

**APPROVED**

12-17-2024

BY THE CITY COUNCIL  
*Amy K. Simpson* CITY CLERK

THE CITY OF OKLAHOMA CITY  
SIMPLIFIED PLANNED UNIT DEVELOPMENT DISTRICT

**SPUD-1672**

**MASTER DESIGN STATEMENT FOR**

217 NW 150<sup>th</sup> St.

**August 28, 2024**  
**October 3, 2024**  
**October 11, 2024**

**PREPARED BY:**

WILLIAMS, BOX, FORSHEE & BULLARD P.C.

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*522 Colcord Drive*

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## **SPUD-1672 MASTER DESIGN STATEMENT**

This document serves as the Master Design Statement and fulfills the ordinance requirements for the Simplified Planned Unit Development (Chapter 59, Section 14150.B of the Oklahoma City Municipal Code, 2020, as amended).

### **I. Special Development Regulations:**

The following Special Development Regulations and/or limitations are placed upon the development of the Simplified Planned Unit Development. Planning and zoning regulations will be those that are in effect at the time of development of this Simplified Planned Unit Development; provided, however, that the density and or intensity of the SPUD shall not be increased. Development is when a permit is issued for any construction or addition to any structure on a development tract. Certain zoning districts are referred to as a part of the Special Development Regulations of this Simplified Planned Unit Development. For purposes of interpretation of these Special Development Regulations, the operative and controlling language and regulations of such zoning districts shall be the language and regulations applicable to the referenced zoning districts as contained in the City of Oklahoma City's Planning and Zoning Code as such exists at the time of development of this Simplified Planned Unit Development. In the event of conflict between provisions of this SPUD and any of the provisions of the Oklahoma City Municipal Code, as amended ("Code"), in effect at the time a permit is applied for with respect to any lot, block, tract and/or parcel of land subject to this SPUD, the provisions of the Code shall prevail and be controlling; provided however, that in the event of a conflict between the Special Use and Development Regulations specifically negotiated as a part of this SPUD and the provisions of the Code in effect at the time a permit is applied for with respect to any lot, block, tract and/or parcel of land subject to this SPUD, such Special Use and Development Regulations of this SPUD shall prevail and be controlling.

1. This site will be developed in accordance with the regulations of the **O-2 General Office District** (OKC Zoning Ordinance, 2020, as amended), except that the following restrictions will apply:

In addition to the uses permitted in the O-2 District, the following uses shall also be permitted on this site:

- |         |  |
|---------|--|
| 8300.14 | Automotive and Equipment: Cleaning and Repairs, Light Equipment [limited to paintless dent repair and excluding all other uses contemplated under this use unit] |
| 8300.21 | Automotive and Equipment: Storage, limited to indoor storage only. Any garage doors or service areas shall face southwest.                                       |
| 8300.60 | Personal Storage [living quarters for caretakers and/or security personnel shall not be permitted]   |

\*No hazardous chemicals or materials may be stored on-site.

2. **Maximum Building Height:**

The maximum building height shall be 20 feet and one story.

**3. Maximum Building Size:**

The maximum building size shall be 4,500 square feet.

**4. Maximum Number of Buildings:**

The maximum number of buildings shall be in accordance with the base zoning district.

**5. Building Setback Lines**

Northwest:	15 feet
East:	15 feet
Southwest:	25 feet

**6. Sight-Proof Screening:**

The existing screening shall be permitted to remain and deemed to conform to applicable regulations.

**7. Landscaping:**

The subject parcel shall meet all requirements of the City of Oklahoma City's Landscaping Ordinance in place at the time of development. The existing tree line along the east SPUD boundary line shall remain undisturbed.

**8. Signs:**

**8.1 Freestanding Accessory Signs**

Freestanding signs will be in accordance with the base zoning district regulations.

**8.2 Attached Signs**

Attached signs will be in accordance with the base zoning district regulations.

**8.3 Non-Accessory / Off-Premise Signs**

Non-accessory / Off-premise signs will be prohibited.

**8.4 Electronic Message Display Signs**

Electronic Message Display signs will be prohibited.

**9. Access:**

Access shall be taken from the existing private access easement off of NW 150<sup>th</sup> St.

**10. Sidewalks:**

N/A.

**II. Other Development Regulations:**

**1. Architecture:**

Exterior building wall finish on all main structures, exclusive of windows and doors, shall consist of brick veneer, rock or stone masonry, stucco, and cementitious siding (including,

but not limited to, the brand commonly known as James Hardie). Exposed metal or exposed concrete block buildings shall not be permitted.

**2. Open Space:**

N/A.

**3. Street Improvements:**

N/A.

**4. Site Lighting:**

The site lighting in this SPUD shall be in accordance with Chapter 59, Article XII, Section 59-12350 of the Oklahoma City Municipal Code, 2020, as amended.

**5. Dumpsters:**

Dumpsters shall be located within an area screened by a fence or masonry wall of sufficient height that screens the dumpster from public streets and residences and shall be placed no closer than 50 feet from all property lines adjacent to residential zoning district or use.

Trash collection facilities in this SPUD shall be in accordance with Chapter 49 of the Oklahoma City Municipal Code, 2020, as amended.

**6. Parking:**

The design and number of all parking facilities in this SPUD shall be in accordance with Chapter 59, Article X of the Oklahoma City Municipal Code, 2020, as amended, except there shall be a maximum of six (6) parking spaces.

**7. Maintenance:**

Maintenance of the common areas in the development shall be the responsibility of the property owner or Property Owners Association. No structures, storage of material, grading, fill, or other obstructions, including fences, either temporary or permanent, that shall cause a blockage of flow or an adverse effect on the functioning of the storm water facility, shall be placed within the common areas intended for the use of conveyance of storm water, and/or drainage easements shown. Certain amenities such as, but not limited to, walks, benches, piers, and docks, shall be permitted if installed in a manner to meet the requirements specified above.

**8. Drainage:**

Development of this parcel will comply with Chapter 16 of the Oklahoma City Municipal Code, 2020, as amended.

**9. Hours of Operation:**

The use unit 8300.14 Automotive and Equipment: Cleaning and Repairs, Light Equipment [limited to paintless dent repair and excluding all other uses contemplated under this use unit] shall only operation during the following days/times:

Monday through Saturday from 8:00 a.m. to 5:00 p.m.

