

Planning Commission Minutes
November 9, 2023

(The agenda was filed with the City Clerk of The City of Oklahoma City at 7:24 a.m. on November 6, 2023)

22. (SPUD-1561) Application by Catholic Charities Archdiocese of Oklahoma City to rezone 1101 South Youngs Blvd from R-4 General Residential, I-1 Light Industrial, and Stockyards City Transitional Development Overlay Districts to SPUD-1561 Simplified Planned Unit Development and Stockyards City Transitional Development Overlay Districts. Ward 6.

Amended Technical Evaluation:

1. ~~Revise Section II.6. Parking to require a minimum off-street parking ratio of one parking stall for every two residential dwelling units.~~
2. ~~Revise Section II. Other Development Regulations to add a new development standard for street orientation that shall read as follows: "12. Building Entries and Street Orientation. All dwelling units facing either SW 10th Street or South Youngs Boulevard shall have their front door entries located on the elevations that face those streets."~~
3. The amount of required landscaping will be doubled along SW 10th Street and South Youngs Blvd. Required landscaping may be placed on private property.
4. The existing curb cut will be removed.

The applicant was present. There were no protestors present.

RECOMMENDED APPROVAL SUBJECT TO TECHNICAL EVALUATION AS AMENDED.

MOVED BY GOVIN, SECONDED BY CLAIR

AYES: CLAIR, POWERS, MEEK, PRIVETT, NEWMAN, GOVIN, PENNINGTON, NOBLE

ABSENT: LAFORGE



STAFF REPORT
The City of Oklahoma City
Planning Commission
November 9, 2023

Item No. IV. 22.

(SPUD-1561) Application by Catholic Charities Archdiocese of Oklahoma City to rezone 1101 South Youngs Blvd from R-4 General Residential, I-1 Light Industrial, and Stockyards City Transitional Development Districts to SPUD-1561 Simplified Planned Unit Development and Stockyards City Transitional Development Districts. Ward 6.

I. GENERAL INFORMATION

A. Contacts

1. Applicant/Developer Representative

Name Mark Zitzow
 Company Johnson & Associates
 Phone 405-235-8075
 Email mzitzow@jaokc.com

B. Case History

This is a new application.

C. Reason for Request

The purpose of this application is to allow a multi-family residential development.

D. Existing Conditions

1. Comprehensive Plan Land Use Typology Area: Urban – Medium (UM)

UM applies to fully urbanized areas of the city, most of which were built prior to the 1960s. Developments are expected to be larger in scale and have greater intensity and mixture of uses than developments found in UL. Development within UM areas should support efficient transit usage and provide pedestrian and bicycle access to retail, services, parks, and other destinations. Priorities for the UM areas include “infill” developments on vacant lots, rehabilitation of underutilized property, and development that supports revitalization of distressed neighborhoods.

2. Size of Site: 0.23 acres

3. Zoning and Land Use

	Subject Site	North	East	South	West
Zoning	I-1/R-4/ SYT	I-1/R-4/ SYT	SPUD-1468/ SRODD	R-4/ SYT	I-1/ SYT
Land Use	Indus/Res	Indus/Res	Office	Residential	Industrial

II. SUMMARY OF SPUD APPLICATION

1. This site will be developed in accordance with the regulations of the **R-4, “General Residential” District** and the SYT Stockyards City Transitional Development Overlay District (OKC Zoning Ordinance, 2020, as amended), except that the following restrictions will apply:

The following use(s) will be the only use(s) permitted on this site:

- Community Recreation: Property Owners Association (8250.3)
- Low Impact Institutional: Residential-Oriented (8200.5)
- Multiple-Family Residential (8200.12) for the purpose of placing multiple single-family homes on one lot.
- Single-Family Residential (8200.14)
- Three- and Four-Family Residential (8200.15)
- Two-Family Residential (8200.16)

2. **Maximum Building Height:**

The maximum height of any building within this SPUD shall be 35 feet.

3. **Maximum Building Size:**

There shall be no maximum building size within this SPUD.

4. **Maximum Number of Buildings:**

The maximum number of buildings within this SPUD shall be six (6). Accessory structures shall not be included within the maximum building limit.

5. **Building Setback Lines:**

North (SW 10th St.): 5 feet
East (S Youngs Blvd.): 5 feet
South: 5 feet
West: 5 feet

6. **Sight-Proof Screening:**

No less than a six-foot and no greater than an eight-foot-high sight proof screen shall be required along the south and west SPUD boundaries. Said sight-proof

screen may be constructed of stucco, brick, stone, wood, steel/iron and/or any combination thereof and shall be solid and opaque.

Fences located along any street shall meet front yard fence requirements. All fencing shall be subject to the Stockyards City Transitional Development Overlay District guidelines and regulations.

7. Landscaping:

The subject site shall meet all requirements of Oklahoma City’s Landscaping Ordinance in place at the time of development and shall be subject to the guidelines and regulations of the Stockyards City Transitional Development Overlay District.

8. Signs:

Signage within this SPUD shall be per the R-4, “General Residential” District regulations and shall be subject to the guidelines and regulations of the Stockyards City Transitional Development Overlay District.

No EMD signs shall be permitted within this SPUD.

9. Access:

Access shall be permitted via one (1) drive along either SW 10th Street or S Youngs Blvd.

10. Sidewalks:

There is an existing five-foot sidewalk along SW 10th St. Should said sidewalk be damaged or removed during construction the developer shall be required to make necessary repairs and/or replace if necessary.

Five-foot sidewalks shall be constructed on S Youngs Blvd. or 6-foot sidewalks shall be required if the sidewalk is constructed adjacent to the curb, subject to the policies and procedures of the Public Works Department. All sidewalks shall be in place prior to any Certificates of Occupancy being issued.

II. OTHER DEVELOPMENT REGULATIONS:

1. Architecture:

All structures constructed within this SPUD shall be subject to review and

approval of a Certificate of Approval according to the Stockyards City Transitional Development Overlay District regulations and guidelines.

However, the proposed structures shall be permitted to be prefabricated buildings and installed on the property.

