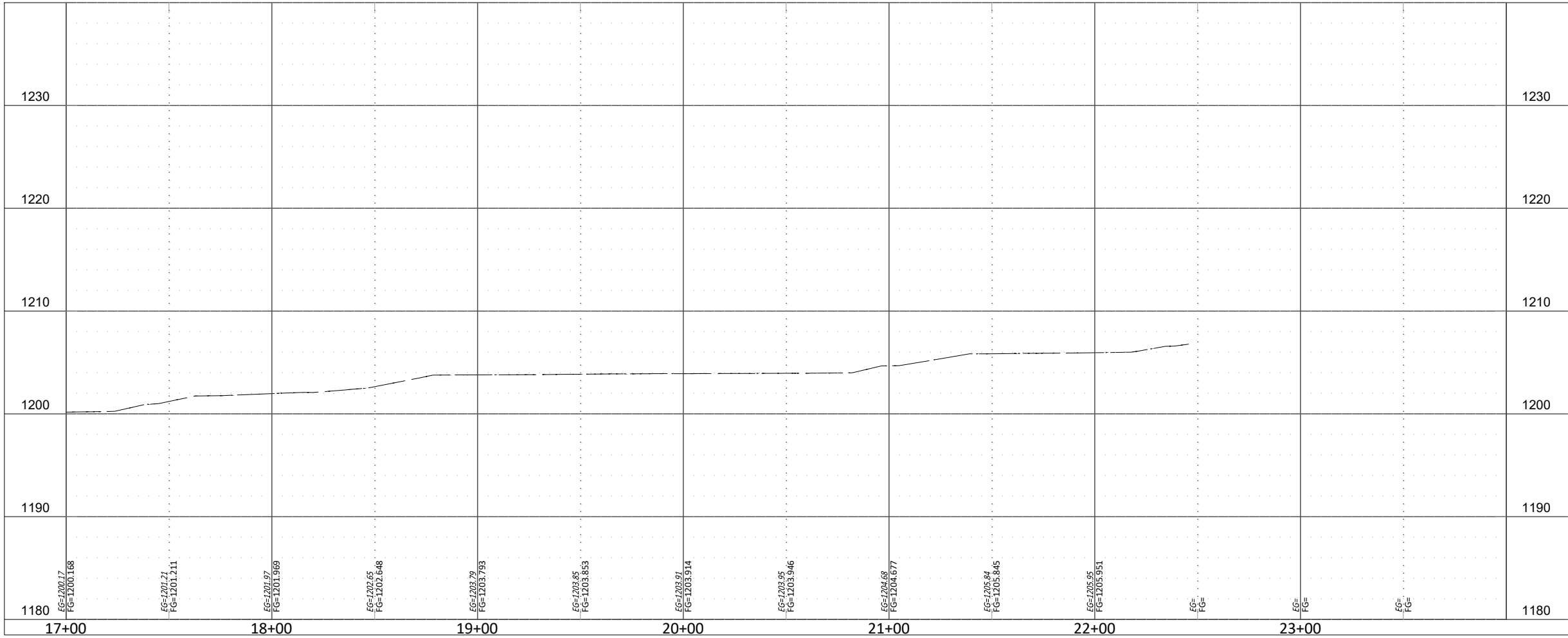
Project #: M4-TS011

ENGINEERING
SURVEYING
PLANNING

ROBERTS
SRB
SMITH
BUDISH

CITY OF OKLAHOMA
CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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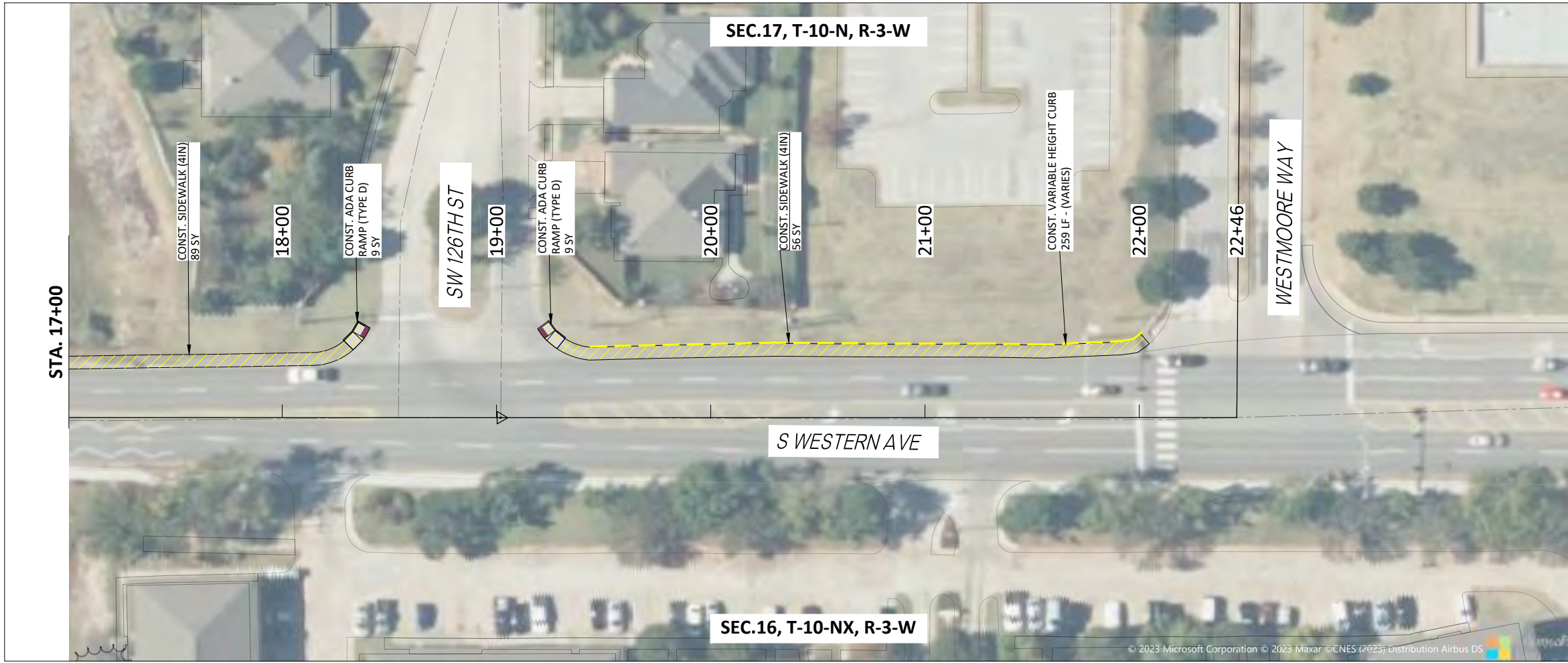
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
WESTERN AVE-P&P - (9)