### **III. Supporting Documents**

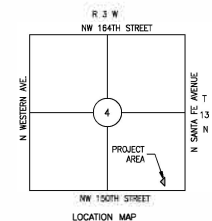
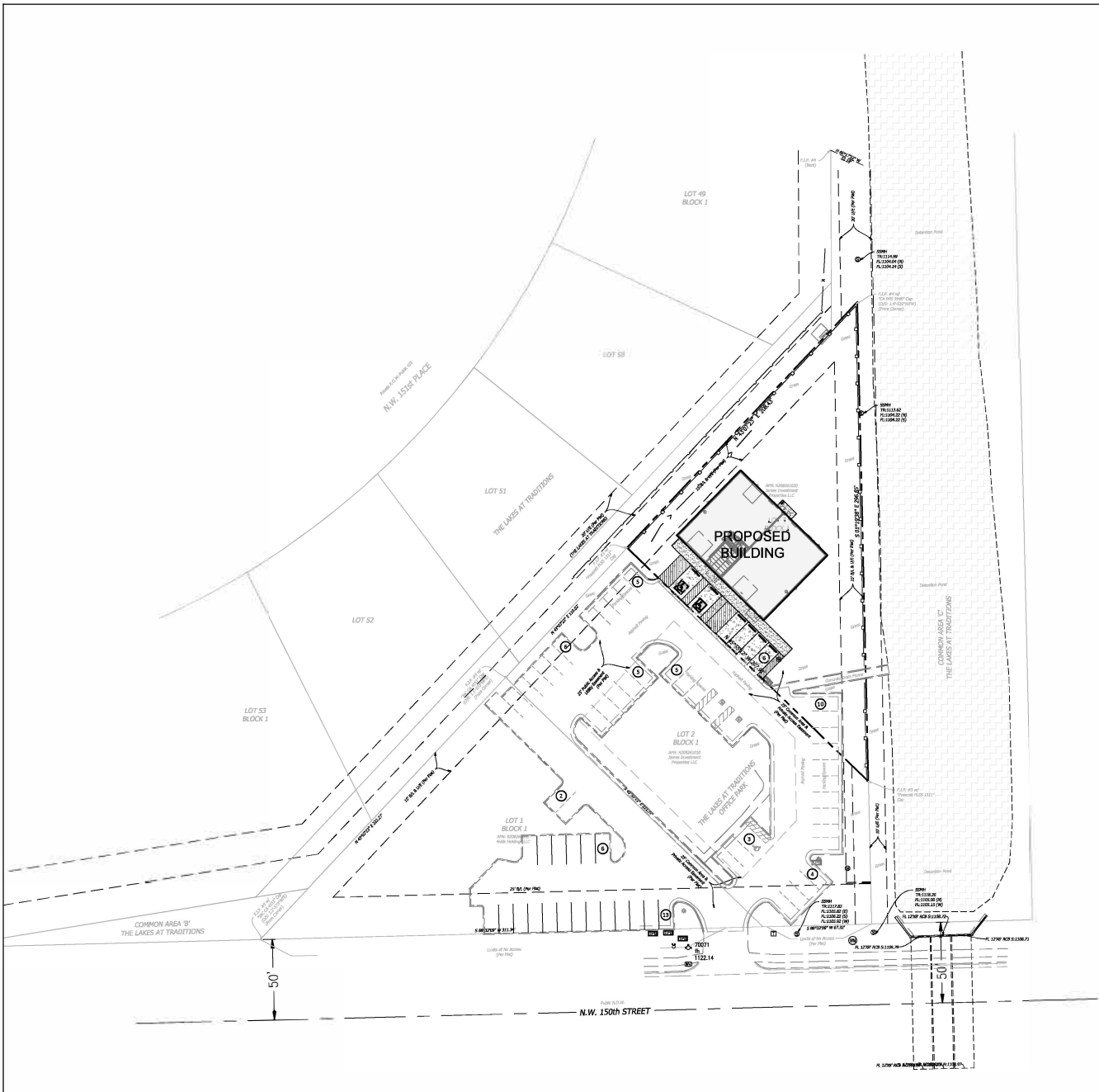
Exhibit A: Legal Description

Exhibit B: Conceptual Site Plan

Exhibit C: Topographic Survey

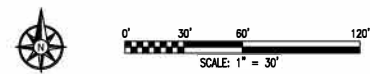
SPUD-1672 Exhibit A - Legal Description

Lot Three (3), Block One (1) of THE LAKES AT TRADITIONS-OFFICE PARK, being a part of the Southeast Quarter (SE/4) of Section Four (4), Township Thirteen (13) North, Range Three (3) West of the Indian Meridian, Oklahoma City, Oklahoma, according to the recorded Plat thereof.



PARKING TABLE

STANDARD PARKING SPACE 9'x18'	60
ADA PARKING SPACE 11'x18'	5
TOTAL	65



**UTILITY WARNING:**  
THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM RECORD DOCUMENTS OR FIELD LOCATIONS BY THE OPERATOR. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH THE SURVEYOR DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

UTILITY ELEVATIONS AND SIZES MAY HAVE BEEN MEASURED UNDER ADVERSE FIELD CONDITIONS. UPON EXPOSING THE UTILITY, ELEVATIONS AND LINE SIZES SHOULD BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHOULD VERIFY CRITICAL ELEVATIONS USING THE BENCHMARK PROVIDED BY THE SURVEYOR OF ENGINEER. ANY DISCREPANCIES SHOULD BE IMMEDIATELY BROUGHT TO THE ENGINEER'S AND SURVEYOR'S ATTENTION.