2. Open Space:

N/A

3. Street Improvements:

N/A

4. Site Lighting:

All site lighting utilized within this SPUD shall be directed away from any adjacent properties. To accomplish this, lights shall utilize shields, shades or other appropriate methods of directing light beams. The site lighting in this SPUD shall be in accordance with Chapter 59, Article XII, Section 59-12350 of the Oklahoma City Municipal Code, 2020, as amended.

5. Dumpsters:

Dumpsters shall not be required within this SPUD. This SPUD may be served by private trash service through the use of roll-out bins. Location and screening of the trash containers shall be subject to the guidelines and regulations of the Stockyards City Transitional Development Overlay District regulations and guidelines.

6. Parking:

The design of all parking facilities in this SPUD shall be in accordance with Section 59, Article X of the Oklahoma City Municipal Code, 2020, as amended.

No Parking shall be required within this SPUD. On-street parking may be sought for this development and shall be designed and approved in conformance with all application regulations, guidelines and procedures.

7. Maintenance: N/A

8. Drainage: Development of this parcel will comply with Chapter 16 of the Oklahoma City Municipal Code, 2020, as amended.

9. Platting:

All land within this SPUD shall be contained within a Final Plat or subdivided via administrative lots splits if minimum requirements are met and any plat dedications shall be approved by the City Council prior to any occupancy permits being issued in the SPUD. However, platting shall not be required if not developed as fee simple lots.

Lot lines may be adjusted to realign the location of homes prior to the platting stage.

11. Other:

Maximum Number of Dwelling Units: The maximum number of dwelling units permitted within this SPUD shall be 12 units.

III. SUPPORTING DOCUMENTS

- Exhibit A: Legal Description
- Exhibit B: Conceptual Site Plan
- Exhibit C: Conceptual Renderings

III. REVIEW COMMENTS

This application was submitted to the following agencies, departments, and/or divisions for review and comment. An asterisk indicates that the agency, department, and/or division responded with no adverse comments.

A. Outside Agencies

1. Oklahoma City-County Health Department

2. Oklahoma City Urban Renewal Authority (OCURA)

3. Oklahoma Gas and Electric (OGE)

4. Oklahoma Natural Gas: Please be advised ONG has an underground or aboveground facility in the area. If you need our facilities relocated to accommodate your excavation or construction, ONG will provide you a cost estimate. Prior to excavation, please contact OKIE-ONE 1-800-522-6543 a minimum of 48 hours prior for exact location of our facilities.

5. Oklahoma Water Resources Board (OWRB)

6. School District(s) Oklahoma City

7. Oklahoma Department of Transportation (ODOT)

B. City Departments

1. Airports

2. Central Oklahoma Transportation and Parking Authority (COTPA)

3. Fire*

4. Information Technology/Geographic Support

5. Parks and Recreation

6. Police

7. Public Works

a. Engineering

b. Streets, Traffic and Drainage Maintenance

Storm Sewer Availability

- 1) The Public Works Drainage Division staff has reviewed the subject application. All development, new construction, and/or substantial improvements planned within the proposed area shall be subject to chapter 16 of the Oklahoma City Municipal Code.
- 2) Storm sewers in accordance with the City's Drainage Ordinance will be required. Development abutting section line roads with drainage flows that exceed the capacity of OKC standard ditch detail (D-100) will necessitate the installation of enclosed storm sewer and/or concrete channel. These improvements shall be placed to provide a minimum of 35' clearance distance from the centerline of the section. Concrete channels must be entirely outside public right-of-way.
- 3) A flood and/or drainage study will be required to establish finished floor elevations, common lot areas or private drainage easements. A final plat should not be submitted until the study has been reviewed and approved by the Public Works Department.
- 4) Plans for drainage improvements within the private drainage reserves and/or common areas must be submitted for review, and payment of inspection fees shall be made prior to construction. Building permits will not be issued until construction is complete. If a subdivision abuts a stream, the private drainage easements and/or common area along stream shall extend to flow line of stream at a minimum.
- 5) Place the following note on the plat and construction plans: Maintenance of the Common Areas and/or Private Drainage Easements shall be the responsibility of the property owner. No structures, storage of material, grading, fill or other

obstructions, including fences, either temporary or permanent shall be placed within the common areas and/or drainage easements shown.

- 6) Drainage easements shall be clearly denoted as public or private in owner's dedication, on the plat, and/or in the plat notes.
- 7) Construction within the limits of this SPUD will require an erosion control plan in accordance with EPA Storm Water Discharge permitting: CFR Vol. 57, No. 175, September 9, 1992. A copy of the EPA Notice of Intent (NOI) will be required prior to the issuance of work orders or permits for construction activities disturbing an area of 1/2 acre or greater.
- 8) Sidewalk shall be installed for all new construction and/or at the time of a residential use to a more intense use on all lots having frontage on public streets classified as a major or minor arterial.
- 9) All private roads /streets will have private storm sewer systems.
- 10) Engineer/developers will be contacting PW for detention determination before they submit their final plat and plans.
- 11) Add to the end of Section I.10 Sidewalks: All sidewalk shall be provided in accordance with the City of Oklahoma City Municipal Code and ADA requirements in place at the time of construction of this SPUD.

c. Stormwater Quality Management

d. Traffic Management*

8. Utilities

a. Engineering

Paving

Wastewater Availability

- 1) An existing 8" wastewater main(s) is within the required distance to the subject site and will be required to be extended to the furthest property line for any proposed development.
- 2) Line capacity is not guarantee and must be checked by the Engineer of Record and reviewed by the City.
- 3) The developer will be required to extend the City wastewater collection system to and across each lot or site within the subdivision in accordance with the City Standard Specifications and Private Development Design Manual.
- 4) Minimum ten (10) feet horizontal separation is required from water mains and five (5) feet for other utilities. Minimum two (2) feet vertical separation required for all crossings. All wastewater mains must have a minimum cover of four (4)

feet and depth no greater than eighteen (18) feet. Developer is responsible for remediating any failure to maintain the minimum four (4) feet of cover.