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

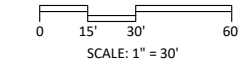
SHEET NUMBER
C2-2

Project #: M4-TS011



STA. 17+00

STA. 24+00



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

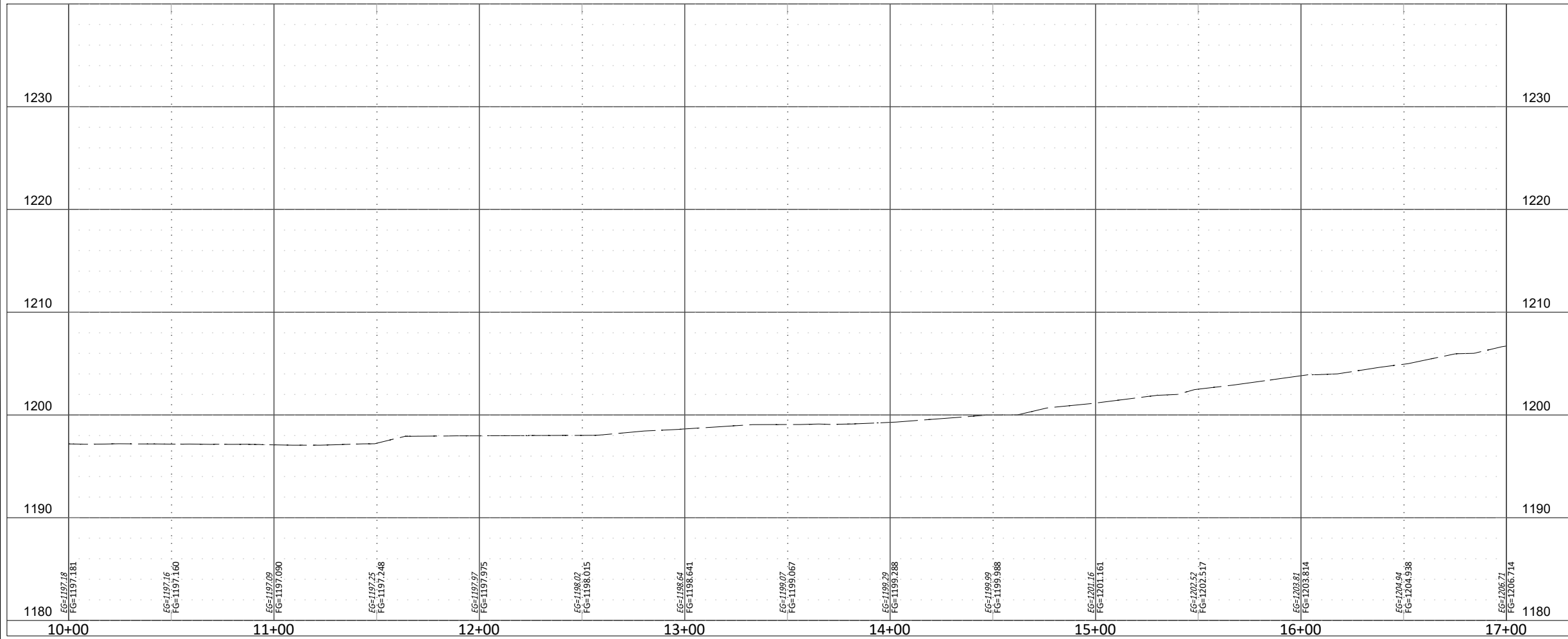
REVISIONS
DESCRIPTION

NO. DATE



CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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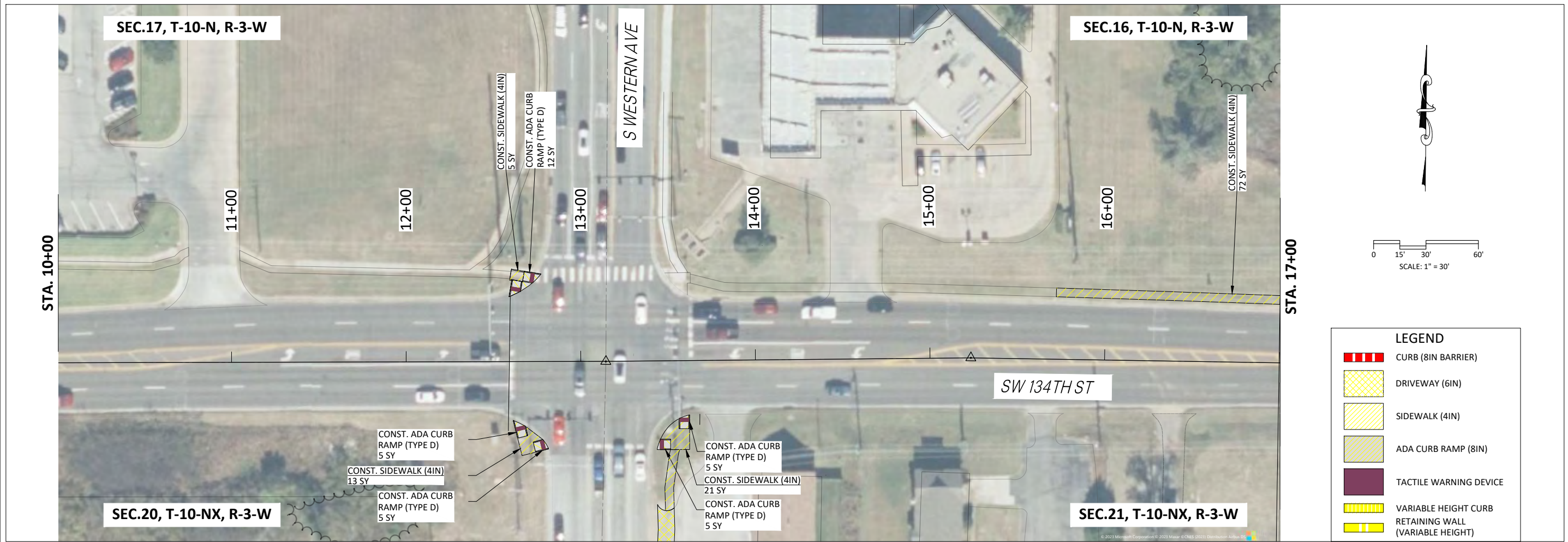
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (1)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C3-1