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**CIVIL**  
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**STRUCTURAL**  
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**JAMES INVESTMENT  
PROPERTIES OFFICE RETAIL**  
Oklahoma City, Oklahoma

Chief:

Project Number:  
23005.0

Seal:

BRYAN W. RICHARDS, P.E.  
EXPIRES: 11/26/2024  
Chief - Survey

07/12/2024

Revisions:

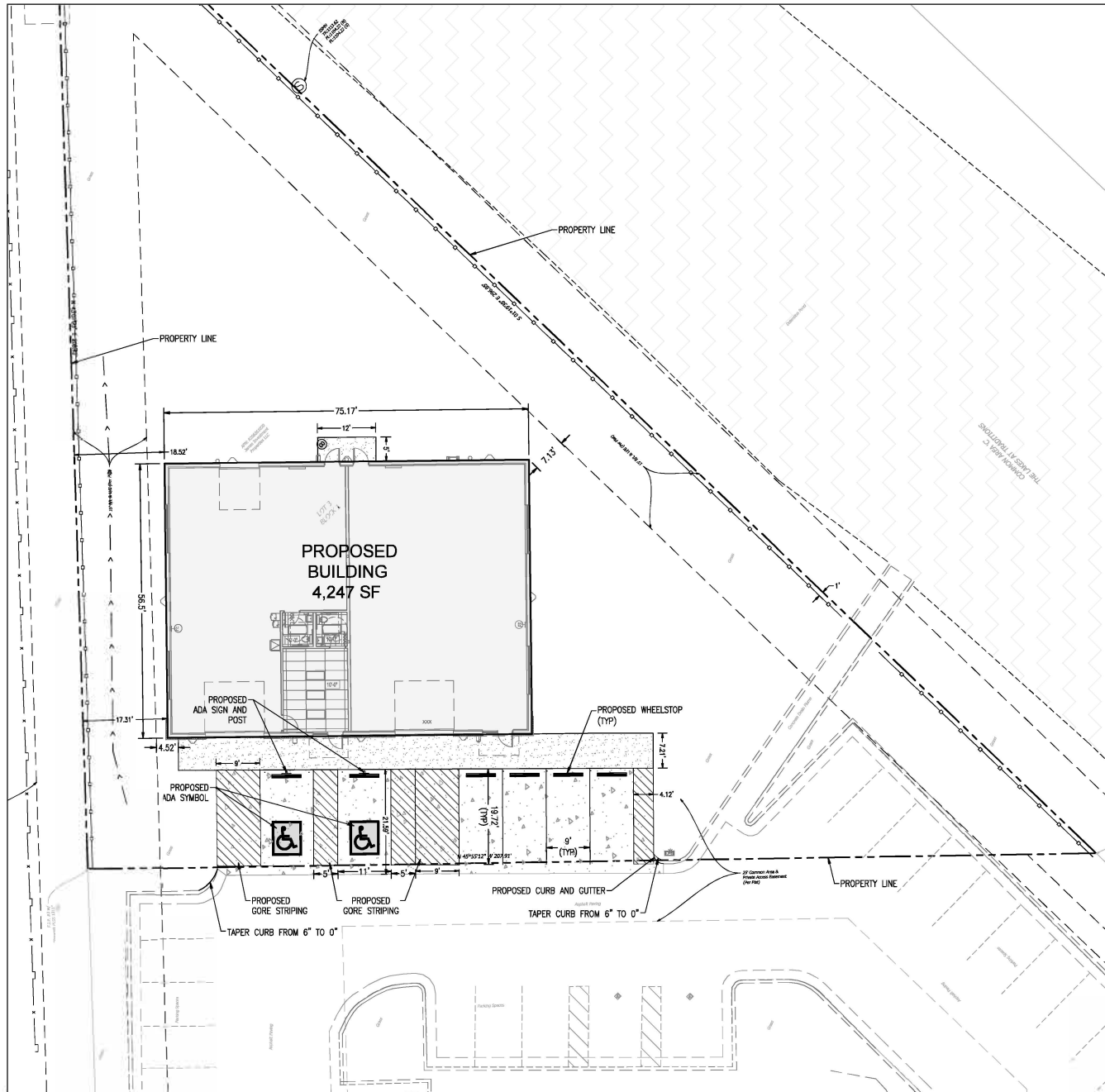
Sheet Title:

OVERALL SITE PLAN

Sheet Number:

**C1.0**

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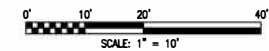
#### GENERAL CONSTRUCTION NOTES

1. ALL WORK PERFORMED AND MATERIALS SUPPLIED SHALL CONFORM TO THE CITY OF OKLAHOMA CITY "STANDARDS AND SPECIFICATIONS" AND THE "SPECIFICATIONS FOR THE CONSTRUCTION OF PUBLIC IMPROVEMENTS".
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ALL UTILITY COMPANIES AND GOVERNMENT AGENCIES WHO MIGHT HAVE UTILITY LINES ON OR ABOUT THE PREMISES, OR WHO MIGHT BE AFFECTED BY THE CONSTRUCTION. THE CONTRACTOR SHALL ALSO COORDINATE HIS ACTIVITIES WITH THE UTILITY COMPANIES TO ENSURE COMPLIANCE WITH THE PROJECT SCHEDULE. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO LOCATE AND PROTECT EXISTING UTILITY LINES, AND SHALL REPAIR ANY DAMAGES.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIS OWN CONSTRUCTION STAKING.
4. ALL SIGNS, PAVEMENT MARKINGS, AND OTHER TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
5. ALL PAVEMENT STRIPING SHALL BE APPLIED IN TWO COATS, FOUR (4) INCHES WIDE, WHITE COLOR UNLESS SHOWN OTHERWISE ON THE PLANS.
6. IN DRIVE AND PARKING AREAS WHERE NEW CONCRETE PAVING IS TO ADJUT EXISTING PAVING, THE CONTRACTOR SHALL CONSTRUCT A 10" THICKENED EDGE IN THE CONCRETE.
7. UNLESS OTHERWISE DIRECTED THE CONTRACTOR SHALL PROVIDE A MEDIUM BROWN FINISH ON ALL CONCRETE WALLS, DAMPS, STAIRS AND PAVED SURFACES (REFER TO ARCHITECTURAL PLANS & SPECS. FOR SIDEWALK FINISH AROUND BUILDING).
8. THE CONTRACTOR MUST FILE A "NOTICE OF INTENT" WITH THE OKLAHOMA DEPARTMENT OF COMMERCE QUALITY AND THE CITY OF OKLAHOMA CITY, PRIOR TO COMMENCING EARTHWORK, CLEARING OR DEMOLITION OPERATIONS.
9. REFER TO ARCHITECTURAL PLANS AND SPECS. FOR CONSTRUCTION INFORMATION AND DETAILS FOR BUILDING ERECTION.
10. ANY AND ALL QUANTITIES SHOWN ARE FOR REFERENCE ONLY AND HAVE BEEN CALCULATED FOR MUNICIPAL OR OWNER PLANNING PURPOSES ONLY. THE CONTRACTOR(S) IS RESPONSIBLE TO VERIFY ALL QUANTITIES AND CONSTRUCT THE PROJECT AS SHOWN ON THE DRAWINGS AND SPECIFICATIONS. PRICE BID SHALL INCLUDE ALL LABOR, MATERIAL AND EQUIPMENT NECESSARY TO CONSTRUCT AND FURNISH A FULLY FUNCTIONAL AND COMPLETE SITE.
11. THE CONTRACTOR SHALL VERIFY AND COORDINATE WITH THE ARCHITECTURAL AND LANDSCAPE ARCHITECT PLANS FOR THE REMAINING WALL, STOPS AND FINISH.
12. THE CONTRACTOR SHALL COORDINATE WITH THE ARCHITECTURAL AND LANDSCAPE ARCHITECTURAL PLANS FOR SITE AMENITY, LANDSCAPE AREAS, FLOWERS AND SPECIAL SIDEWALK FINISHES.
13. THE CONTRACTOR SHALL OBTAIN A COPY OF THE OMC APPROVED WA, SD, AND DP PLANS FOR COORDINATION OF ALL WORK WITHIN THE PUBLIC R-2-W OR UTILITY EXTENSIONS.
14. CONTRACTOR SHALL FIELD VERIFY NEW SIDEWALKS ALIGN WITH EXISTING BUILDING DOORWAYS.