- 5) Wastewater main(s) must be centrally located in a twenty (20) feet wide easement or larger if necessary. No trees, signs, dumpster, fence, and/or structures shall be permitted over any proposed or existing utility main. An approved revocable permit must be obtained to have any private improvement located within any utility easement and/or right-of-way.
- 6) All existing unused private wastewater service connections must be abandoned and capped at the main in accordance with City Standard Specifications and Private Development Design Manual.
- 7) A wastewater extension is required for service.

b. Solid Waste Management

- 1) The City can provide service for up to 3 units on a single water meter. Any more than 3 units on the same meter will have to be serviced by a private hauler. A dumpster could be utilized provided there is not enough frontage space for cart placement.

a. Water/Wastewater Quality

Water Availability

- 1) An existing 6" water main(s) is within the required distance to the subject site and the developer will be required to extend the water system to each lot or site within the development in accordance with City Standard Specifications and Private Development Design Manual.
- 2) Minimum ten (10) feet horizontal separation required from wastewater mains and five (5) feet for other utilities. Minimum two (2) foot vertical separation required for all utility crossings. All water mains must have a minimum cover of five (5) feet along section line roads and four (4) feet elsewhere.
- 3) Proposed and/or existing water mains must be located centrally within a twenty (20) feet wide or larger utility easement or right-of-way. No trees, signs, dumpster, fence, and/or structures shall be permitted over any proposed or existing utility main. An approved revocable permit must be obtained to have any private improvement located within any utility easement and/or right-of-way.
- 4) Dead-end water mains must be avoided when possible. All existing unused water services must be capped and abandoned at the water main in accordance with the City Standard Specifications.
- 5) In accordance with ODEQ regulations, the City provides water at a minimum pressure of 25 psi. The developer is responsible for designing and construction of all fixtures to provide adequate domestic and fire protection under minimum pressure conditions. The developer will be responsible for any failure of domestic

or fire protection systems which require water pressure more than 25 psi. The developer is responsible for installing pressure reducers if necessary for protection of developer's service lines, plumbing, and fixtures.

- 6) All existing and/or proposed water meters must be located in the right-of-way and/or utility easement within grassy areas outside of sidewalks, driveways, streets, and paving.
- 7) All domestic and fire suppression services must have a separate water service connection. Fire hydrant spacing shall not exceed 500 feet and must be located within ten (10) of a hard surface (i.e. sidewalk, street, paving).
- 8) Supply capacity for development is not guaranteed and will be reviewed at time of review of construction documents.
- 9) If this is not platted then this will be mastered metered and will be considered private meaning each structure will not be separately metered. Dumpster location will need to be provided or a private hauler will need to agree to provide poly carts. The City will not serve if not platted.

9. Planning

a. Comprehensive Plan Considerations

1) LUTA Development Policies:

Site Design:

- Maintain historical lot and block sizes where possible and appropriate.
- Utilize Best Management Practices (BMP) for stormwater.

Location:

- Mixture of density, lot size and building scale is appropriate as long as land use compatibility is achieved.

Density: The Urban Medium Intensity LUTA outlines a density range of 10 to 40 dwelling units per acre.

National, state, and local permitting require basic best management practices for stormwater management. The SPUD is requested to for a multifamily residential development. The proposed 12 dwelling units over 0.23-acre would be 52 du/acre. The SPUD is within a design district and retains the requirements for design review by the Stockyards City Urban Design Committee.

Automobile Connectivity:

- Maintain and enhance the connectivity of the street network.

The subject site currently has one improved curb cut along SW 10th Street. The SPUD regulations allow one drive along either SW 10th Street or S Youngs Boulevard.

Pedestrian Connectivity:

- Provide sidewalk connections to nearby uses.

Sidewalks are currently available along SW 10th Street and will be required by the SPUD regulations along both SW 10th Street and S Youngs Boulevard.

- 2) Compatibility:** The comprehensive plan includes a land use compatibility matrix used to identify potential conflicts between major land use categories. *In this case, a portion of the site is already zoned R-4, which allows multifamily residential adjacent to a single-family home. The site is also adjacent to industrial zoning on the west. The comprehensive plan states that in some cases, a proposed project may need to take measures to reduce the impact of an existing use, and specifically identifies placing residential uses next to an industrial use as an example. The SPUD site is within and would remain subject to the design review regulations, guidelines, and requirement for a Certificate of Approval for changes to the site.*
- 3) Service Efficiency:**
 - Water: *Served*
 - Sewer: *Fully Served*
 - Fire Service: *Urban Service Level*
- 4) Environmentally Sensitive Areas:** The following apply to the proposed development site:
 - Riparian Areas: *N/A*
 - Upland Forests: *N/A*
 - Vulnerable Aquifers: The aquifer in this area is considered highly vulnerable to depletion. The comprehensive plan recommends preserving natural open spaces and utilizing low impact development techniques whenever possible in new developments in order to maintain water quality and allow for the infiltration of stormwater onsite.
- 5) Transportation System:** This site is located off S Youngs Boulevard and SW 10th Street, both Neighborhood Streets in the Urban Medium LUTA. The nearest transit (bus) service is located south of the subject site, along Exchange Avenue.
- 6) Other Development Related Policies**
 - Encourage the integration and mixing of land uses in urban areas. (SU-1)
 - Support diversity and integration of housing unit types and sizes in all land use typology areas in order to meet the diverse needs of households of different sizes, generational needs, incomes, and preferences. (SU-4)
 - Encourage redevelopment and infill development on vacant, underutilized, and brownfield sites in urbanized areas. (SU-19)
 - Require sidewalks on both sides of all streets in urban LUTAs. (C-7)
 - Improve the functionality and efficiency of the street network by:

- Providing direct connections from residential developments to nearby places and to each other.
- Providing street and sidewalk stubs to adjacent vacant land in anticipation of future development.
- Connecting new development to existing street and sidewalk stubs, and to existing trail, open space, and bicycle networks.
- Reducing block sizes and use of dead-end streets.
- Maintaining the existing street grid to preserve connectivity and mobility options. (C-11)
- Support and incentivize the adaptive use of existing buildings, infill development, and brownfield development. (G-10)
- Preserve mature, healthy trees and incorporate them into the design of new development or redevelopment projects to the greatest extent possible. (G-26)
- Encourage unified planning for all adjoining land owned or controlled by a project's developer to ensure proper circulation and land use relationships (SU-47)

b. Plan Conformance Considerations

The subject site is located at the southwest corner of S Youngs Boulevard and SW 10th Street, in an area generally located east of S Agnew Avenue and north of Exchange Avenue. The site is undeveloped and zoned I-1 and R-4. The site and land west of S Youngs Blvd are within the Stockyards City Transitional Development Overlay District (SYT), and land east of S Youngs Blvd is within the Scenic River Overlay Design District (SRODD). Across SW 10th Street to the north is a fenced vacant lot and adjacent to the west of it is another fenced lot used for outdoor storage. To the east, across S Youngs Blvd, is a campus for the Catholic Charities that is currently under development pursuant to SPUD-1468 and the Riverfront Design Committee approval of SRCA-23-00016. To the south is a single-family residence. Adjacent on the west is a one-story metal building that houses a diesel truck repair business.

The SPUD is requested to build new multifamily housing. The SPUD proposes a modified R-4 base and maintains the SYT regulations, guidelines, and design review process. The SPUD eliminates parking requirements and modifies setbacks to allow up to 12 dwellings in a maximum of 6 buildings. The proposal to increase density in the Urban Medium LUTA is over the UM density range, but in conformance with comprehensive plan goals to provide a mixture of residential housing types and infill development on vacant lots. The SPUD applies front yard fencing requirements (4-foot max height, cannot be sight-proof) to both street frontages.

This SPUD application is subject to review and recommendation by the Stockyards City Urban Design Committee. The case will be heard after the completion of this report, as the Design Committee will meet to discuss the matter on November 8, 2023. The staff report is attached, and the Urban Design Staff's recommendations are included as Technical Evaluations. The Design Committee's recommendation will be provided to the Planning Commission at their meeting.

IV. STAFF RECOMMENDATION

Staff recommendations are advisory only and do not constitute Planning Commission decisions. Staff recommendations are based on a technical evaluation of information submitted at the time of review. Determination of conformance with policies contained in the comprehensive plan is the purview of the Planning Commission. Planning Commission decisions require a vote of five members to approve or deny an item. The Planning Commission may consider in its decision-making process any additional relevant information presented at the public hearing by the public, applicant, and/or City departments.

Approval of the application subject to the following Technical Evaluations:

1. Revise Section II.6. Parking to require a minimum off-street parking ratio of one parking stall for every two residential dwelling units.
2. Revise Section II. Other Development Regulations to add a new development standard for street orientation that shall read as follows: "12. Building Entries and Street Orientation. All dwelling units facing either SW 10th St or S Youngs Blvd shall have their front door entries located on the elevations that face those streets."

All Engineering Division and Water/Wastewater Utilities Division requirements must be met. Additional changes to the SPUD may be required during either Divisions review of construction plans and prior to City Council approval.

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STAFF REPORT

Stockyards City Urban Design Committee

November 8, 2023

Agenda Item VII.C.
Case No. SPUD-1561
Property Address 1101 S Youngs Blvd
Applicant Name Mark Zitzow, Johnson & Associates for Catholic Charities Archdiocese of Oklahoma City
1 E Sheridan Ave, Suite 200
Oklahoma City, OK 73104

District Stockyards City Transitional Development Overlay District

A. ITEMS FOR CONSIDERATION

1. Recommendation

To provide a recommendation to the Planning Commission for a request to rezone from 1-1 Light Industrial and R-4 General Residential to Simplified Planned Unit Development (SPUD-1561).

B. BACKGROUND

1. Location

The subject site is located at the southwest corner of S Youngs Blvd and SW 10th St.

2. Site History/Existing Conditions

The subject site is a vacant lot.

3. Surrounding Environment

To the west, there is a one-story metal building that houses a diesel truck repair business. To the north, across SW 10th St, there is a fenced vacant lot at the northwest corner S Youngs Blvd and SW 10th St and adjacent to the west of that lot there is another fenced lot that is used for outdoor storage.

To the south, there is a single-story manufactured home that is used as a single-family residence. To the east, across S Youngs Blvd, there is a campus for the Catholic Charities that is currently under development pursuant to SPUD-1468 and the Riverfront Design Committee approval of SRCA-23-00016.

4. Intended Uses

The Master Design Statement (MDS) for this SPUD list a variety of potential residential uses as follows:

- 8200.12, Multiple-Family Residential (for the purpose of placing multiple single family homes on one lot)
- 8200.14, Single-Family Residential

8200.16, Two-Family Residential

The MDS also list other assorted potential uses such as:

8250.3, Community Recreation: Property Owners Association
8200.5, Low-Impact Institutional: Residential-Oriented

It should be noted that this residential project is associated with the Catholic Charities Archdiocese of Oklahoma City and the expressed intent for this project is to provide housing options for the homeless. However, there are no restrictions within the SPUD document that would require it to be developed for, or to remain in service of, that need in perpetuity. Based on the uses listed as permitted in the Master Design Statement (or MDS), there is always the possibility that this project could ultimately be developed as, or could eventually transition to, a standard multi-family residential development.

5. SPUD Document Overview

Per Section 59-4250.7.D.(1) “The Stockyards City Urban Design Committee shall have the opportunity to comment upon and make recommendations on actions undertaken by other City boards, committees, and commissions with respect to the effect of such actions upon the District.” For this reason, staff has placed this item on the agenda to allow the Committee to offer a recommendation on the Simplified Planned Unit Development (SPUD) application to the Planning Commission.

A SPUD application is an effort to rezone a property. Typically, the reason a SPUD is applied for, rather than just rezoning to a standard zoning classification (like I-1 or R-4), is because there is something about the proposed development that doesn't quite work (like a setback of building height) or isn't a permitted use in the standard zoning.

In these instances, a SPUD is proposed as a customized version of the zoning for the site that is specifically geared to what the project proposes. If ultimately adopted by the City Council, this SPUD document would become the applicable zoning for the site. When the Stockyards City Urban Design Committee considers the subsequent Certificate of Approval application (that would still be required for the proposed development), the regulations and guidelines contained in the Master Design Statement (or MDS) of the SPUD would be what the Committee uses to evaluate the project. Fortunately, the MDS document has incorporated many of the SYT standards (including sight-proof screening, landscaping, signage, architecture, and dumpsters). But there are other standards contained in the MDS that are either new or different than the SYT standards. This is why it is important that the Committee is comfortable with the standards being set by the SPUD for this development.