Project #: M4-TS011



ENGINEERING SURVEYING PLANNING

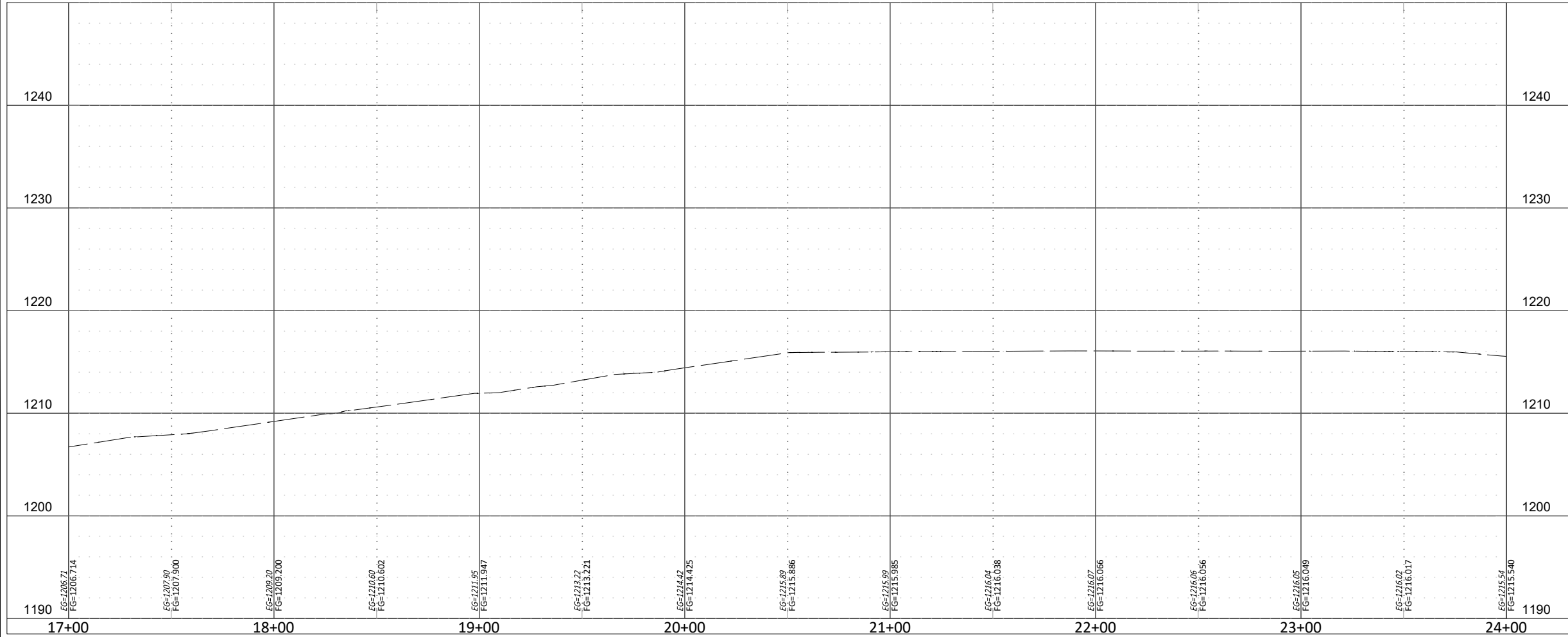
SRB

SMITH, H. T. ROBERTS, J. T. BUDISH, W. L. WILLIAMS

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

REVISIONS		DESCRIPTION	
NO.	DATE	DESCRIPTION	DATE

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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CHECKED BY:	BE

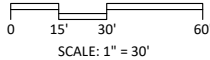
VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C3-2
Project #: M4-TS011

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (2)



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)