#### LEGEND

- PROPOSED CONCRETE SIDEWALK
- PROPOSED CONCRETE PAVEMENT
- PROPOSED WHEELSTOP

## SPUD-1672 Exhibit B



**UTILITY WARNING:**  
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UTILITY ELEVATIONS AND SIZES MAY HAVE BEEN MEASURED UNDER ADVERSE FIELD CONDITIONS. UPON EXPOSING THE UTILITY, ELEVATIONS AND LINE SIZES SHOULD BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. CONTRACTOR SHOULD VERIFY CRITICAL ELEVATIONS USING THE BENCHMARK PROVIDED BY THE SURVEYOR ON EACH LOT. ANY DISCREPANCIES SHOULD BE IMMEDIATELY BROUGHT TO THE ENGINEER'S AND SURVEYOR'S ATTENTION.



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#### STRUCTURAL

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# JAMES INVESTMENT PROPERTIES OFFICE RETAIL

Oklahoma City, Oklahoma

Client:

Project Number:  
23006.0

Seat:



BRYAN W. RICHARDS, P.E.  
Expire: 11/26/2024  
LBR - 11/16/18

07/12/2024

Revisions:

Sheet Title:

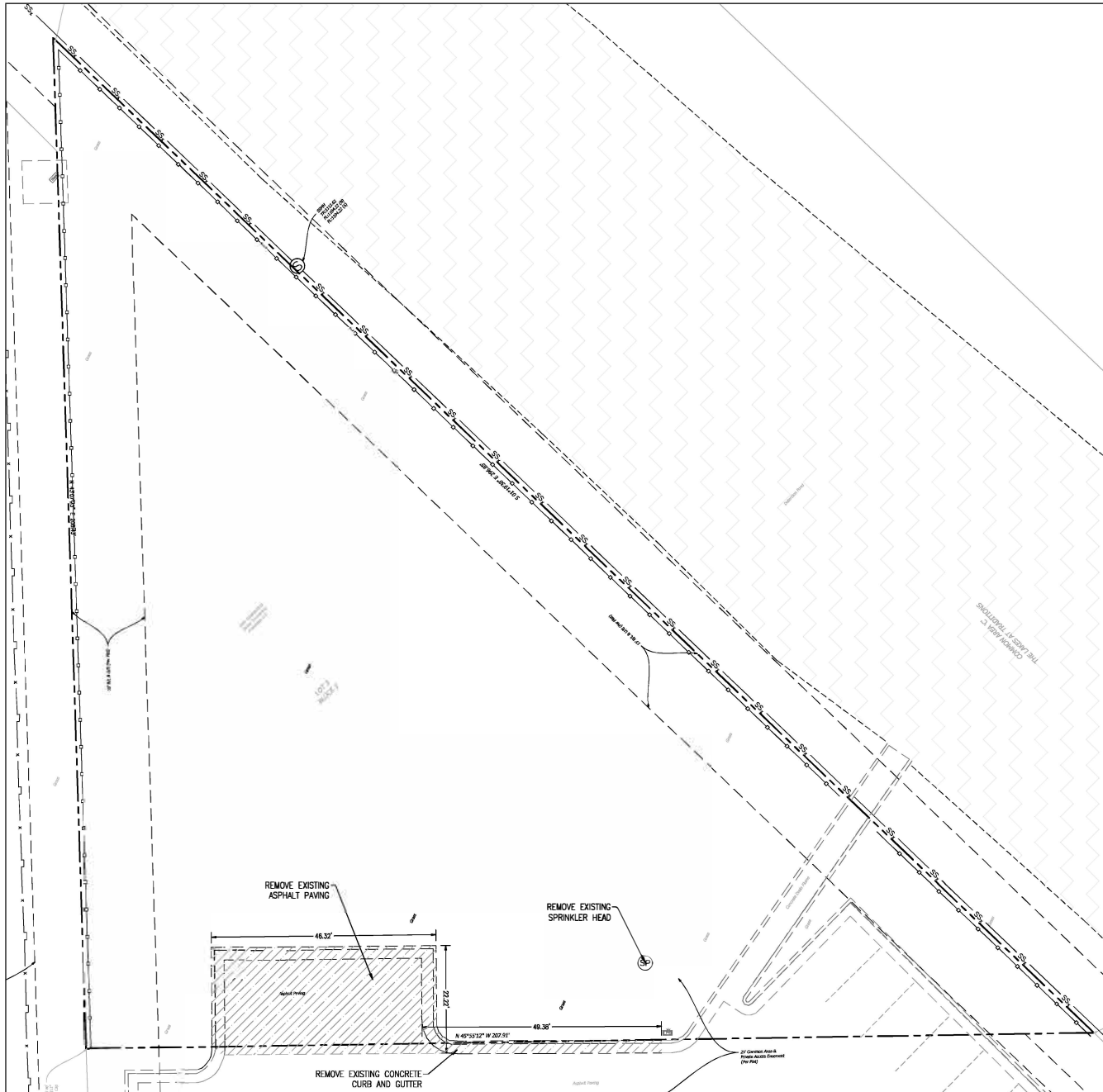
SITE PLAN

Sheet Number:

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#### GENERAL DEMOLITION NOTES