Staff has coordinated with the applicant on the content and direction of the SPUD document. The attached SPUD document is a final draft that reflects the revisions that have been agreed on. This draft represents the version of the SPUD document that the Committee is making a recommendation on to the Planning

Commission. Staff and the applicant have not agreed on all elements of the proposed SPUD and the issues that remain outstanding are discussed in detail in this report. The issues to be discussed will be framed in light of what the provisions of the SYT Code requires contrasted against what the applicant is asking for in the MDS of the SPUD.

Perhaps most significant for Stockyards City Urban Design Committee's review, the MDS for the proposed SPUD will maintain the design review requirements of the Stockyards City Transitional Development Overlay District. Accordingly, any and all exterior modifications at the subject site will remain under the purview of the Stockyards City Urban Design Committee and subject to the applicable Certificate of Approval provisions. Certificate of Approval applications for future development within the SPUD area will be subject to administrative or Committee review and approval pursuant to the standard SYT thresholds for those reviews.

C. ITEMS IN COMPLIANCE

1. Unless noted below in Section D. Issues and Considerations, all other case items of this proposal comply with the Development Regulations and Guidelines of the Stockyard City Transitional Development Overlay District zoning ordinance as referenced below:
 - a. **RE:** §13550.2.C., District Use Regulations
 - b. **RE:** §13550.3.B.(1)., Development Regulations, Block Pattern
 - c. **RE:** §13550.3.C.(2), Development Guidelines, Block Pattern
 - d. **RE:** §13550.3.D.(2)(a), Built Form Regulations, Materials
 - e. **RE:** §13550.3.D.(2)(b), Built Form Regulations, Building Design
 - f. **RE:** §13550.3.D.(3)(a), Built Form Guidelines, Materials
 - g. **RE:** §13550.3.D.(3)(b), Built Form Guidelines, Building Massing
 - h. **RE:** §13550.3.D.(3)(c), Built Form Guidelines, Building Design
 - i. **RE:** §13550.3.D.(4)(b), Circulation and Access Regulations
 - j. **RE:** §13550.3.D.(6)(b), Landscaping and Screening Regulations
 - k. **RE:** §13550.3.D.(6)(c), Landscaping and Screening Guidelines
 - l. **RE:** §13550.3.D.(7)(b), Fencing and Wall Regulations
 - m. **RE:** §13550.3.D.(7)(c), Fencing and Wall Guidelines
 - n. **RE:** §13550.3.D.(8)(b), Signage Regulations
 - o. **RE:** §13550.3.D.(8)(c), Signage Guidelines
 - p. **RE:** §13550.3.D.(9)(b), Lighting Regulations
 - q. **RE:** §13550.3.D.(9)(c), Lighting Guidelines

Note: All other Regulations and Guidelines of the Stockyards City Transitional Development Overlay zoning ordinance not listed above are either not applicable

to this case or apply to elements of this project to be considered as a separate case at a later date.

D. ISSUES AND CONSIDERATIONS

1. Development Regulations

This proposal does not comply with the Development Regulations of the Urban Design Overlay District zoning ordinance as follows:

a. RE: Permitted Land Uses and Land Use Compatibility

- 1) Description: The MDS includes Single-Family, Two-Family residential, Three- and Four-Family Residential and Multiple-Family Residential uses as permitted uses.
- 2) Reference: §59-6250.2., District Use Regulations
Table 6250.1, which lists out the various permitted and conditional uses allowed in the base I-1 zoning district does not include any residential uses.
- 3) Considerations: While the southern half of the subject site is zoned R-4 General Residential, the northern half of the site is zoned I-1, Light Industrial. This is likely one of the main reasons behind the need for the proposed SPUD.

One concern with adding housing to the development mix on the west side of S Youngs Blvd is that this area is predominantly comprised of various light industrial uses. Residential uses are not permitted in industrial zones for a reason. The primary reason that residential uses are not included in industrial zones is that there is normally a land use compatibility issue. Many industrial uses come complete with noise, odors, vibrations, truck traffic and other impacts that might be considered incompatible with residential uses.

These factors of potential incompatibility can make living in or near industrial areas less than desirable. Often times, many disadvantaged segments of the population can end up housed in areas that become impacted by these detrimental influences. This can in turn lead to concerns regarding social equity and environmental justice. From this perspective, staff believes that sites in close proximity to industrial uses are generally not ideal for residential use.

However, because the proposed housing is associated with the Catholic Charities Archdiocese of Oklahoma City (as project the applicant) and because they have a facility located directly across the street (on the east side of S Youngs Ave) from the subject site, there are advantages to the residential use of this site. This facility, once completed, will offer various goods and services and essentially function as a women's sanctuary.

There is also a residential neighborhood on the east side of S Youngs Blvd encompassing the blocks moving north up from SW 10th St to SW 8th St. Having a residential neighborhood nearby provides greater land use compatibility for the proposed project.

While the presence of industrial uses all around the subject site may not be ideal, this may be at least partially offset by the nearby presence of services that will be available at the Catholic Charities facility directly across the street and the adjacent residential development to the east.

b. RE: Conceptual Site Plan (Exhibit B) of the MDS

1) Description: The MDS includes a Conceptual Site Plan (Exhibit B) that shows the development of residences with no street orientation.

2) Reference: §59-13550.3.A.(1)., Intent

“(c) To maintain the physical continuity of the street frontage.

(d) To ensure development within the SYT maintains a strong visual and physical relationship to adjacent streets, the River, and its tributary waterways, and neighboring development through the use of reduced setbacks and other design elements.”

Reference: §59-13550.3.D., Built Form

“(3)(c)2., Buildings should be oriented towards the primary street frontage, the River, and/or a tributary waterway so that entrances are visible and accessible to pedestrians from adjacent sidewalks or trails.”