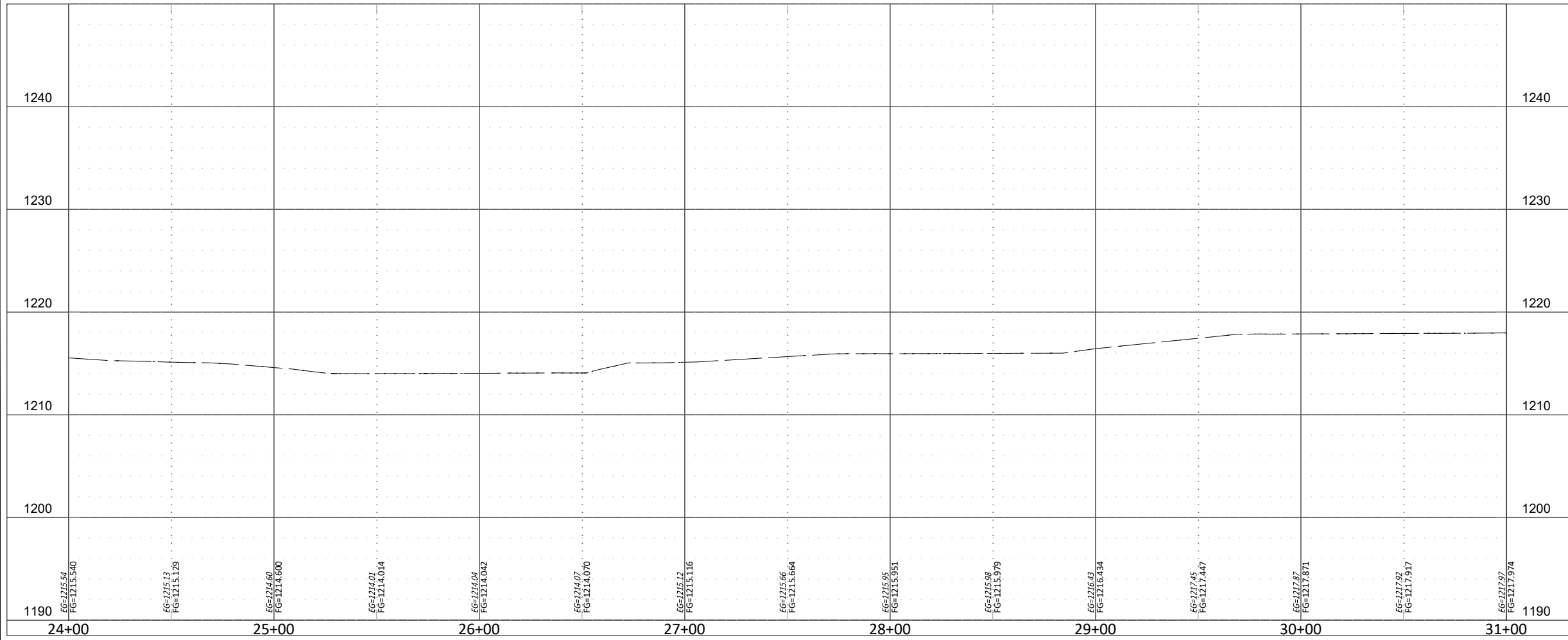
REVISIONS	
NO.	DESCRIPTION

ENGINEERING
SURVEYING
PLANNING

SRB

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C3-3

Project #: M4-TS011



ENGINEERING
SURVEYING
PLANNING

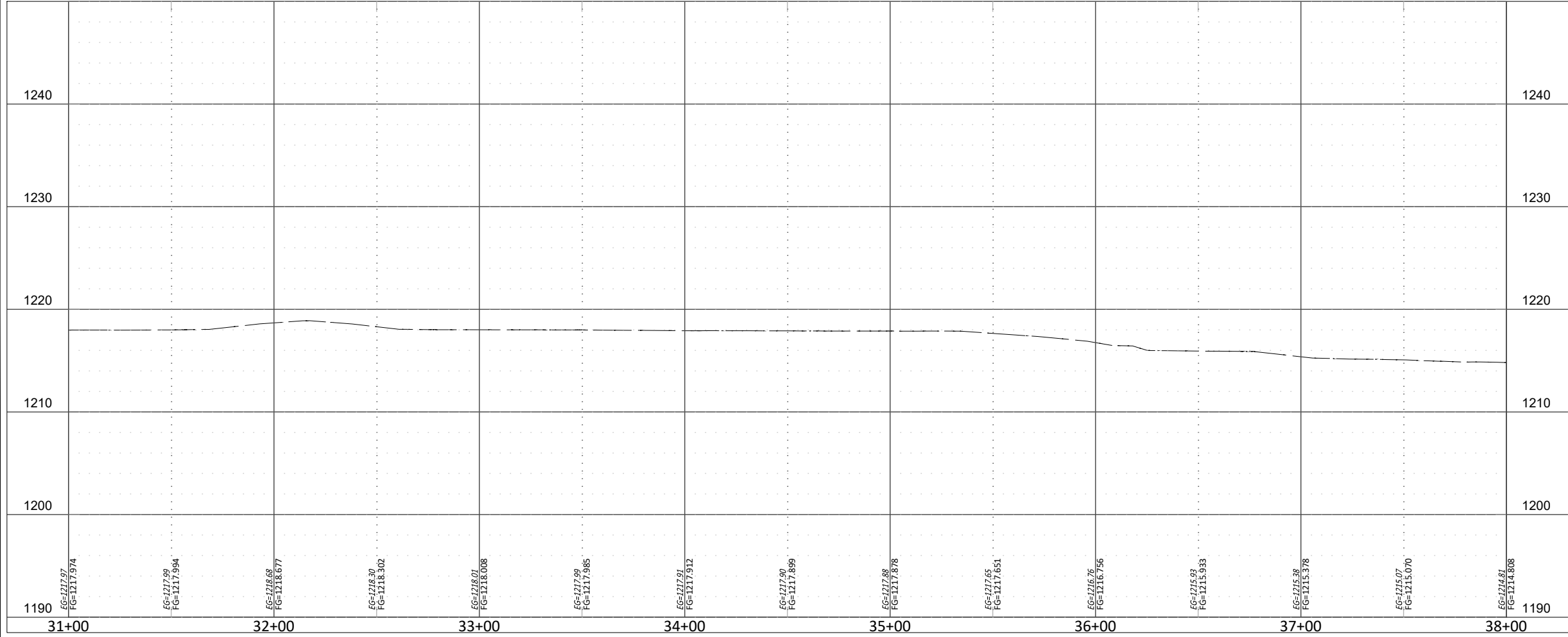
ROBERTS
SRB
SMITH
BUDISH

CITY OF OKLAHOMA
CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

REVISIONS			
NO.	DATE	DESCRIPTION	

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (3)

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



GENERAL NOTES:

- CENTERLINE PROFILE CREATED BY USING 2 FT. CONTOUR FROM OKC OPEN DATA PORTAL.
- A TITLE COMMITMENT, INDICATING APPLICABLE EASEMENTS, HAS NOT BEEN PROVIDED, THEREFORE ALL EASEMENTS MAY NOT BE SHOWN HEREON.

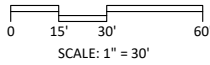


DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C3-4

Project #: M4-TS011



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS
DESCRIPTION

DATE

NO.

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (4)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1"
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SHEET NUMBER
C3-4

Project #: M4-TS011

REVISIONS
DESCRIPTION

DATE

NO.

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (4)