1. CONTRACTOR IS RESPONSIBLE FOR RESTRICTING ACCESS TO THE CONSTRUCTION AREAS AT ALL TIMES.
2. CONTRACTOR MUST COORDINATE AND MAINTAIN PUBLIC ACCESS TO ADJACENT BUSINESSES ALONG SHARED DRIVEWAYS.
3. CONTRACTOR IS RESPONSIBLE FOR DISPOSAL OF ALL REMOVED CONCRETE, ASPHALT, BUILDING, AND OTHER DEBRIS.
4. REMOVAL OF TREES INCLUDE COMPLETE REMOVAL OF TREE & STUMP.
5. CONTRACTOR SHALL NOT DISTURB EXISTING UTILITIES NOT MARKED FOR REMOVAL OR ABANDONMENT.
6. REMOVAL OF NECESSARY POWER POLING SHALL BE COORDINATED BY THE CONTRACTOR WITH THE APPROPRIATE UTILITY COMPANY.
7. FENCE REMOVAL SHALL INCLUDE FENCE, POST, GATES, AND POST FOOTINGS UNLESS OTHERWISE NOTED.
8. UTILITY REMOVAL INCLUDES BURIED LINES AND COORDINATION WITH UTILITY COMPANIES.
9. PAVEMENT REMOVAL SHALL INCLUDE SAW CUTTING NEAT LINE AND FULL DEPTH REMOVAL OF PAVEMENT AND SUBGRADE MATERIALS.
10. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING UTILITY LOCATIONS, SIZES, MATERIAL AND DEPTHS PRIOR TO CONSTRUCTION ACTIVITY. THE SURVEY INFORMATION SHOWN WAS NOT COLLECTED BY BMR DESIGN GROUP, LLC.
11. NO FILL EMBANKMENT MATERIAL OR MATERIAL STORAGE SHALL BE PERMITTED WITHIN THE FEMA FLOOD PLAIN LIMITS.
12. PRIVATE UTILITY REMOVAL AND RELOCATION SHALL BE COORDINATED WITH THE UTILITY PROVIDER. CONTRACTOR SHALL CONTACT AND COORDINATE.
13. SEE UTILITY PLANS FOR ADDITIONAL REMOVAL REQUIRED FOR INTERCONNECTIONS.

#### LEGEND

 REMOVE PAVEMENT/SIDEWALK/SUBGRADE

## SPUD-1672 Exhibit B



0' 10' 20' 40'  
SCALE: 1" = 10'

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**PRAIRIE**  
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**JAMES INVESTMENT  
PROPERTIES OFFICE RETAIL**  
Oklahoma City, Oklahoma

Client:

Project Number:  
23006.0

Seal:



BRYAN W. RICHARDS, P.E.  
EXPIRES: 11/26/2024

Date: 07/12/2024

Revisions:

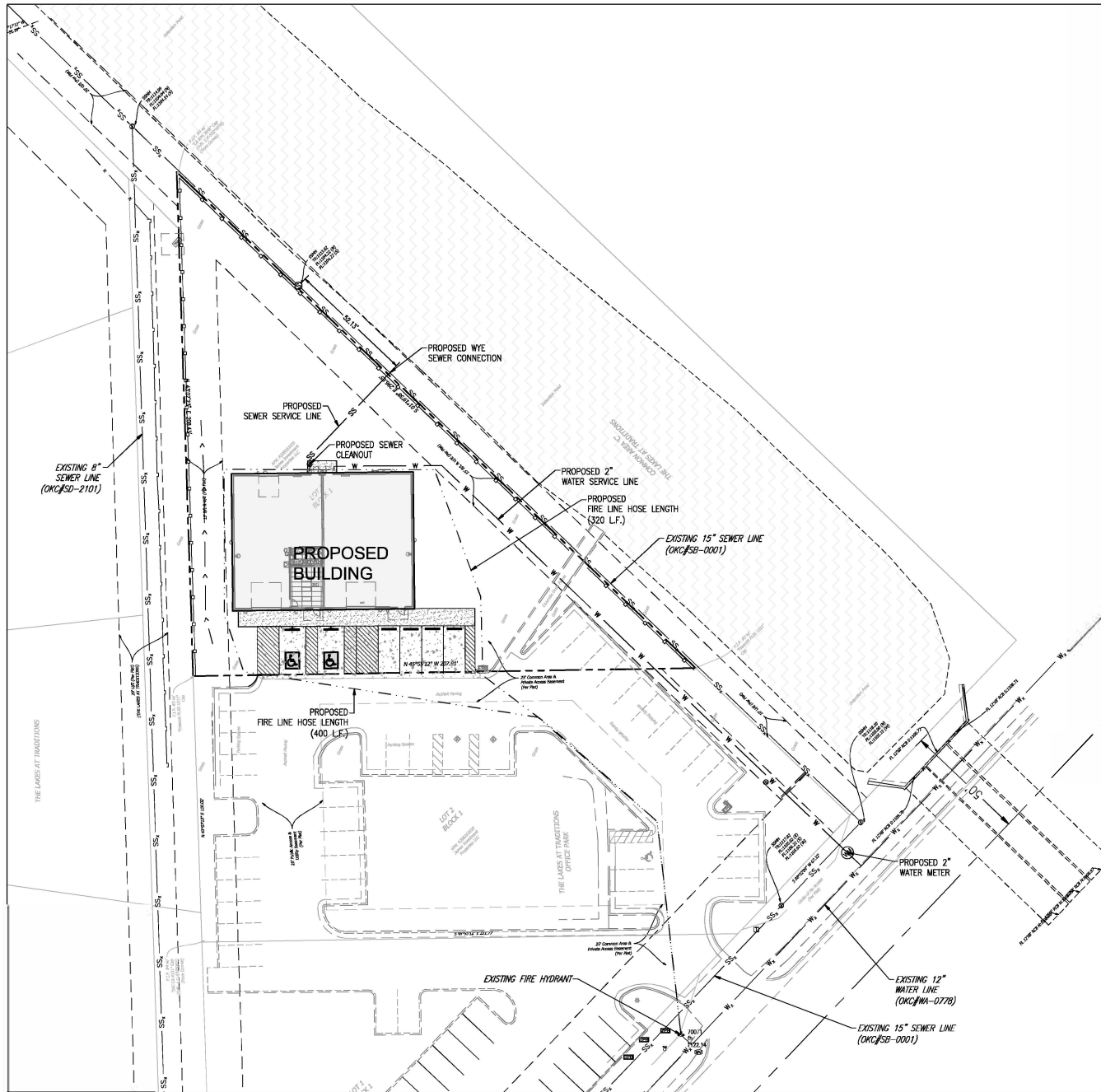
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DEMO PLAN