3) Considerations: The residential structures shown on the Conceptual Site Plan (Exhibit B to the MDS) are oriented toward a proposed interior courtyard at the center of the site. The dwelling units shown on the north side of the site have their rear elevation facing SW 10th St and the dwelling units shown on the east side of the site have their side elevations facing S Youngs Blvd.

This design concept is a significant departure from the regulations and guidelines that would apply to most development within the SYT Overlay. The residential dwellings that line SW 10th St from S Youngs all the way east over to S Pennsylvania Ave are all oriented with their front doors facing SW 10th St. Therefore, what has been proposed in this SPUD is a substantial deviation from the established neighborhood pattern of residential development within the vicinity of this site.

While there may be a few non-residential buildings in the district that have a side-on condition in their orientation to the street, these buildings were likely built prior to the establishment of the SYT Overlay. Because these are typically larger buildings, on corner lots it is not unusual to have a side-on condition for one of

the street frontages. However, there are no examples of even a non-residential structure having a rear-on condition at a street frontage. The proposed orientation of the buildings on this site represents a significant departure from the established character of development in Stockyards and the development pattern established by the homes situated along the north side of SW 10th St directly east of S Youngs Blvd.

While the potential benefits of an interior courtyard for a project like this are recognized, staff is not convinced that the internal courtyard could not remain without the homes being revised in their orientation toward the adjacent streets. Staff is also concerned about the precedent that would be set by having the first structure built in the SYT with their rear elevations facing the street. Aside from the potentially negative aesthetic considerations, there is also the concern over what in essence is a development that turns its back toward the streets, closing off these buildings, and their residents, from the Stockyards community.

To avoid this, staff believes that a new development standard should be included in the MDS that reads as follows:

“12. Building Entries and Street Orientation

All dwelling units abutting either SW 10th St or S Youngs Blvd shall have their front door entries located on the elevations that face those streets.”

Accordingly, in order to offer a recommendation of approval of this SPUD to the Planning Commission, staff would recommend the inclusion of a condition of approval that requires the MDS to be modified to add this new development regulation concerning street orientation.

c. **RE: MDS Parking Standards**

1) Description: The MDS proposes that no on-site parking be required for this development.

2) Reference: Master Design Statement, Section II.6., Parking
“The design of all parking facilities in this SPUD shall be in accordance with Section 59, Article X of the Oklahoma City Municipal Code, 2020, as amended.

No parking shall be required within this SPUD. On-street parking maybe sought for this development and shall be designed and approved in conformance with all application regulations, guidelines, and procedures.”

Reference: §59-10600.3., Off-Street Parking Exemptions

“The erection, expansion, or use of any principal building or secondary structure located in the following districts shall not be required to provide minimum off-street parking:

- A. Fringe Parking Overlay District.*
- B. NB Neighborhood Business District.*
- C. DBD Downtown Business District.*
- D. DTD-1 Downtown Transitional District Limited.*
- E. DTD-2 Downtown Transitional District General.*
- F. C-CBD Central Business District.*
- G. NC Neighborhood Conservation District, Tract 5, as defined in Appendix A of this chapter.*
- H. UD Urban Design Overlay, except west of N Classen Blvd and in the Lincoln Boulevard Corridor Subdistrict.*
- I. BC Bricktown Core Development District.*
- J. The Farmers Market District of the Scenic River Overlay District (SRODD).”*

- 3) Considerations: Whether a development does or does not have adequate parking typically is not a primary concern of Oklahoma City’s design review bodies. However, the Committee’s responsibility and area of interest and expertise is not only the architectural qualities of particular buildings but the overall impact of development and revitalization on the character of the Stockyards District. The construction of parking, or a lack of parking where needed, may impact the character of a property, block, or district.

Per Section 59-10600.3., Off-Street Parking Exemptions, the SYT Overlay District is not specifically exempt from satisfying all the applicable off-street parking requirements. Therefore, all properties in the SYT Overlay District are subject to the standard off-street parking requirements.

Under these standards, a project containing multiple-family dwelling units, if it were built in accordance with the Code, would be required to provide 1.5 parking stalls per dwelling unit. With 12 proposed dwelling units in this proposed project, a minimum of 18 off-street parking stalls would be required (presuming that all 12 units are efficiency or one-bedroom units). The proposed SPUD is requesting to go from the minimum of 18 required off-street parking stalls to 0 required parking stalls.

Staff acknowledges that, given the projected tenant demographic for these dwelling units, it could be very likely that residents of this project would not have a car. However, tenants, guests, service providers, or others may have cars. If this development were to

ultimately become a standard multi-family residential development, there would be an even greater potential that all or most residents would have vehicles. While on-street parking is permitted on SW 10th Street and on S Youngs Blvd, the streets are narrow at 22 feet or less, and may not be adequate to accommodate a significant amount of on-street parking and vehicular traffic safely.

Due to the anticipated use of the property, staff supports a reduction in the required parking but believes that requiring no parking may be problematic for the site and surrounding district. Staff recommends that the MDS be revised to include a minimum parking requirement of one stall for every two dwelling units.

One way that off-street parking requirements can be, and often are, reduced is by the provision of bicycle parking spaces or bicycle racks. Projected tenants may not own a car but could likely own a bicycle; the inclusion of bicycle parking facilities may be a benefit to this project. SPUDs do not typically include requirements specifically for bicycle parking infrastructure and the applicant has not addressed this subject at this time; any automobile or bicycle parking facilities would be reviewed as part of a Certificate of Approval, if proposed.

Accordingly, in order to offer a recommendation of approval of this SPUD to the Planning Commission, staff would recommend the inclusion of a condition of approval that requires the MDS to be modified to include this minimum off-street parking ratio. If parking is required, this may impact the ability of the applicant to develop as many dwelling units as they have proposed, which would in turn reduce the amount of parking necessary at this ratio.