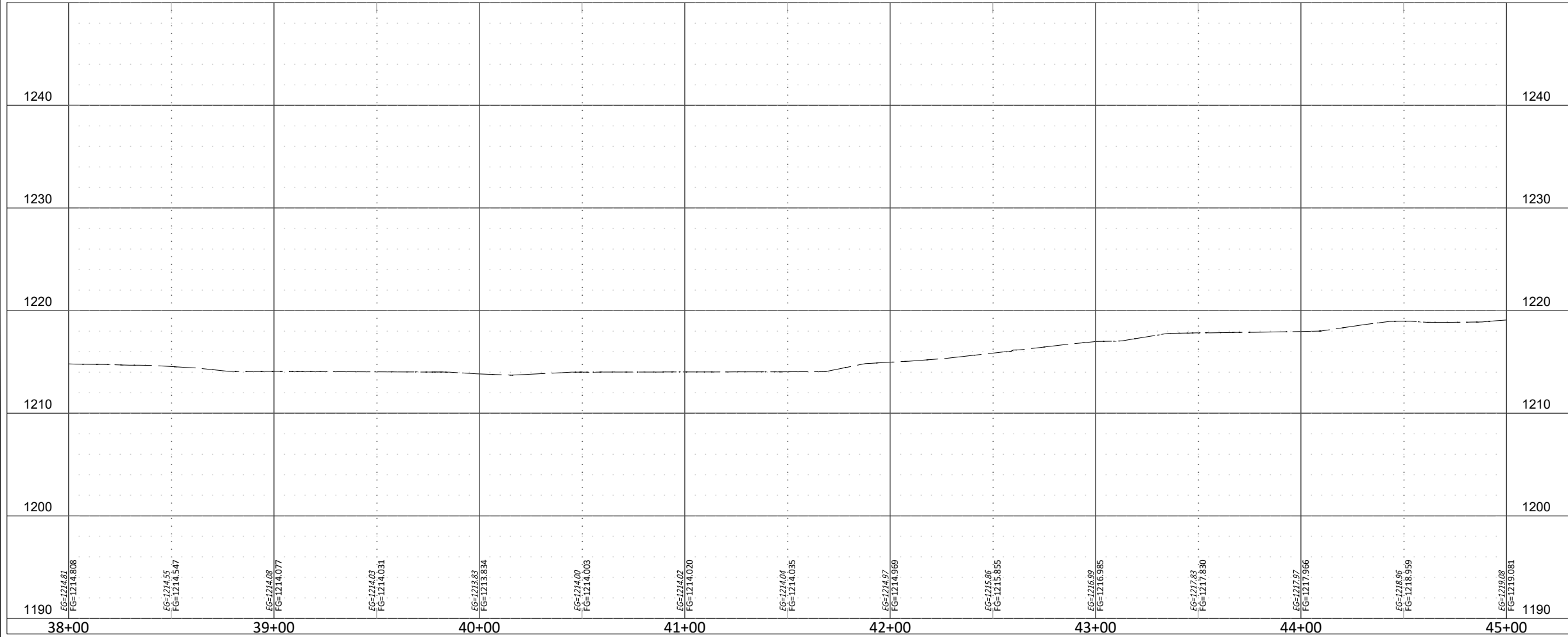
DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
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VERIFY SCALE
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SHEET NUMBER
C3-4

Project #: M4-TS011

12/14/2023 2:35 PM DERRICK BERGFELD Z:\176822 MAPS\4 SIDEWALKS PHASE 1A 1B\PHASE 1B\DRAWINGS\2C-PROF.DWG



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DATE:	12.13.23
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VERIFY SCALE
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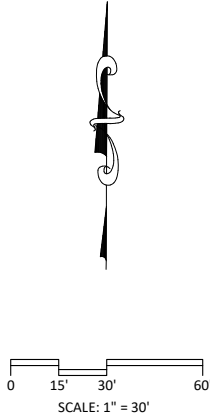
SHEET NUMBER
C3-5

Project #: M4-TS011



STA. 45+00

STA. 38+00



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (5)

DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
0 1" BAR IS ONE INCH ON ORIGINAL DRAWING. IF NOT ONE INCH ON THIS SHEET, ADJUST ACCORDINGLY.