Sheet Number:

**C3.0**

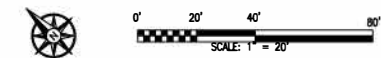
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1. CONTRACTOR SHALL COORDINATE WITH THE CITY FOR ALL INTERCONNECTIONS TO PUBLIC UTILITIES.
2. CONTRACTOR SHALL CALL ONE PRIOR TO ANY CONSTRUCTION ACTIVITIES. CALL ONE PRIOR TO TROOPING, SH-3022 / L-3022-222-222. THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND OTHER SOURCES. THE ENGINEER MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPOSE ALL SUCH UTILITIES IN THE AREA EITHER IN SERVICE OR IN DISUSE. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION INDICATED ALTHOUGH THE SURVEYOR HAS CRITICAL ELEVATIONS USING THE BENCHMARK PROVIDED BY THE SURVEYOR OR ENGINEER. ANY DISCREPANCIES SHOULD BE IMMEDIATELY BROUGHT TO THE ENGINEER'S AND SURVEYOR'S ATTENTION.
3. CONTRACTOR SHALL VERIFY ALL UTILITY CONNECTIONS TO THE BUILDING(S) WITH THE A.E.P. PLANS.
4. UNLESS OTHERWISE NOTED, PIPE 3 INCHES AND SMALLER SHALL BE PER STYLE OR PER WATER PIPE.
5. ALL VALVES, MANHOLE LIDS, AND SEWER CLEAN-OUTS LOCATED IN PAVED AREAS, SHALL BE NOTED FOR IN-20 TRAVEL LOCATIONS.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ELEVATIONS AND ADJUSTING ALL CORNERS AND MANHOLES, WITHIN BORDERS, TRAVEL BORDERS, AND OTHER UTILITIES TO MEET THE SPECIFICATIONS OF EACH UTILITY COMPANY RESPECTIVELY.
7. CONTRACTOR SHALL MAKE OR LOWER ALL EXISTING VALVE BOXES AND COVERS, PER UTILITY COMPANY, WITHIN BORDERS, TRAVEL BORDERS, AND OTHER UTILITIES TO MEET THE SPECIFICATIONS OF EACH UTILITY COMPANY RESPECTIVELY.
8. ALL 4\"/>

LEGEND	
W <sub>1</sub>	EXISTING WATER LINE
W <sub>2</sub>	BUILDING HOSE (DB)
W <sub>3</sub>	EXISTING FIRE HYDRANT
SS <sub>1</sub>	EXISTING SANITARY SEWER LINE
CH <sub>1</sub>	EXISTING SANITARY SEWER MANHOLE
CH <sub>2</sub>	EXISTING OVERHEAD ELECTRIC LINE
NG <sub>1</sub>	EXISTING NATURAL GAS LINE
TUC <sub>1</sub>	EXISTING COMMUNICATIONS
W	PROPOSED WATER/FIRE LINE
SS	PROPOSED SANITARY SEWER
SS	PROPOSED SANITARY SEWER CLEAN OUT
G	PROPOSED NATURAL GAS
PUG	PROPOSED ELECTRIC
SD	PROPOSED STORM DRAIN LINE
W <sub>1</sub>	PROPOSED GAS METER
W <sub>2</sub>	PROPOSED 2\"/>

## SPUD-1672 Exhibit B



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**JAMES INVESTMENT  
PROPERTIES OFFICE RETAIL**  
Oklahoma City, Oklahoma

---

Client: \_\_\_\_\_

Project Number: 23006.0

Scale: \_\_\_\_\_

**PROFESSIONAL ENGINEER**  
BRYAN W. RICHARDS, P.E.  
EXPIRATION: 11/30/2024  
Oklahoma  
23079

07/12/2024

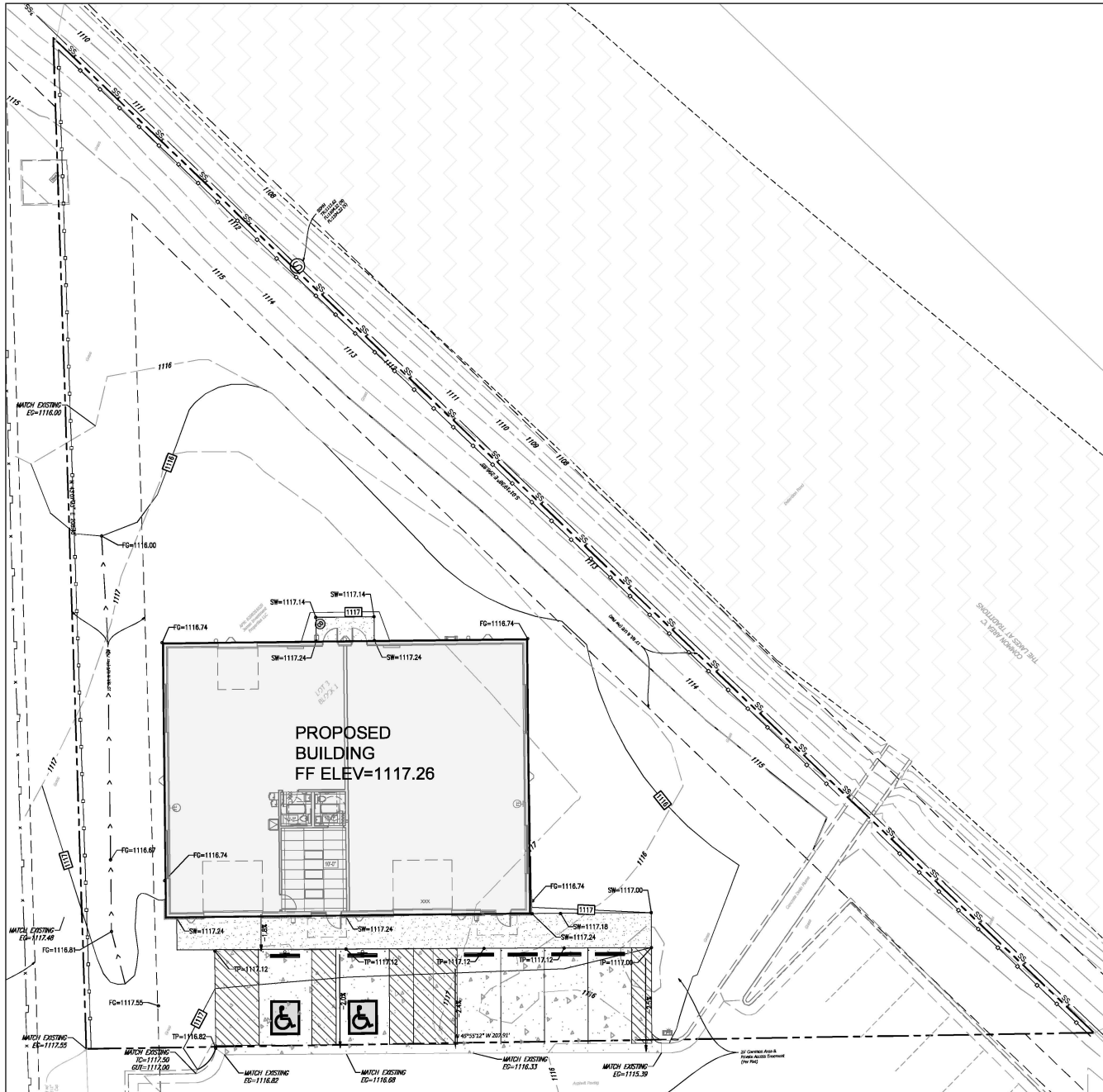
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Sheet Title: \_\_\_\_\_