E. STAFF RECOMMENDATION

1. Recommend **approval** of the application to the Planning Commission for a request to rezone from I-1 Light Industrial and R-4 General Residential to Simplified Planned Unit Development (SPUD-1561) on the basis that the project meets the Regulations and Guidelines of the Stockyards City Transitional Development Overlay District zoning ordinance as referenced in Section C and D of the Staff Report with the **conditions** that:
 - a. the applicant shall revise Section II.6., Parking of the MDS to require a minimum off-street parking ratio of one parking stall for every two residential dwelling units.
 - b. the applicant shall revise Section II., Other Development Regulations to add a new development standard for street orientation that shall read as follows:

“12. Building Entries and Street Orientation

All dwelling units facing either SW 10th St or S Youngs Blvd shall have their front door entries located on the elevations that face those streets.”

Note: Staff recommendations do not constitute Commission decisions.

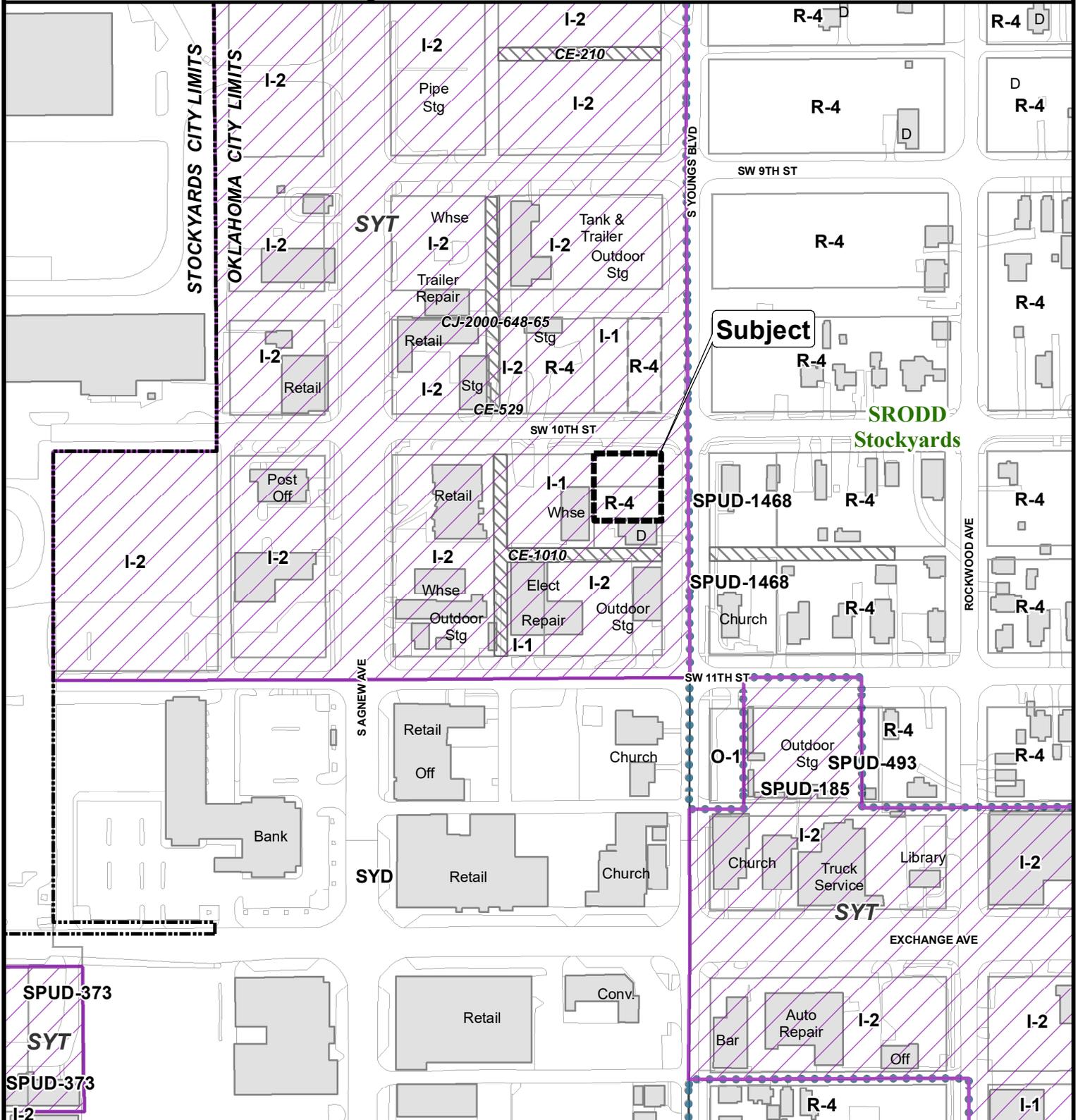
Attachments: Zoning Map, Application, Design Statement, Drawings, Photos.

Case No: SPUD-1561

Applicant: Catholic Charities of The Archdiocese of Oklahoma City

Existing Zoning: I-1 / R-4 / SYT

Location: 1101 S. Youngs Blvd.



Note: "Subject" is located approximately 1,537' West of S. Pennsylvania Ave. and 1,765' North of SW 15th St.