SHEET NUMBER
C3-5

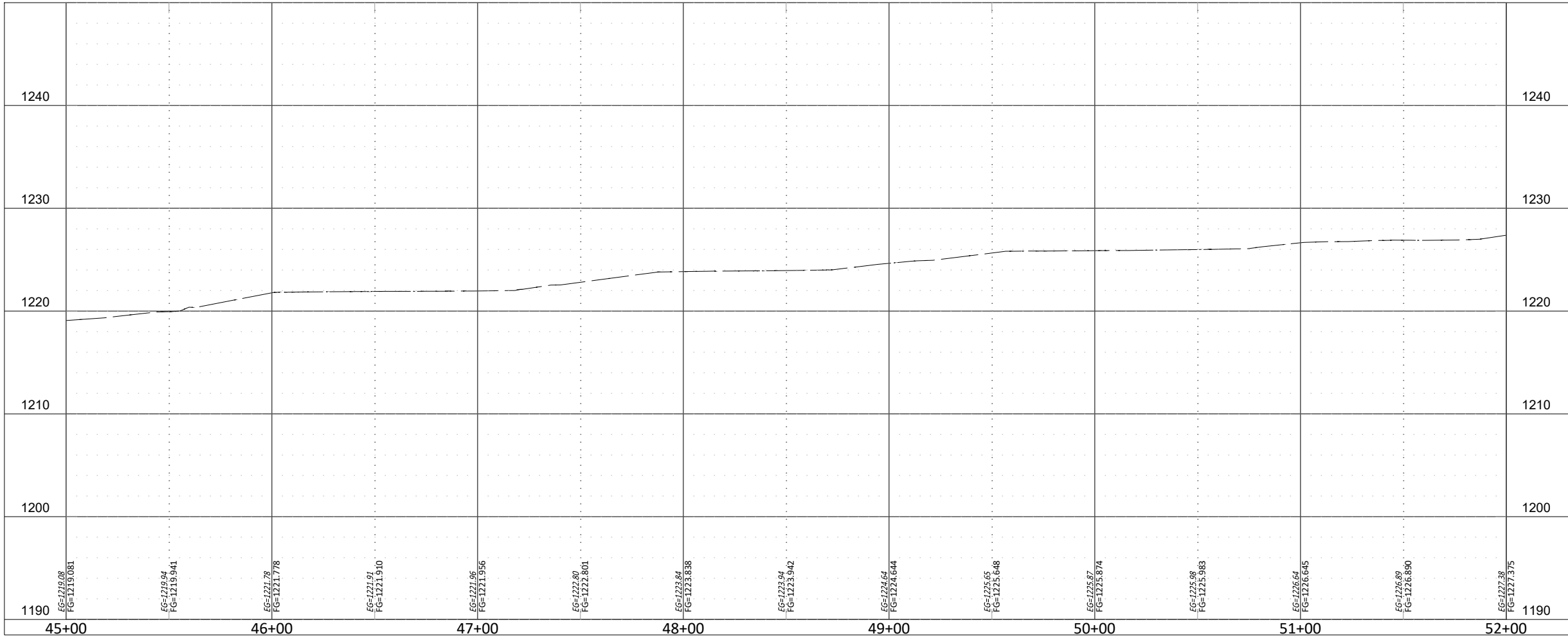
Project #: M4-TS011

ENGINEERING
SURVEYING
PLANNING

ROBERTS
SRB
CITY OF OKLAHOMA
BUDGETARY

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

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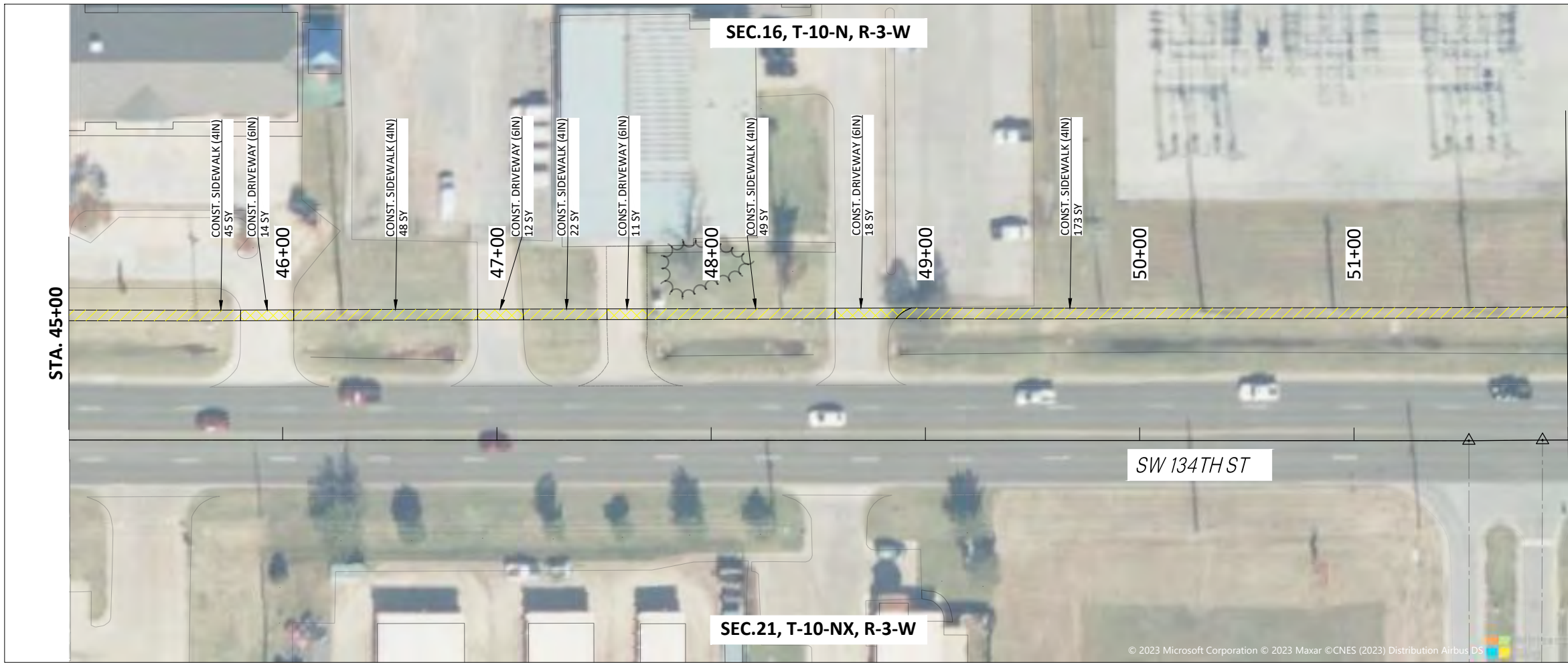
CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (6)