Sheet Number: **C4.0**

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**UTILITY PLAN**



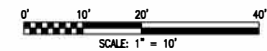
#### GENERAL GRADING NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING ALL UTILITY COMPANIES AND GOVERNMENT AGENCIES WHO MIGHT HAVE LINES ON OR ABOUT THE PREMISES OR WHO MIGHT BE AFFECTED BY THE CONSTRUCTION. THE CONTRACTOR SHALL ALSO COORDINATE HIS ACTIVITIES WITH THE UTILITY COMPANIES TO ENSURE COMPLIANCE WITH THE PROJECT SCHEDULE. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO LOCATE AND PROTECT EXISTING UTILITY LINES, AND SHALL REPAIR ANY DAMAGES.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIS OWN CONSTRUCTION STAGING.
3. PRIOR TO PLACEMENT OF FILL, THE GROUND SHALL BE STRIPPED (6\"/>

#### LEGEND

---	EXISTING CONTOUR (1' INTERVAL)
---	PROPOSED CONTOUR (1' INTERVAL)
->	DRAINAGE FLOW ARROW
DUT	DITCH
CUT	CUT
TC	TOP OF CULVERT
SW	TOP OF SIDEWALK
TW	TOP OF WALL
BW	BOTTOM OF WALL
FL	FLOW LINE ELEVATION
TD	TOP OF DITCH
EG	EXISTING GRADE
FG	FINISH GRADE

## SPUD-1672 Exhibit B



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**JAMES INVESTMENT  
PROPERTIES OFFICE RETAIL**  
Oklahoma City, Oklahoma

Client:

Project Number:  
23005.0  
Sheet:



BRYAN W. RICHARDS, P.E.  
EXPIRES: 11/30/2024  
Civil - Review

07/12/2024

Revisions:

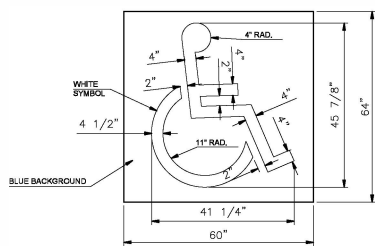
Sheet Title:

**GRADING PLAN**

Sheet Number:

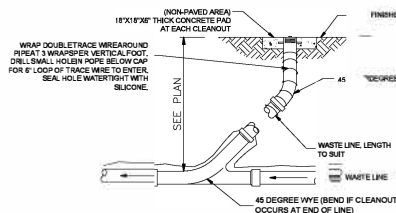
**C5.0**

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1 TYPICAL HANDICAP PARKING STRIP  
NOTE: ALL ADA MARKINGS SHALL MEET CURRENT FEDERAL AND LOCAL GUIDELINES

THE CONTRACTOR SHALL OBTAIN CURRENT CITY OF OKLAHOMA CITY STANDARD DRAWING DETAILS FOR THE FOLLOWING ITEMS:  
WATER SERVICE LINE, METER AND DETAILS  
SANITARY SEWER LINE AND DETAILS  
CONCRETE JOINT DETAILS  
ADA RAMP AND SURFACE MATERIALS  
STANDARD DRIVEWAY DETAILS  
CONTRACTOR SHALL PROVIDE PAVEMENT JOINT LAY OUT TO ENGINEER FOR REVIEW AND APPROVAL



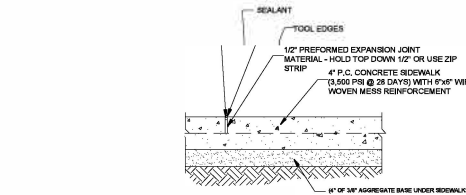
8 SANITARY SEWER CLEAN OUT DETAIL  
NOTE: 1. LIDS IN TRAFFIC AREAS SHALL BE H-20 RATED



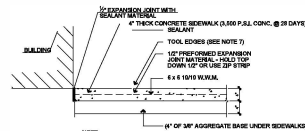
2 ADA PARKING SPACE SIGN  
NOTE: ALL ADA SIGNAGE SHALL MEET CURRENT FEDERAL AND LOCAL GUIDELINES



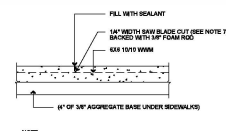
3 ADA PARKING SPACE SIGN  
NOTE: ALL ADA SIGNAGE SHALL MEET CURRENT FEDERAL AND LOCAL GUIDELINES



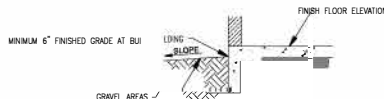
4 SIDEWALK DETAIL  
NOTE:  
1. SEE CITY STANDARDS FOR ADA RAMPS AND TACTILE SURFACING DETAILS.  
2. EXPANSION JOINTS TO BE LOCATED 20' TO 25' ON CENTER THROUGHOUT WALKS AND PATIO.