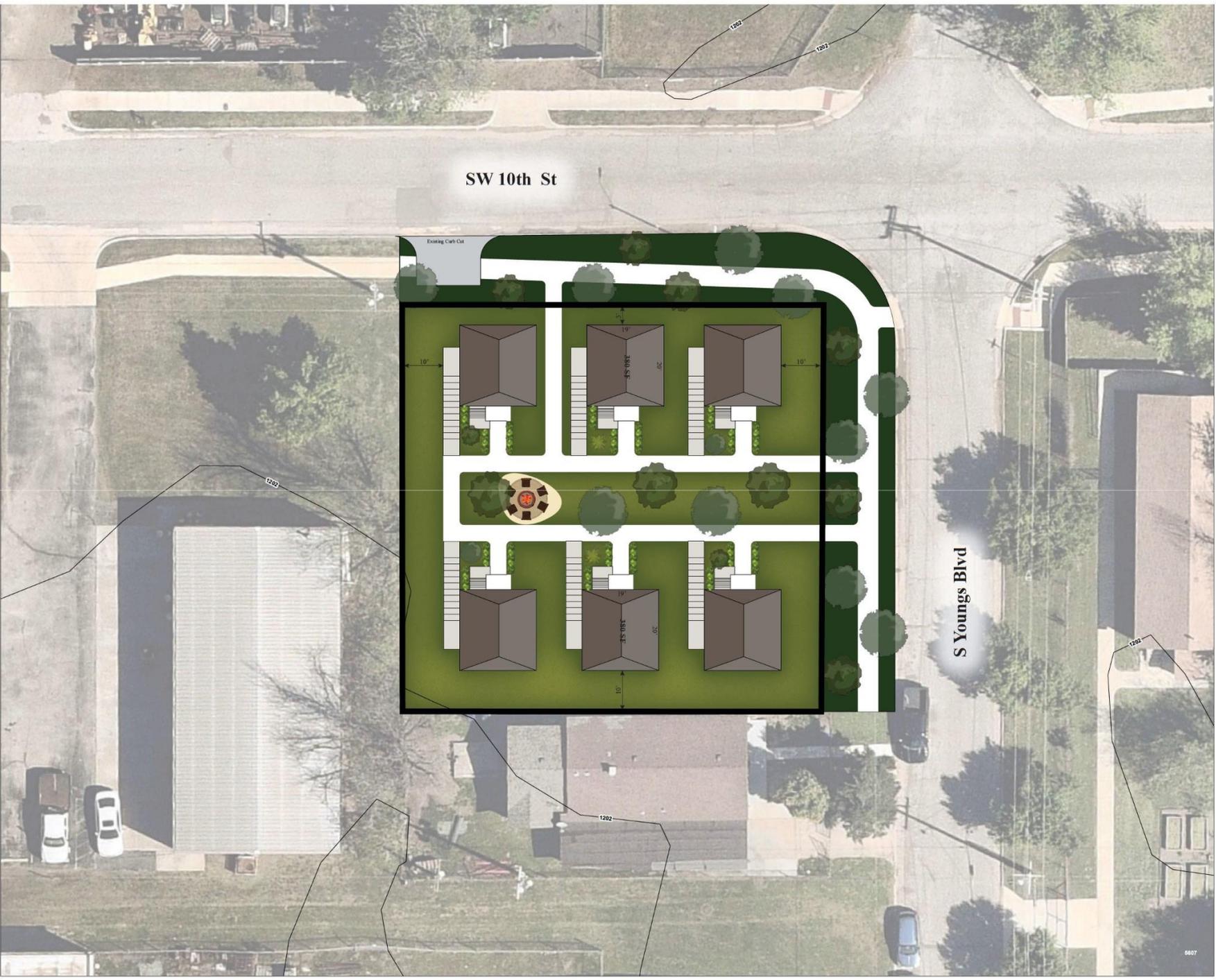
Simplified Planned Unit Development



The City of OKLAHOMA CITY



0 100 200 Feet



**SPUD-1561
1101 S Youngs Blvd.**

Exhibit B
Conceptual Site Plan

+/- .23 Acres



Johnson & Associates
1 E. Sheridan Ave., Suite 200
Oklahoma City, OK 73104
(405) 235-8075 FAX (405) 235-8075

ENGINEERS SURVEYORS PLANNERS

8/14/23
Conceptual site plan showing feasible option
permitted under proposed rezoning



NO.	DESCRIPTION	BY	DATE

SHEET TITLE:

PROJECT DESCRIPTION:
BOXABL Concept Layout
5.19.23



DATE:
10/31/2023

SCALE:

SHEET:
A-1



Elevation 1



Elevation 2

NO.	DESCRIPTION	BY	DATE

SHEET TITLE:

PROJECT DESCRIPTION:
BOXABL Concept Layout
5.19.23



DATE:
10/31/2023

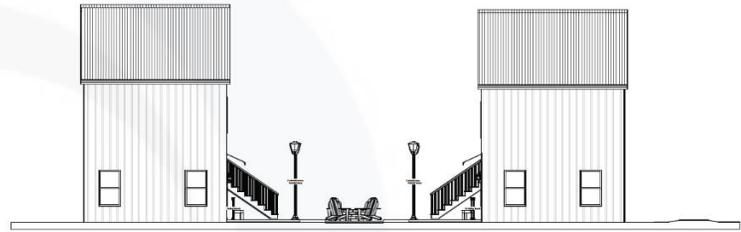
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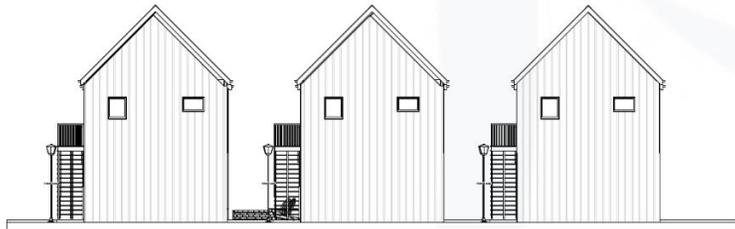
A-4



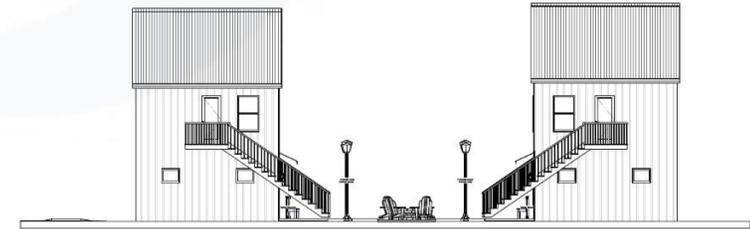
Elevation 3



Elevation 4



Elevation 5



Elevation 6

NO.	DESCRIPTION	BY	DATE

SHEET TITLE:

PROJECT DESCRIPTION:
BOXABL Concept Layout
 5.19.23



DATE:

10/31/2023

SCALE:

SHEET:

A-5

Case No: SPUD-1561

Applicant: Catholic Charities of The Archdiocese of Oklahoma City

Existing Zoning: I-1 / R-4 / SYT

Location: 1101 S. Youngs Blvd.



Aerial Photo from 2/2022

Note: "Subject" is located approximately 1,537' West of S. Pennsylvania Ave. and 1,765' North of SW 15th St.

Simplified Planned Unit Development



The City of
OKLAHOMA CITY



0 100 200
Feet