DATE: 12.13.23
DESIGN BY: GJW
DRAWN BY: DHB
CHECKED BY: BE

VERIFY SCALE
0 1"
BAR IS ONE INCH ON ORIGINAL
DRAWING. IF NOT ONE INCH ON
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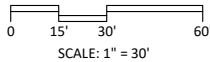
SHEET NUMBER
C3-6

Project #: M4-TS011



STA. 52+00

STA. 45+00



LEGEND

- CURB (8IN BARRIER)
- DRIVEWAY (6IN)
- SIDEWALK (4IN)
- ADA CURB RAMP (8IN)
- TACTILE WARNING DEVICE
- VARIABLE HEIGHT CURB
- RETAINING WALL (VARIABLE HEIGHT)

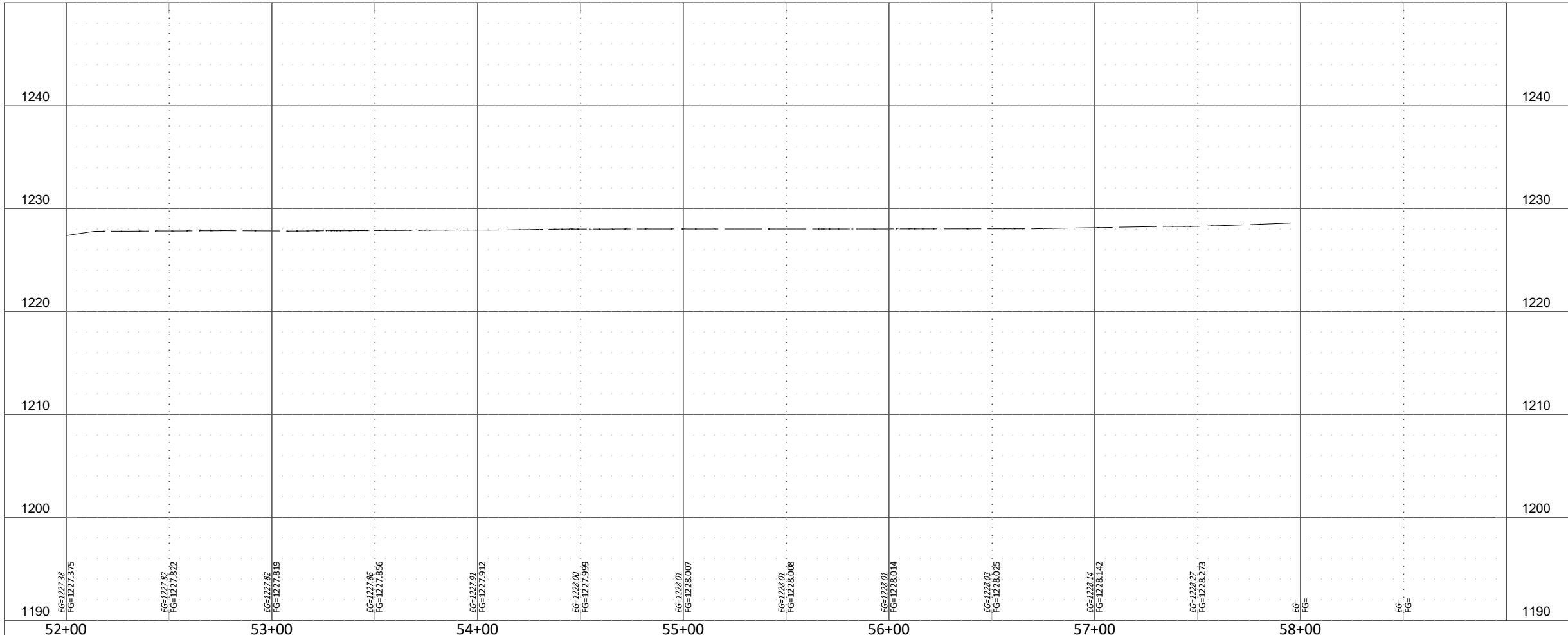
REVISIONS
DESCRIPTION

NO. DATE



CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

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GENERAL NOTES:

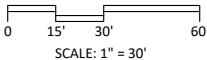
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DATE:	12.13.23
DESIGN BY:	GJW
DRAWN BY:	DHB
CHECKED BY:	BE

VERIFY SCALE
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SHEET NUMBER
C3-7
Project #: M4-TS011



LEGEND	
	CURB (8IN BARRIER)
	DRIVEWAY (6IN)
	SIDEWALK (4IN)
	ADA CURB RAMP (8IN)
	TACTILE WARNING DEVICE
	VARIABLE HEIGHT CURB
	RETAINING WALL (VARIABLE HEIGHT)

REVISIONS	
NO.	DESCRIPTION

CITY OF OKLAHOMA CITY, OKLAHOMA
MAPS 4 SIDEWALK AND RELATED AMENITIES
CIVIL - PLAN & PROFILE
134TH ST-P&P - (7)

ENGINEERING
SURVEYING
PLANNING

CERTIFICATE OF AUTHORIZATION
NO. 3949 EXPIRES JUNE 30, 2025

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