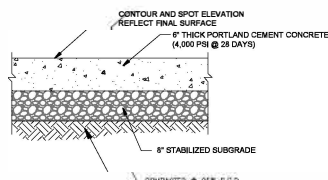
5 SIDEWALK EXPANSION JOINT  
P.C. CONCRETE NOTES:  
1. 12\"/>



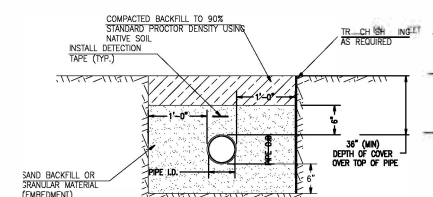
6 SIDEWALK SAW CONTROL JOINT



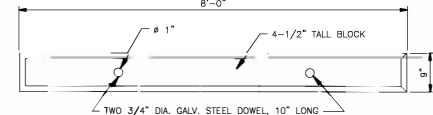
9 GRADING ADJACENT TO BUILDING (NONPAVED AREAS)



10 LIGHT DUTY CONCRETE  
NOTE:  
1. ALL PAVEMENT AND SUBGRADE RECOMMENDATIONS PROVIDED ARE FROM THE GEOTECHNICAL ENGINEERS REPORT PROVIDED.  
2. THE CONTRACTOR SHALL SUBMIT A JOINT LAYOUT PLAN TO THE ENGINEER TO REVIEW AND APPROVE.

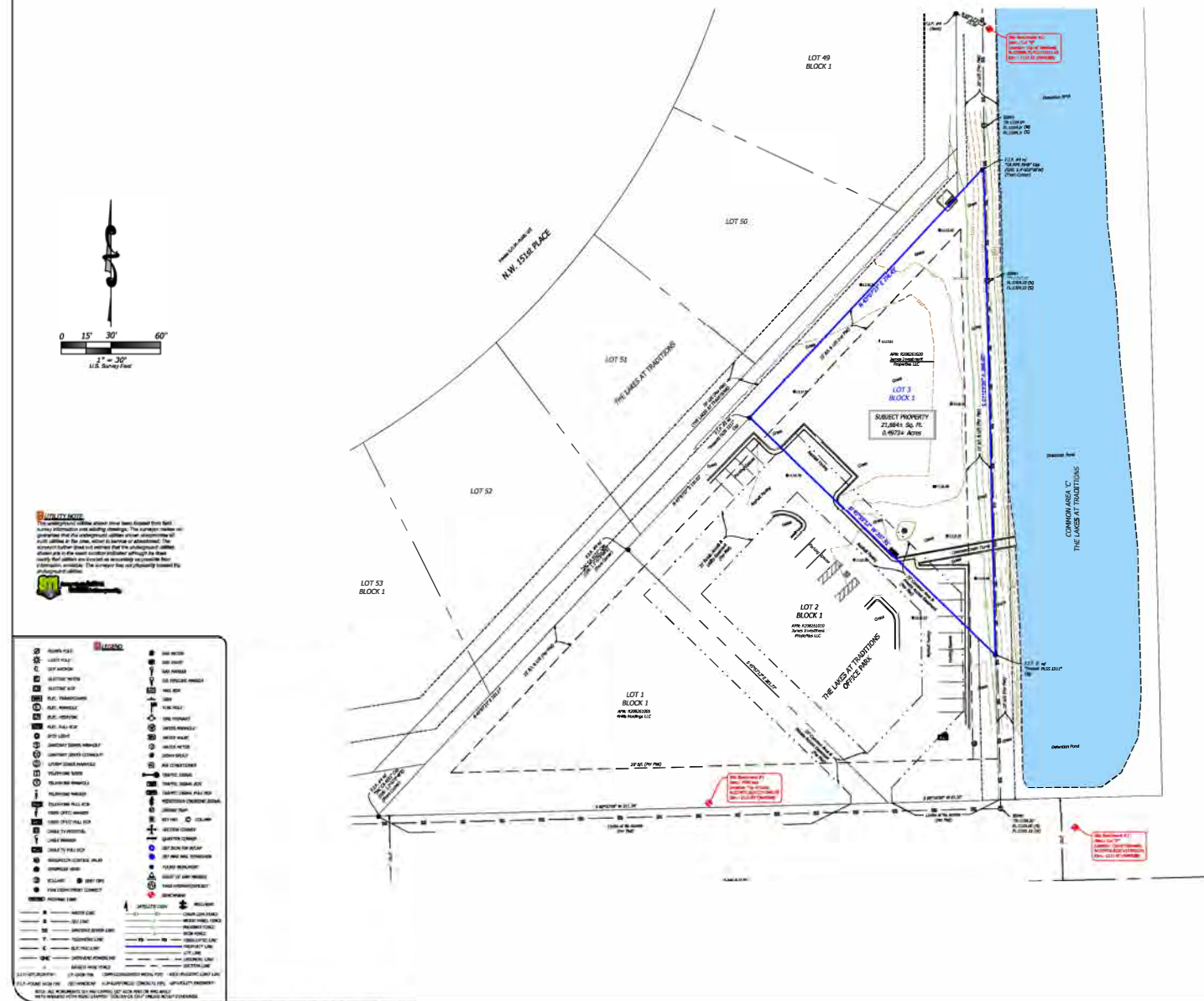


7 PRIVATE PVC TRENCH DETAILS  
PVC Pipe Notes:  
1. Pipe - PVC pressure pipe shall be ASTM C-800, Pressure Class 200 and with a minimum dimension ratio of fourteen (DR-14).  
2. Fittings - Fittings shall be maximum joint type as specified under ductile iron fittings. All fittings shall have polyethylene encasement tubing with a thickness of 8 mil.  
3. Tracer Wire (Conductor) - Install one (1) strand of number twelve (12) gauge copper tracer wire along top of all PVC piping. String to top of ground and one or driveways, fire hydrants, and other spotting cases.  
4. Joint Restraints - block or mechanical joint restraint for AWWA C-800. Joint restraint shall be manufactured by Dain Iron Steel, Inc. or Uni-Fit by The First Water, Inc. or approved equal.  
5. Water Service Connections address steel saddle and gasket.  
6. All bends require concrete thrust block or approved restrained joint pipe.



11 PRE-CAST CONCRETE WHEEL STOP  
N.T.S.





- 1. GENERAL NOTES**
1. The property is located in the City of Oklahoma City, Oklahoma, and is subject to the City of Oklahoma City's Comprehensive Zoning Ordinance, Chapter 10, Section 10-1-1, which requires that all property be zoned for its intended use.
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TOPOGRAPHIC SURVEY  
OF  
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Sheet 1 of 1